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342	DMT

THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE, PLUS SHEETS CPCD-80(1) (MOD), JS-75 (MOD), MC-5 2 (MOD), SC-NH (MOD) AND GDA 2, Gp B 2, Gp NS-1R, RAIL 1502, RRB & RRR, BPA, PEB-MH (1), PEP AND PMO (1) (INCLUDED WITH BRIDGE SHEETS), HAVE BEEN ISSUED BY ME AND ARE APPLICABLE TO THIS PROJECT.

WILLIAM C. LOVIL, JR. P.E. 3/8/90
DATE



SPECIFICATIONS ADOPTED BY THE STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION SEPTEMBER 1, 1982 AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS, SHALL GOVERN ON THIS PROJECT. REQUIRED CONTRACT PROVISIONS FEDERAL AID CONSTRUCTION CONTRACTS FORM FHWA 1278 AUGUST, 1989.

STATE OF TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

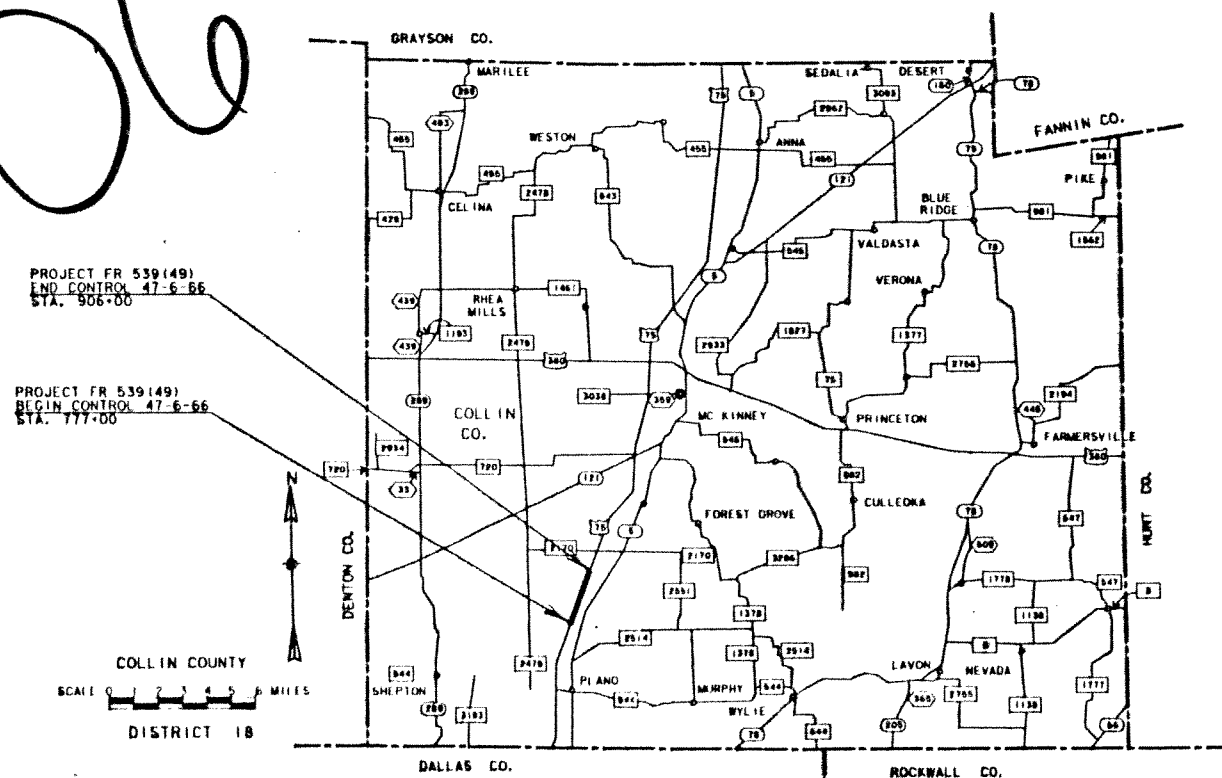
PLANS OF PROPOSED STATE HIGHWAY IMPROVEMENT FEDERAL PROJECT NO. FR 539(49) U.S. 75 COLLIN COUNTY

FROM: NORTH OF SPRING CREEK PARKWAY, NORTH
TO: NORTH OF BETHANY ROAD

NET LENGTH OF PROJECT = 12,900 FT. = 2.442 MI.

ROADWAY = 12.30131 Fr = 2.329 MI.
BRIDGES = 598.695 = 0.113 MI.

