

INDEX OF SHEETS

SHEET NO. DESCRIPTION

1	TITLE SHEET
2	GENERAL PROJECT LAYOUT
3 - 5	TYPICAL SECTIONS
6 - 8	GENERAL NOTES AND SPECIFICATION DATA
9 - 10	STANDARD OF STORM SEWER, CULVERTS, STRUCTURES TO BE REMOVED, BASE REPAIR, TERMINAL ANCHORAGE, ROADWAY QUANTITIES, BRIDGE AND CONSTRUCTION PAVEMENT MARKINGS
11 - 11a	ESTIMATE AND QUANTITY SHEETS
12 - 12a	TRAFFIC CONTROL PLANS
13 - 13a	PLAN PROFILE SHEETS
14 - 14a	RAIL DETAILS
15 - 15a	EXISTING INTERSECTION AND TURNAROUND MODIFICATION DETAILS
16 - 16a	SUMMARY OF PAVEMENT MARKING QUANTITIES
17 - 17a	SUMMARY OF SUELL SIGNS
18 - 18a	SUMMARY OF SIGNS WITH REMOVABLE COPY REFLECTORIZED
19 - 19a	PERMANENT PAVEMENT MARKING & SIGNING LAYOUT
20 - 20a	SUMMARY OF SAFETY LIGHTING
21 - 21a	DRIVEWAY DETAILS
22 - 22a	BRIDGE LAYOUT
23 - 23a	STORM SEWER DRAINAGE AREA AND PLAN PROFILE
24 - 24a	STORM SEWER HYDRAULIC COMPUTATIONS (BRIDGES)
25 - 25a	DRAINAGE AREA MAP AND HYDRAULIC DATA (BRIDGES)
26 - 26a	CULVERT DRAINAGE AREA AND HYDRAULIC COMPUTATIONS
27 - 27a	CROSS-SECTIONS AT CULVERT SITES
28 - 28a	CONC. PAY DETAIL (CONTINUOUSLY REINFORCED STEEL BARS) (MOD)
29 - 29a	CONC. PAY DETAIL (CONTINUOUSLY REINFORCED STEEL BARS) (MOD)
30 - 30a	CONC. PAY DETAIL (CONTINUOUSLY REINFORCED STEEL BARS) (MOD)
31 - 31a	CONC. PAY DETAIL (CONTINUOUSLY REINFORCED STEEL BARS) (MOD)
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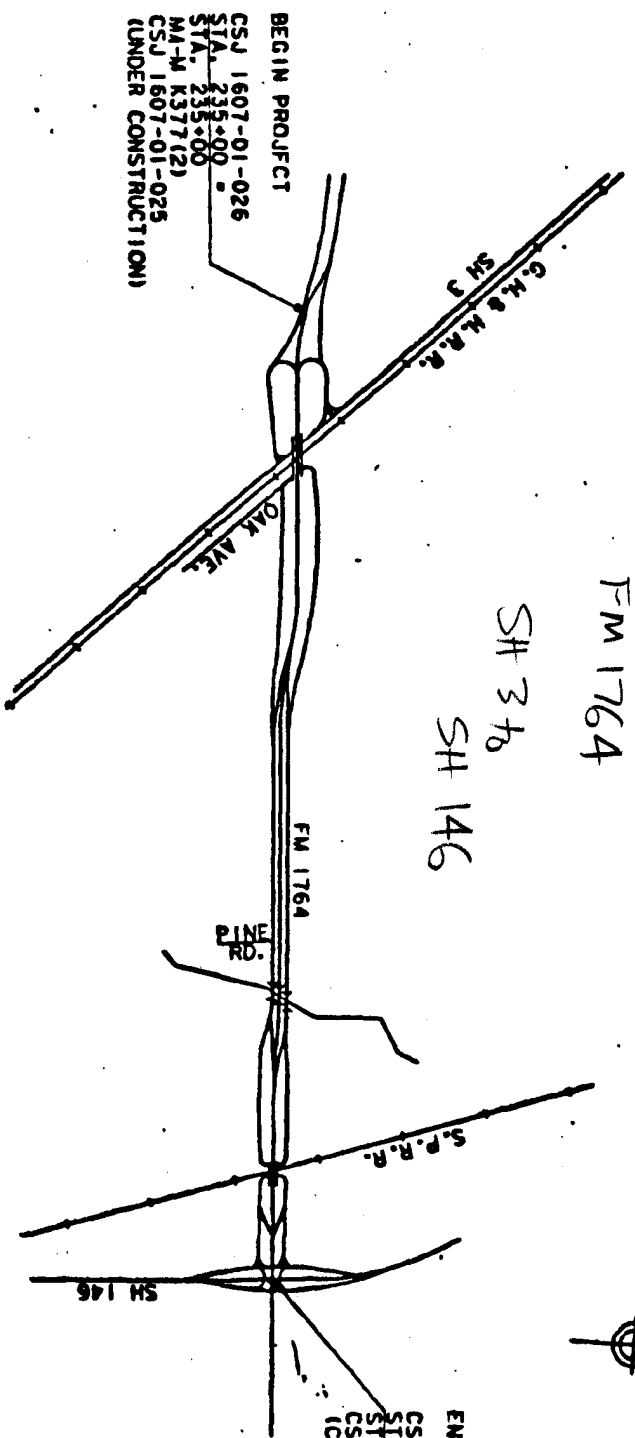
STATE OF TEXAS
FINAL PLANS
STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT

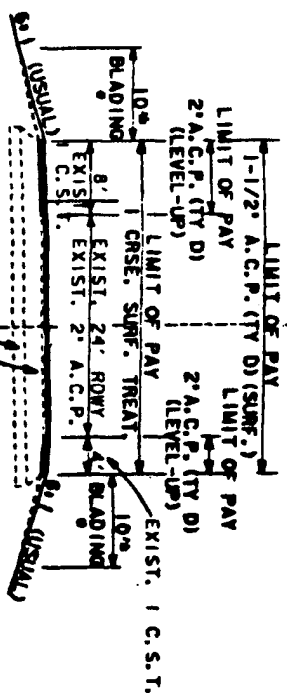
FEDERAL AID PROJECT
NET LENGTH OF PROJECT - 13134.35 FT. - 2.486 MI.
GALVESTON COUNTY
FM 1764

FROM S.H. 3 TO S.H. 146
GRADING, STRUCTURES, CONCRETE PAVEMENT, CEMENT STABILIZED BASE, LIKE TREATED SUBGRADE, ASPHALTIC CONCRETE PAVEMENT, CONCRETE TRAFFIC BARRIER, PAVEMENT MARKINGS, SIGNING AND SAFETY LIGHTING.

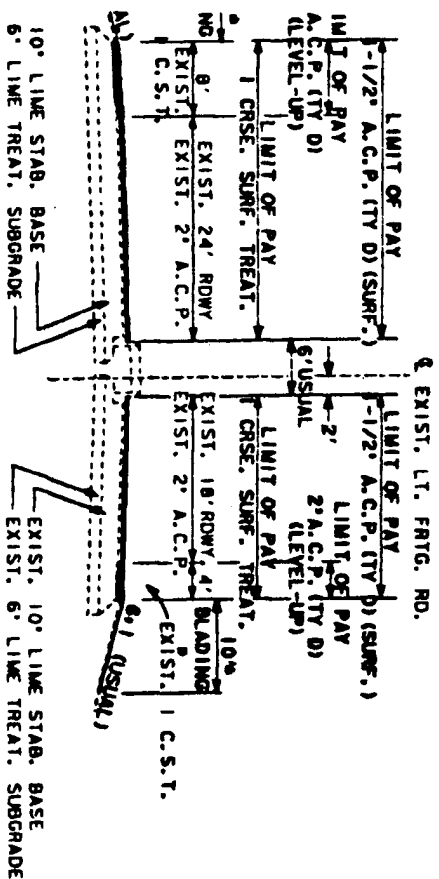
ROADWAY	13008.098 FT. - 2.483 MI.
BRIDGE	125.252 FT. - 0.023 MI.
TOTAL	13134.350 FT. - 2.486 MI.



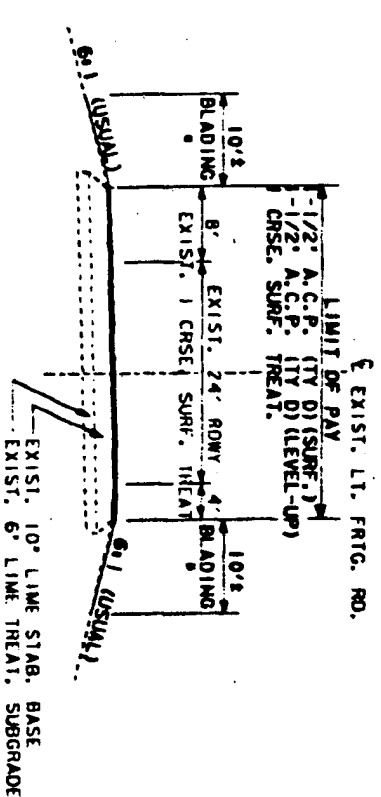
EXIST. LT. FRIG. RD.