UPGRADING OF A FREEWAY FACILITY CONSISTING OF:
GRADING, STRUCTURES, STORM SEWERS, LIME TREATED
SUBGRADE, ASPHALT STABILIZED BASE, CONCRETE PAVEMENT,
PAVEMENT MARKINGS, SIGNING AND SAFETY ILLUMINATION.



CITY OF PLANO
APPROVED: [Signature] DATE 3/8/90
TITLE: City Manager

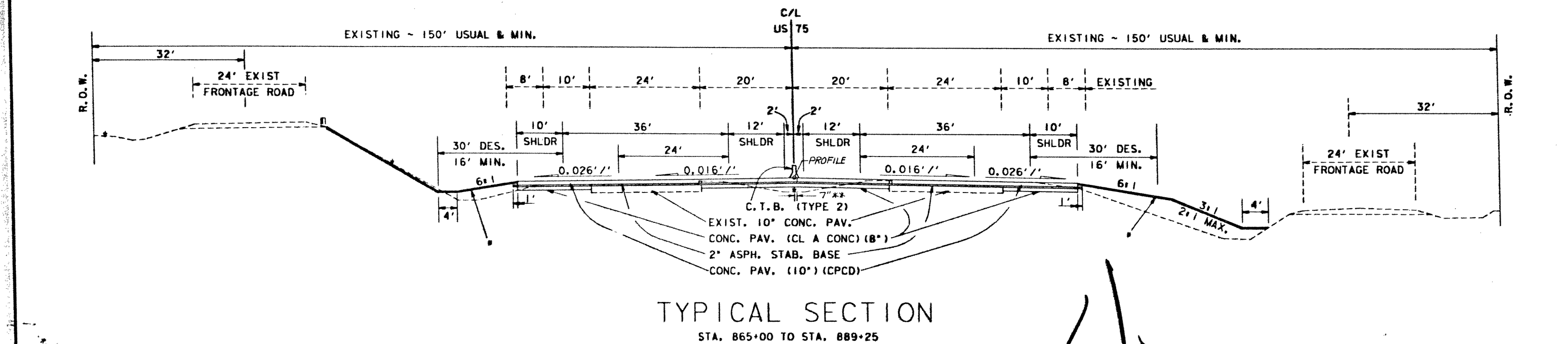
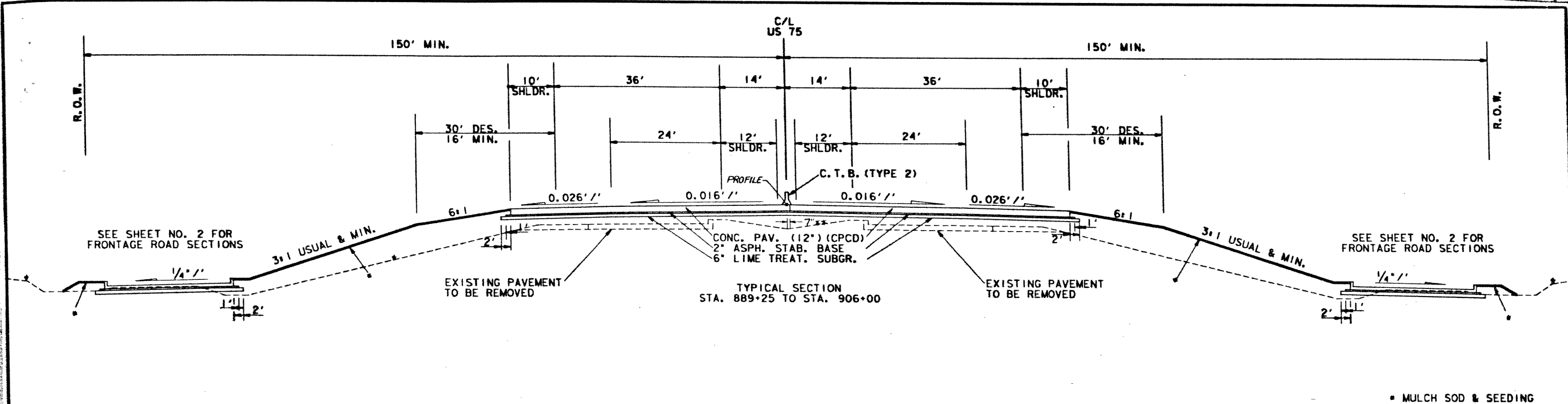
CITY OF ALLEN
APPROVED: [Signature] DATE 3/9/90
TITLE: City Manager

STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

RECOMMENDED FOR LETTING: 3-12-90
FOR DISTRICT DESIGN ENGINEER: [Signature]
RECOMMENDED FOR LETTING: 3/8-90
FOR SUPERVISING RESIDENT ENGINEER: [Signature]
RECOMMENDED FOR LETTING: 5-1-90
FOR DISTRICT ENGINEER: [Signature]

APPROVED FOR LETTING: [Signature]
BRIDGE ENGINEER: 5-1-90
APPROVED FOR LETTING: [Signature]
FOR CHIEF ENGINEER, HIGHWAY DESIGN: [Signature]

Rev 5/29/90
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
APPROVED: [Signature]
DIVISION ADMINISTRATOR: [Signature]
DATE: [Signature]

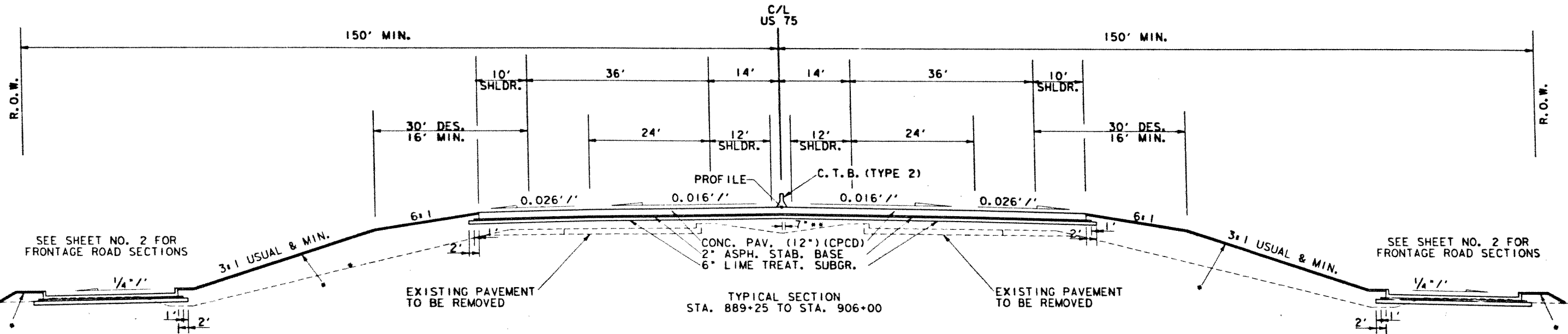


TYPICAL SECTION
STA. 865+00 TO STA. 889+25

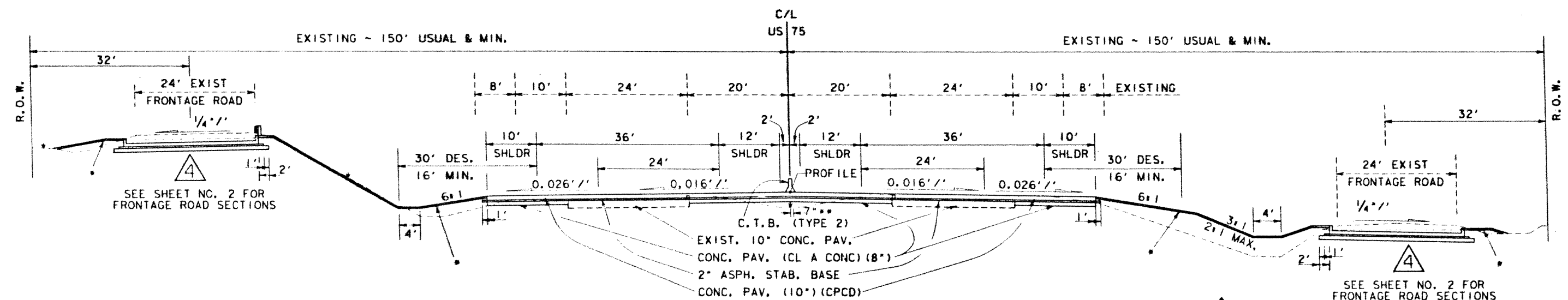
4



TYPICAL
SECTIONS



MULCH SOD & SEEDING



TYPICAL SECTION
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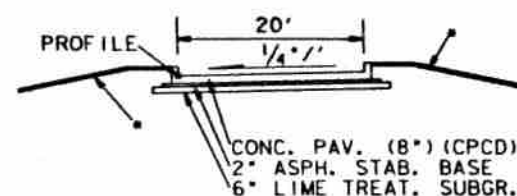
4A