PROP. OVERLAY OF
EXIST. LT. FRONTAGE RD.
STA. 235+52.04 TO STA. 241+17

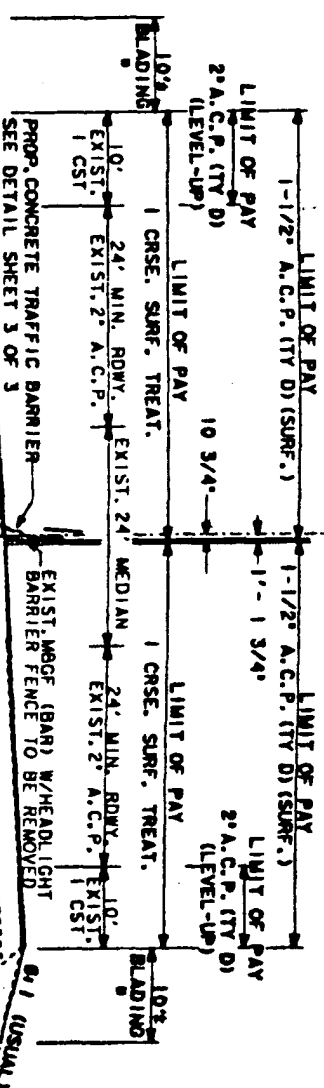


PROP. OVERLAY OF
EXIST. LT. FRONTAGE RD.
STA. 241+17 TO STA. 249+90

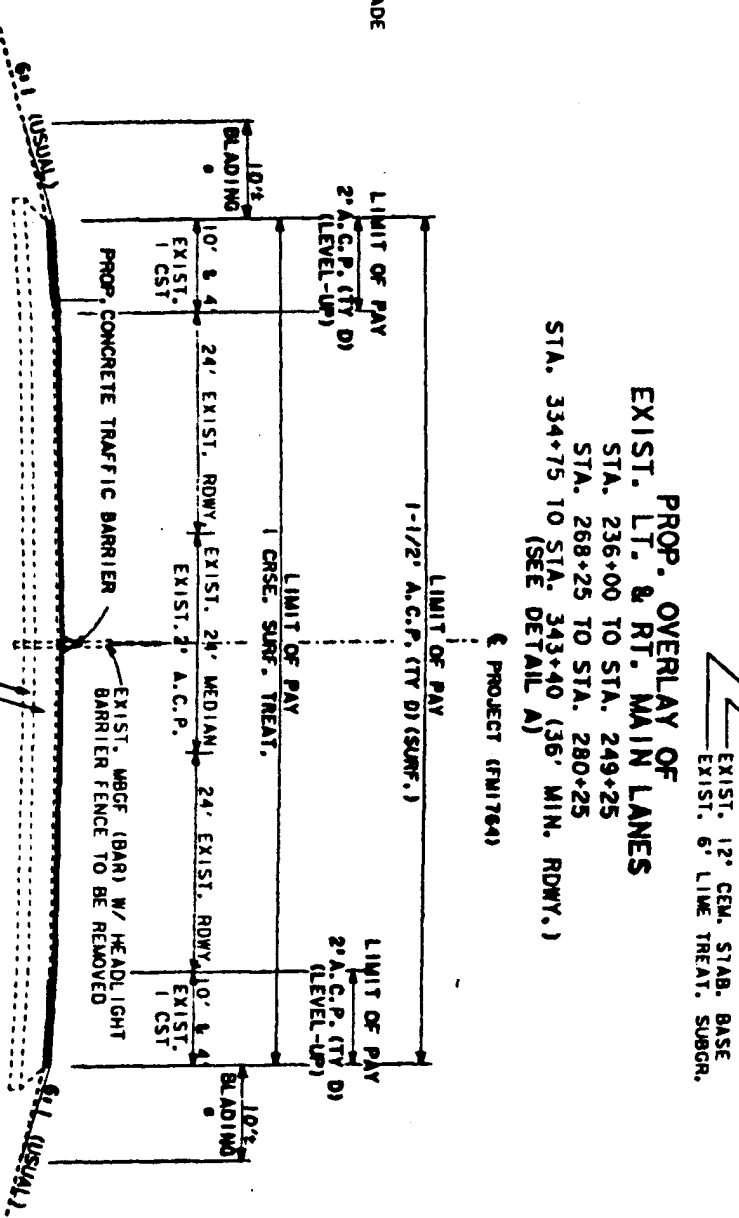


PROP. OVERLAY OF
EXIST. LT. FRONTAGE RD.
STA. 251+45 TO STA. 285+00
STA. 333+00 TO STA. 348+00

PROJECT (FM1764)

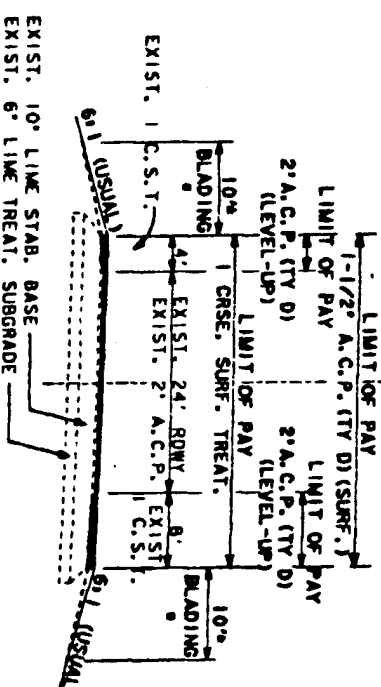


PROP. OVERLAY OF
EXIST. LT. & RT. MAIN LANES
STA. 236+00 TO STA. 249+25
STA. 268+25 TO STA. 280+25
STA. 334+75 TO STA. 343+40 (36' MIN. ROWY.)
(SEE DETAIL A)

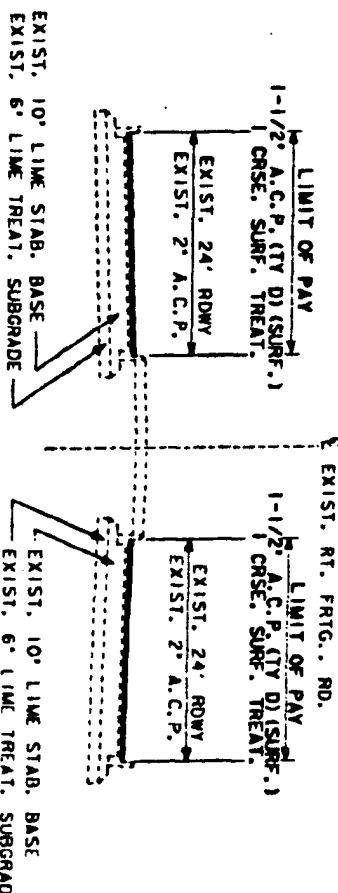


PROP. OVERLAY OF
EXIST. LT. & RT. MAIN LANES
STA. 249+25 TO STA. 252+08
STA. 258+19 TO STA. 268+25
STA. 280+25 TO STA. 282+90
STA. 343+40 TO STA. 349+13 (36' MIN. ROWY.)
STA. 354+03 TO STA. 360+42.71 (36' MIN. ROWY.)