FIELD CHANGE NO. 4

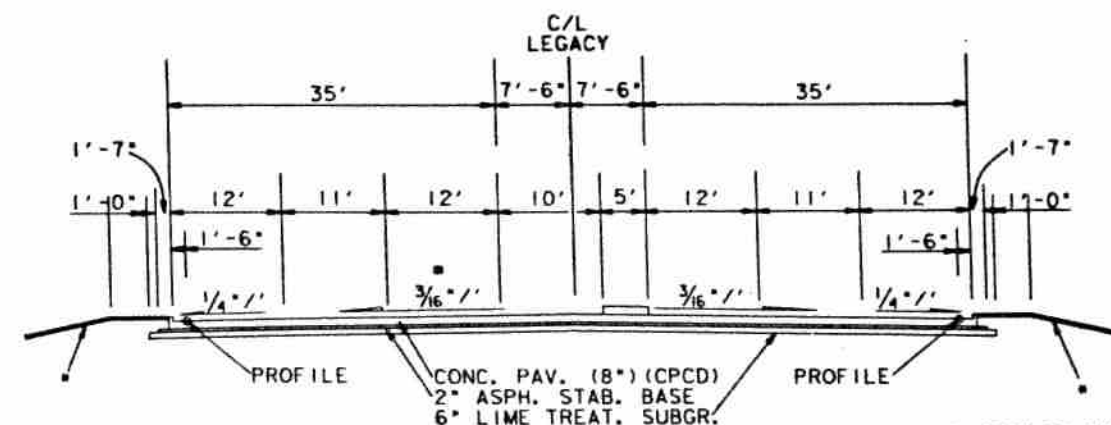
STATE OF TEXAS
BOWLEY W. BATH
41667
REGISTERED PROFESSIONAL ENGINEER
TYPICAL SECTIONS

1/2" PREFORMED EXP. JT. MATL. & JT. SEALER

DATE	STATE	PROJECT NO.	US75
6/1/91	TX	FR 539(49)	
BY	DATE	BY	DATE
COLLIN	47	6	60 4A

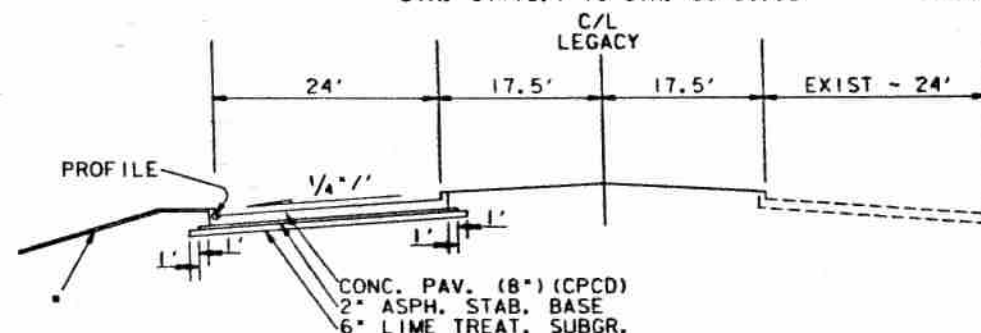
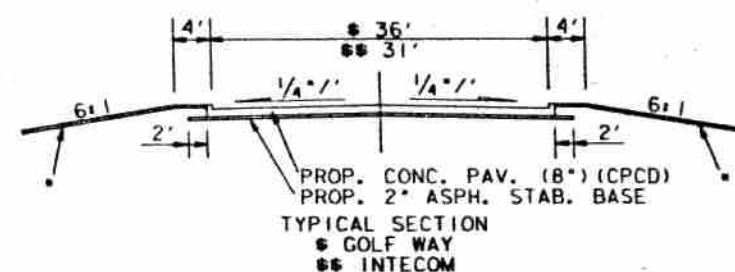


TYPICAL SECTION CONNECTION ROAD

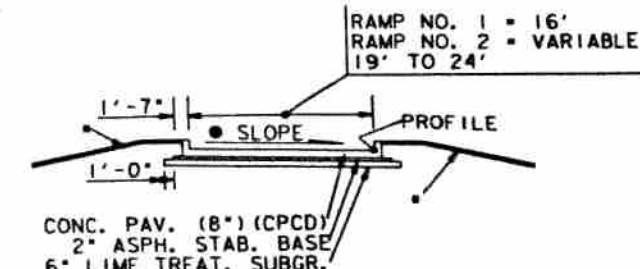


TYPICAL SECTION
STA. 57+79.7 TO STA. 60+90.93

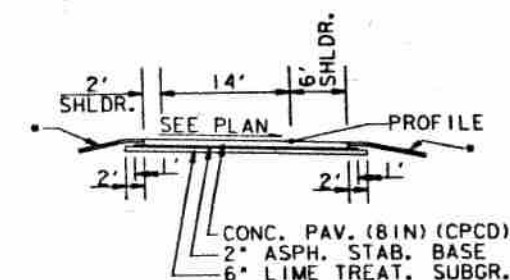
CROSS SECTION LT. STA. 58+00 = 1/4" SLOPE
TRANSITION FROM 1/4" TO TYPICAL SECTION
FROM STA. 58+00 TO 59+00.



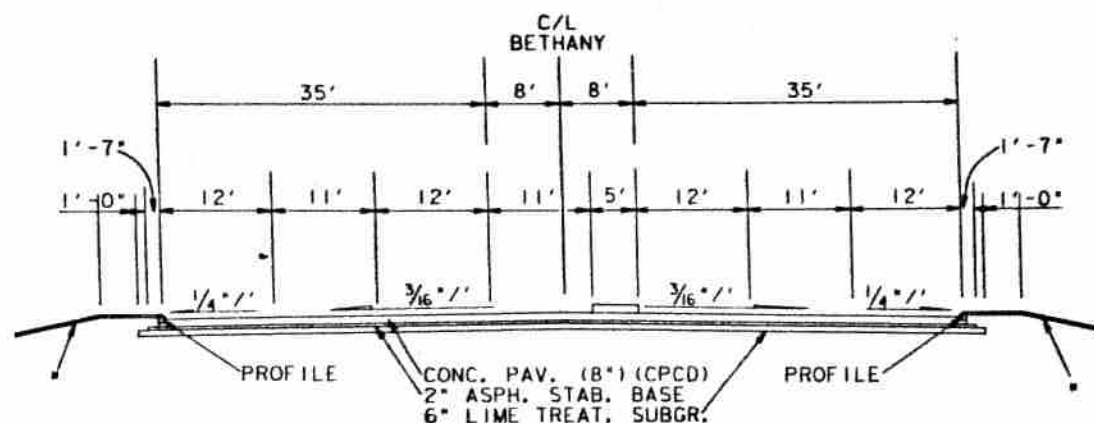
TYPICAL SECTION LEGACY DRIVE
STA. 56+01 TO 57+79.7