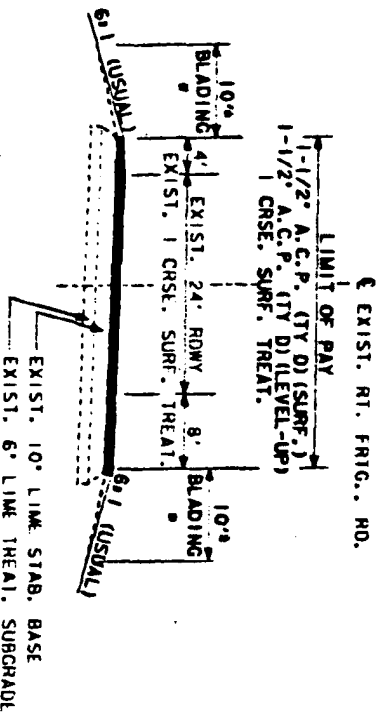
EXIST. RT. FRIG. RD.



PROP. OVERLAY OF
EXIST. RT. FRONTAGE RD.
STA. 235+65 TO STA. 241+37
TYPICAL SECTION TRANSITIONS TO CURB & GUTTER AT STA. 241+37



PROP. OVERLAY OF
EXIST. RT. FRONTAGE RD.
STA. 242+60 TO STA. 257+33
RT LANE STA. BEGINS AT STA. 241+37



PROP. OVERLAY OF
EXIST. RT. FRONTAGE RD.
STA. 259+00 TO STA. 287+50

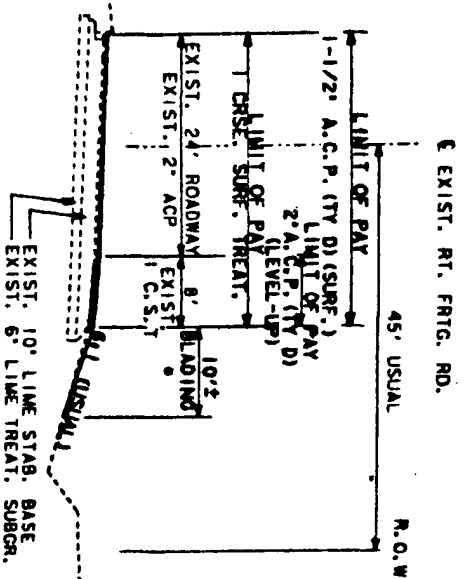
TYPICAL SECTIONS

*LIMIT OF PAY FOR
BROADCAST SEEDING

SCALE: 1" = 10' HOR.
1" = 5' VERT.

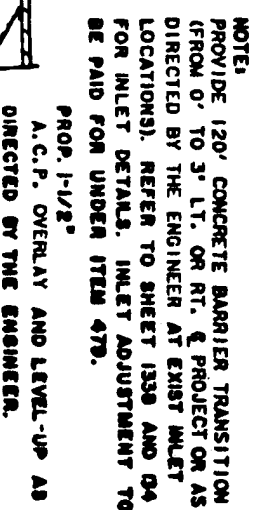
SHEET 1 OF 3 SHEETS

NO.	DATE	BY	CHKD.	APP'D.
1	10/1/14	J. L. HARRIS	J. L. HARRIS	J. L. HARRIS
2	10/1/14	J. L. HARRIS	J. L. HARRIS	J. L. HARRIS
3	10/1/14	J. L. HARRIS	J. L. HARRIS	J. L. HARRIS