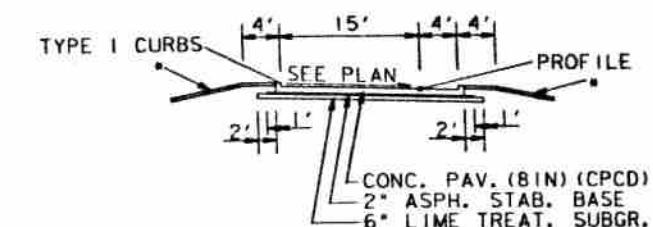
RAMP NO. 1 = 1/4" SLOPE
RAMP NO. 2 = 3/4" SLOPE
TYPICAL SECTION LEGACY RAMP



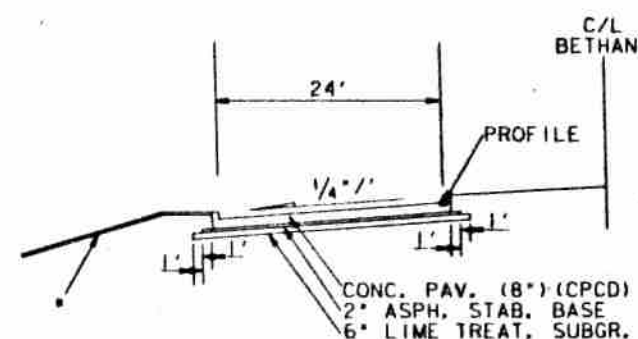
TYPICAL RAMP SECTION
WITH SHOULDERS



TYPICAL SECTION
BETHANY ROAD



TYPICAL RAMP SECTION
WITH CURBS



TYPICAL SECTION TURNAROUND

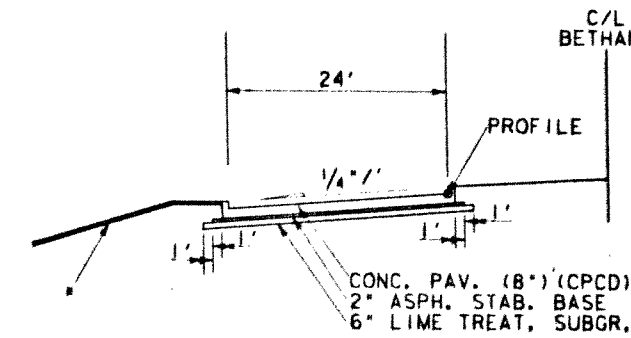
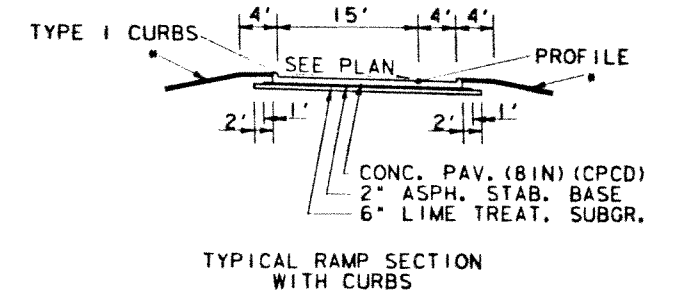