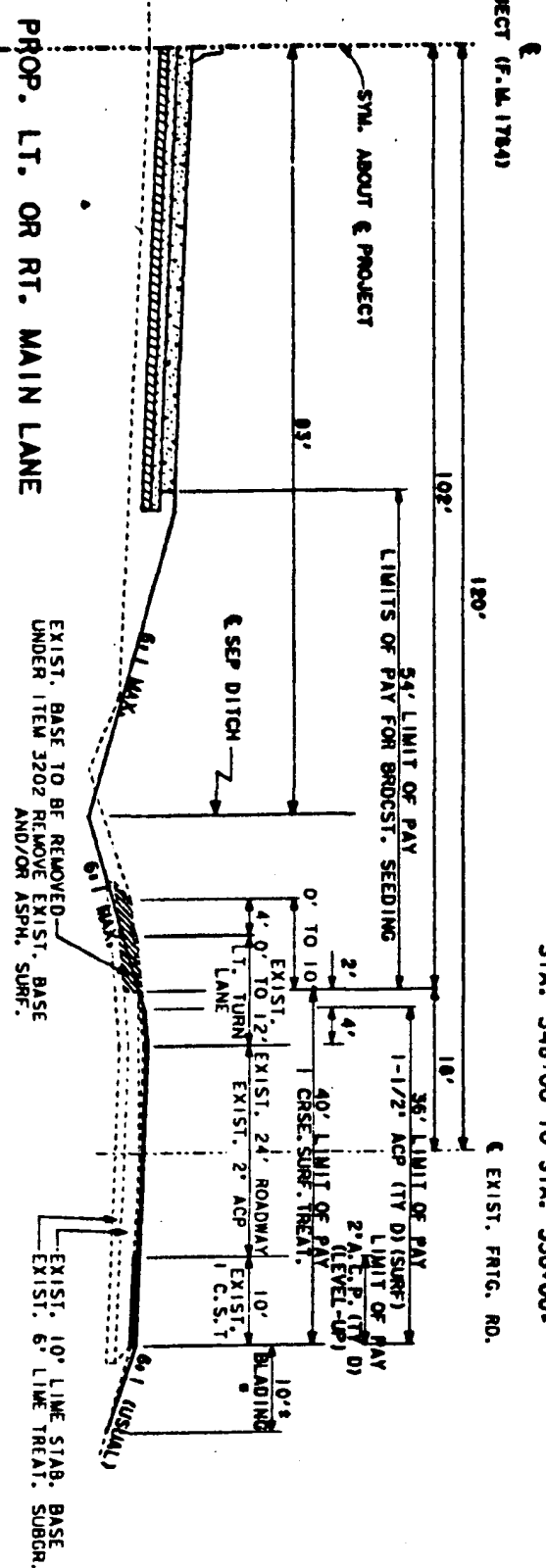


**PROP. OVERLAY OF
EXIST. LT. & RT. MAIN LANES
STA. 360+42.71 TO STA. 363+66**

PROP. OVERLAY OF
EXIST. RT. FRONTAGE ROAD
STA. 348+00 TO STA. 350+00±

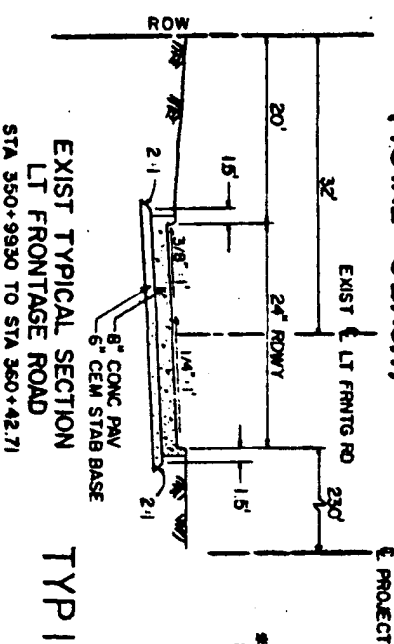
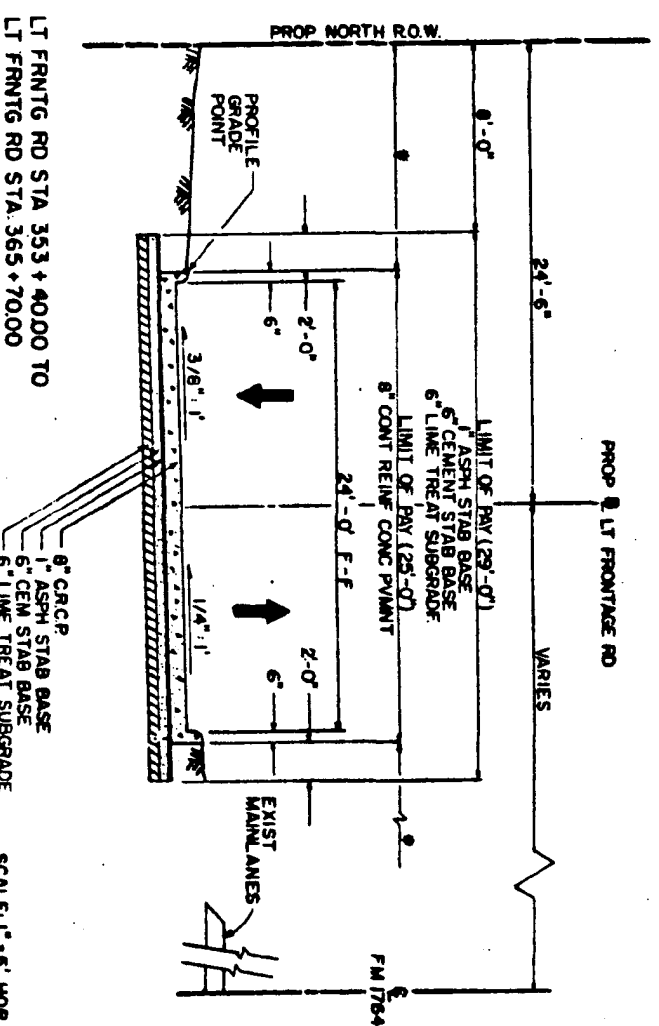


A.C.P. OVERLAY AND LEVEL-UP AS
DIRECTED BY THE ENGINEER.



**PROP. OVERLAY OF
EXIST. LT. OR RT. FRONTAGE ROAD.**

**TYPICAL SECTION AT
EXISTING LEFT TURN LANES
(TOTAL - 6 EACH)**

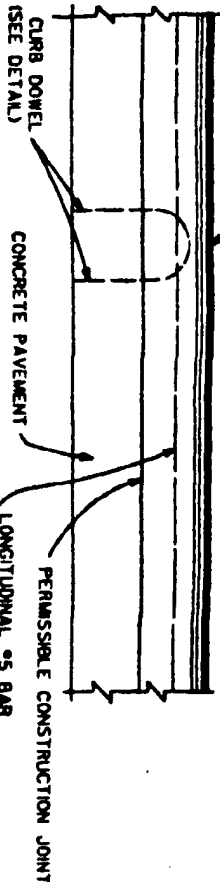


TYPICAL SECTIONS

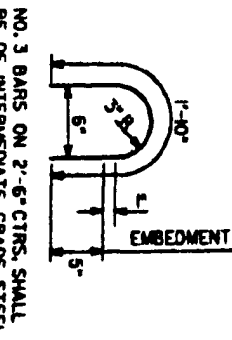
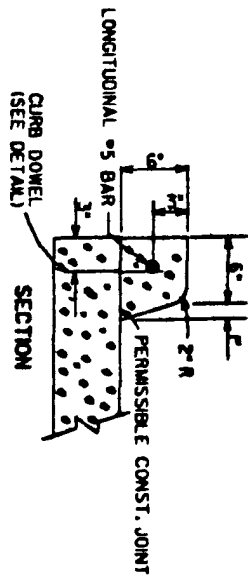
SHEET 3 OF 3 SHEETS

TD	STAFF	PLANT AND PRODUCT NO.	INSTR.
SILK #	6 TEXAS	M-M K37(13)	NO.
%	COUNTRY	DATE RECEIVED =	DATE
12	GALVESTON	6/5/01/96	5

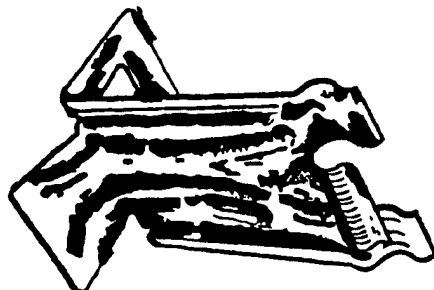
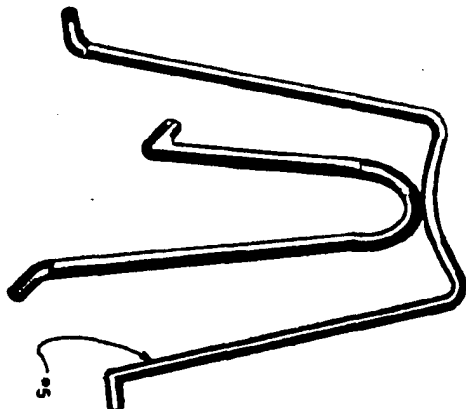
TOP AND FACE OF CURB TO RECEIVE STEEL TROWEL FINISH



ELEVATION

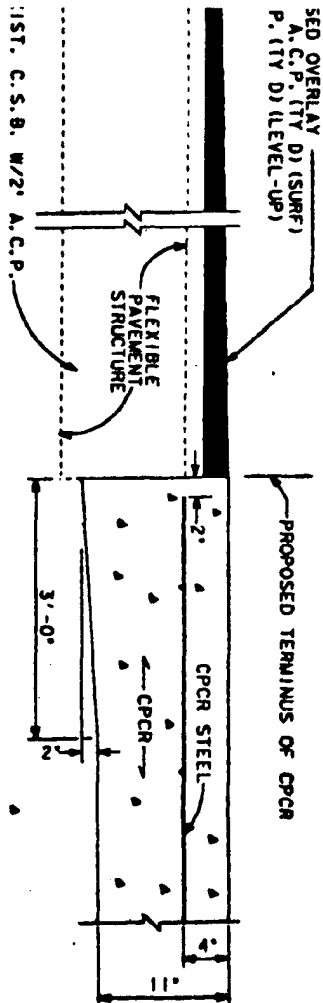


TYPICAL 6" CURB (MONOLITHIC)



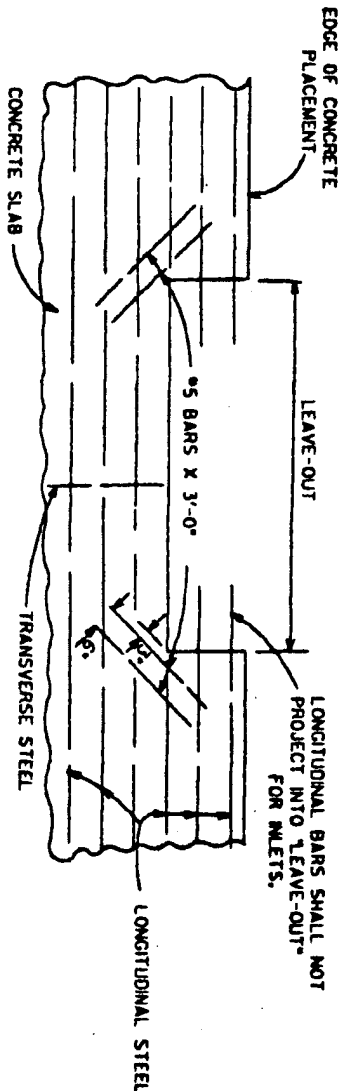
BAR MAT CHAIRS OF THESE TYPES OR EQUAL, AS APPROVED BY THE ENGINEER, MAY BE USED TO SUPPORT REINFORCING STEEL TO DIMENSIONS SHOWN. CHAIR SPACING SHALL NOT EXCEED 48" C-C (TRANSVERSE) AND 48" (LONGITUDINAL). GALVANIZING OF CHAIRS WILL NOT BE REQUIRED.