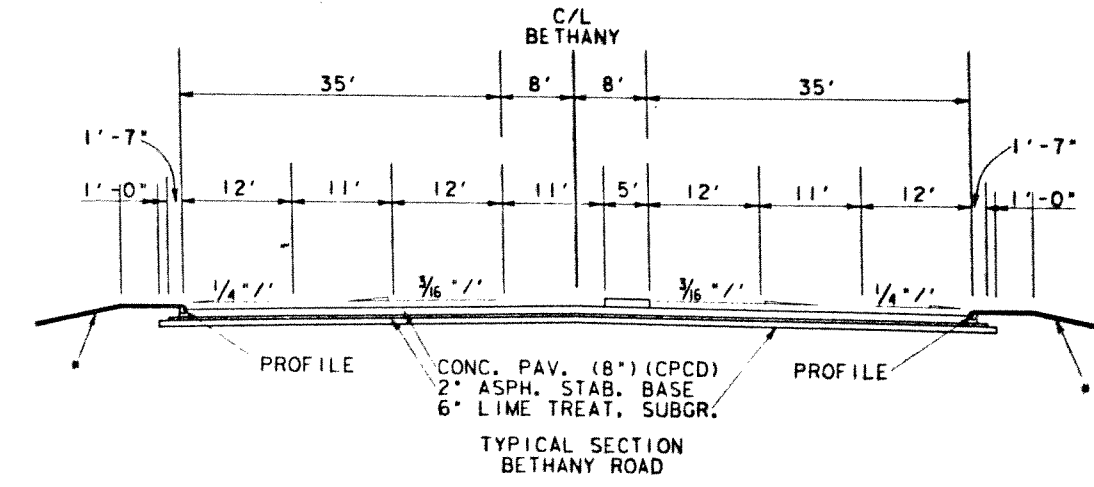
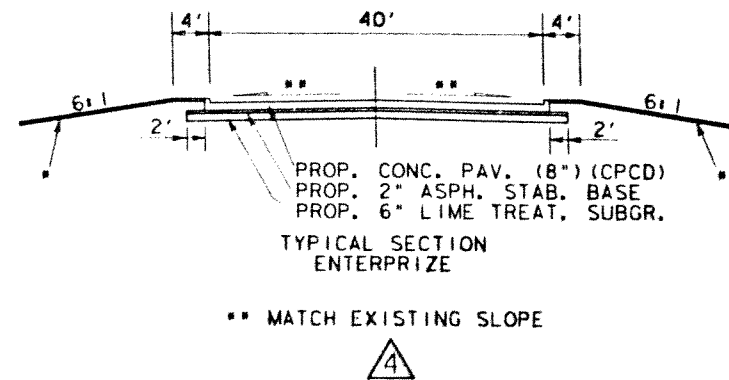
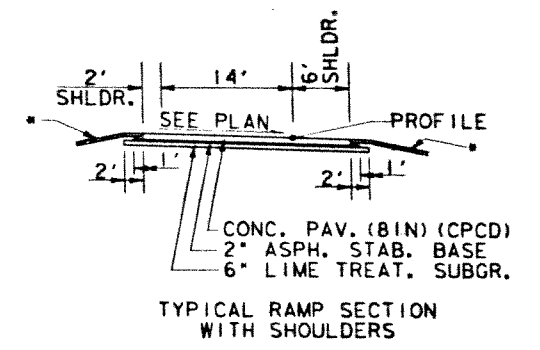
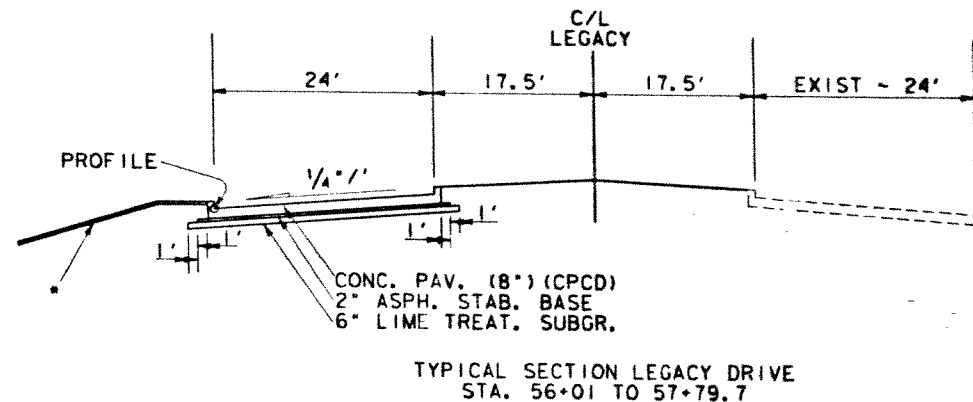
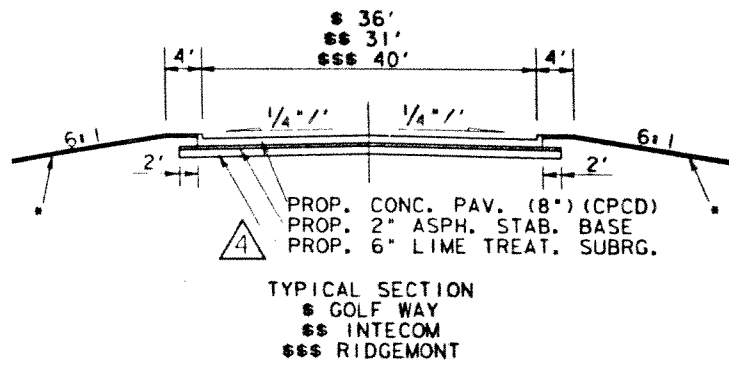
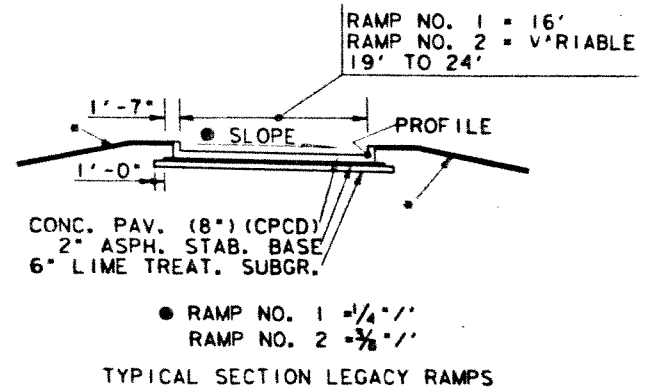
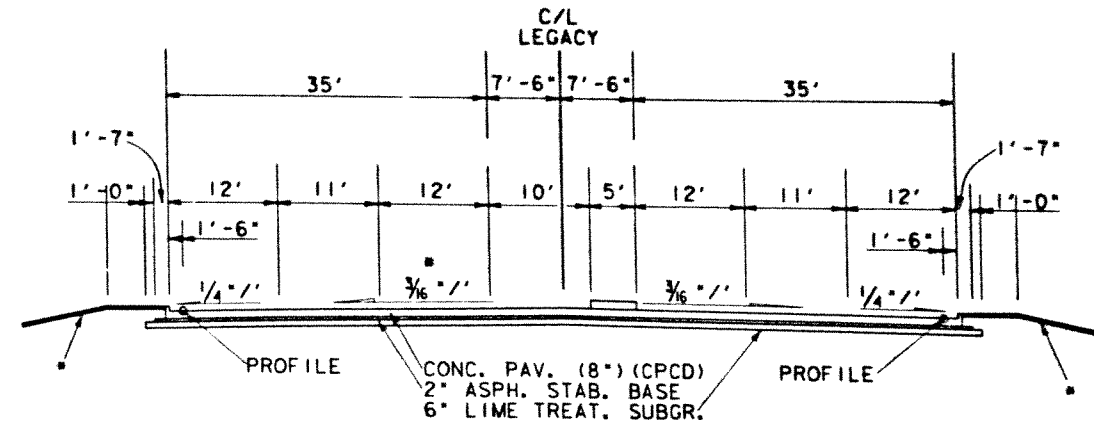
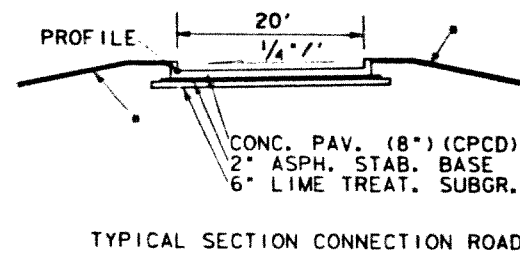
4" MULCH SOD & SEED.