SUGGESTED CHAIR DETAILS

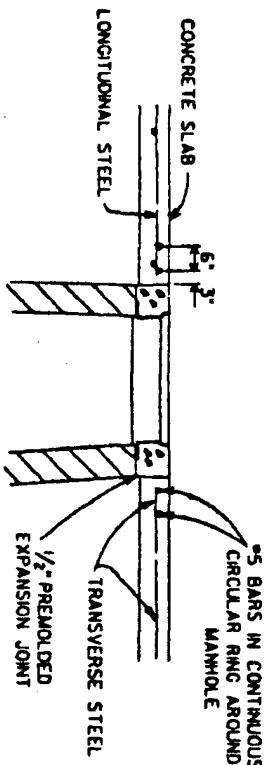


DETAIL FOR JUNCTURE WITH FLEXIBLE TYPE PAVEMENT STRUCTURE

NOTE: ADDITIONAL CONCRETE FOR THICKENED EDGE SHALL BE CONSIDERED INCIDENTAL TO VARIOUS BID ITEM.



DETAIL OF LEAVE-OUT
SEE GENERAL NOTES IN REFERENCE TO 'LEAVE-OUTS'



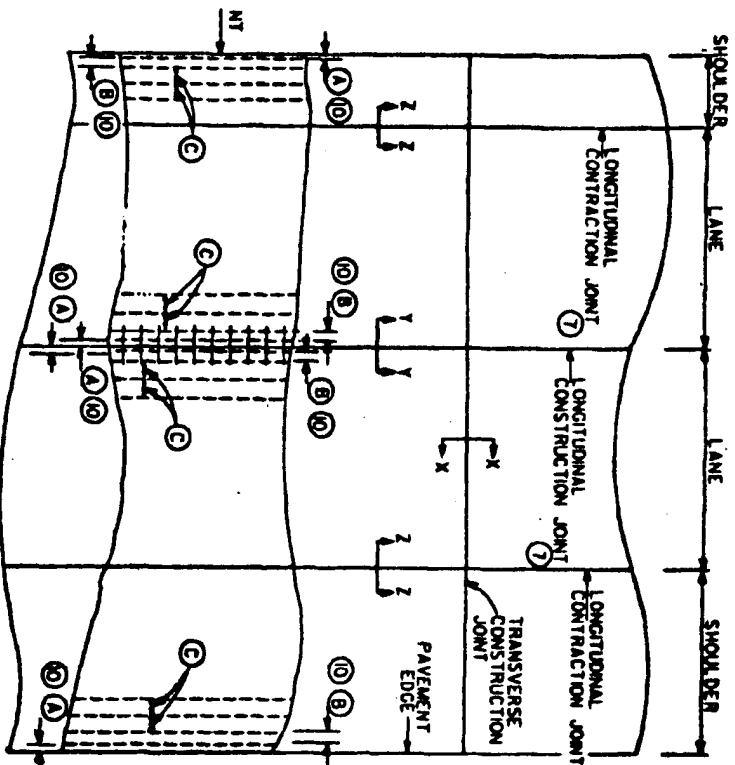
SECTION THRU MANHOLE
SHOWING ADDED REINFORCING STEEL IN SLAB

REVISED 12/23/86, ADD DETAIL FOR JUNCTURE WITH FLEXIBLE TYPE PAVEMENT STRUCTURE.

STATE DEPARTMENT OF HIGHWAYS
AND PUBLIC TRANSPORTATION
DISTRICT 12
CONCRETE PAVEMENT DETAILS
CONTINUOUSLY REINFORCED
STEEL BARS
(MOD)

NO.	DATE	REVISION	BY	CHKD.
1	11/85	MAJOR REVISION	JUL 86	
2	12/86	MOD		
3	12/86	MOD		

124



SPACING C		NUMBER OF BARS REQUIRED FOR VARIOUS TYPICAL PLACEMENT WIDTHS (F.T.)									
		(10)									
		(IN.)	12	16	22	24	27	34	38		
6	24	32	44	48	54	68	76				
7	21	27	37	41	46	58	65				
8	18	24	33	36	41	51	57				
9	16	22	30	32	36	46	51				

① X	T (IN.)	LONGITUDINAL BAR SIZE	SPACING C (IN.)	TRANS. BAR SIZE	MAXIMUM ALLOWABLE ② PAYMENT WIDTH (FT.) FOR GIVEN TRANSVERSE STEEL SPACINGS (FT.)			B W ③ S (FT.-FT.)
					1'-	2'-	3'-	
0.6	8	6	9	4 5 6	120 106 264	60 93 132	40 62 88	120.0 186.0 264.0
	9	6	8	4 5 6	106 95 234	53 82 117	35 55 78	106.7 165.3 234.7
	10	6	7	4 5 6	96 148 21	48 74 105	32 49 70	96.0 148.0 21.2
	11	6 4 ②	7 6	4 5 6	87 135 192	43 67 96	29 45 64	87.3 135.3 192.0
	12	6 5 ②	6 9	4 5 6	80 124 176	40 62 88	26 41 58	80.0 124.0 176.0
0.5	13	5 ①	8	4 5 6	73 114 162	36 57 81	24 38 54	73.8 114.5 162.5
	14	5 ①	7.5	4 5 6	68 106 150	34 53 75	22 35 50	68.6 106.3 150.9
	15	5 ①	7	4 5 6	64 99 140	32 49 70	21 33 46	64.0 99.2 140.8

- ## GENERAL NOTES

1. NO EXPANSION JOINTS WILL BE USED EXCEPT AT STRUCTURE ENDS OR FIXED OBJECTS AS SHOWN ELSEWHERE IN THE PLANS.
2. FOR FURTHER INFORMATION REGARDING THE PLACEMENT OF CONCRETE AND REINFORCEMENT REFER TO THE GOVERNING SPECIFICATIONS FOR "CONCRETE PAVEMENTS."
3. DETAILS AS TO PAVEMENT WIDTH, PAVEMENT THICKNESS AND THE CROWN CROSS-SLOPE SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
4. WITHIN ANY AREA BOUNDED BY TWO FEET OF PAVEMENT LENGTH MEASURED PARALLEL TO THE CENTERLINE AND TWELVE FEET OF PAVEMENT WIDTH MEASURED PERPENDICULAR TO THE PAVEMENT CENTERLINE, NOT OVER 33% OF THE REGULAR LONGITUDINAL STEEL SHALL BE SPLICED.
5. THE LONGITUDINAL STEEL SHALL BE PLACED AT THE VERTICAL SLAB CENTER WITH A TOLERANCE OF 1/2 INCH. TRANSVERSE STEEL SHALL BE PLACED DIRECTLY ABOVE OR BELOW THE LONGITUDINAL STEEL.
6. SPLICES SHALL BE A MINIMUM OF 33 TIMES THE NOMINAL STEEL DIAMETER (D).
7. MULTIPLE PIECE THE BARS SHALL BE REQUIRED AT THE LONGITUDINAL CONSTRUCTION JOINTS. THE MULTIPLE PIECE THE BAR ASSEMBLIES SHALL HAVE STOP TYPE COUPLINGS AND SHALL HAVE ROLLED THREADS ON THE BARS. THE MULTIPLE PIECE THE BAR ASSEMBLIES SHALL DEVELOP A MINIMUM ULTIMATE TENSILE STRENGTH EQUAL TO 1-1/2 TIMES THE YIELD STRENGTH OF THE TRANSVERSE BARS BEING JOINED. THE BARS SHALL BE DEFORMED REINFORCING BARS. THE BAR ASSEMBLIES MADE FROM STEELS OTHER THAN ASTM GRADE 60 AND WITH DEFORMATIONS OTHER THAN ASTM STD. MAY BE USED PROVIDED IT CAN BE PROVEN TO THE SATISFACTION OF THE ENGINEER THAT THEY ARE IN EVERY RESPECT THE EQUAL OF THE ASSEMBLIES SPECIFIED. LABORATORY TESTING OF THE PROPOSED ASSEMBLIES, AT THE CONTRACTOR'S EXPENSE, MAY BE REQUIRED.
8. AT TRANSVERSE CONSTRUCTION JOINTS THE REGULAR LONGITUDINAL STEEL SHALL EXTEND A MINIMUM OF FOUR FEET ON EITHER SIDE OF THE JOINT.
9. VIBRATION WITH HAND-MANIPULATED MECHANICAL VIBRATORS WILL BE REQUIRED ADJACENT TO ALL TRANSVERSE CONSTRUCTION JOINTS.
10. THE CHAIRS USED TO SUPPORT THE STEEL SHALL BE OF SUFFICIENT STRUCTURAL QUALITY AND NUMBER TO HOLD THE STEEL MAT WITHIN THE PLACEMENT HEIGHT TOLERANCES. CHAIRS SHALL BE OF A TYPE APPROVED BY THE ENGINEER.
11. JOINTS, GROOVE AND SEAL DETAILS SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
12. LONGITUDINAL AND TRANSVERSE STEEL SPACING SHALL NOT VARY MORE THAN ONE-TWELFTH OF THE SPACING SHOWN HEREON.
13. IF WIDTHS OCCUR, OTHER THAN THE TYPICAL WIDTHS SHOWN, INDIVIDUAL BARS (WIRES) OF THE SIZE SPECIFIED HEREON MAY BE ADDED OR REMOVED TO OBTAIN THE APPROPRIATE WIDTH. SPACING REQUIREMENTS SHALL NOT BE EXCEEDED, HOWEVER.
14. LONGITUDINAL JOINTS ARE SHOWN OFFSET FOUR INCHES FROM THE THEORETICAL LANE LINE AND MAY BE OFFSET TO EITHER SIDE IF THE SPACING OF REINFORCING STEEL IS PROPERLY ADJUSTED.
15. SEE STANDARD
- 15-15 CONCRETE PAVING DETAILS, JOINT SEALS, FOR JOINT SEALING DETAILS NOT SHOWN HEREIN.



OPTIONAL STEEL PLACEMENT ②

CONCRETE PAVEMENT DETAILS CONTINUOUSLY REINFORCED STEEL BARS

CRCP (B) - 85

[illegible]