4/26/90

TYPICAL
SECTIONS

STATE	FEDERAL AID PROJECT NO.	
TXAS	FR 539(49)	US75
COUNTY		
COLLIN	47	6 66 5



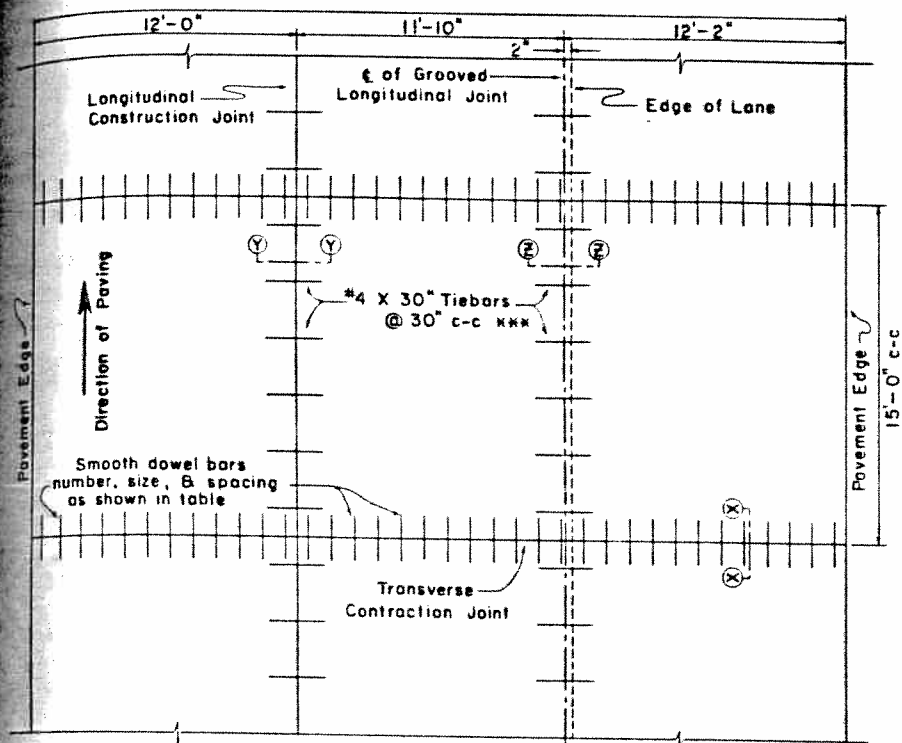
SA



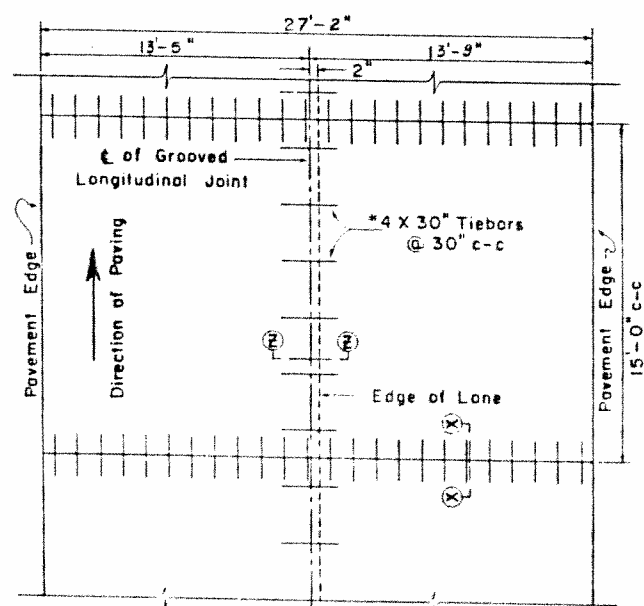
TYPICAL
SECTIONS

FIELD CHANGE NO. 4

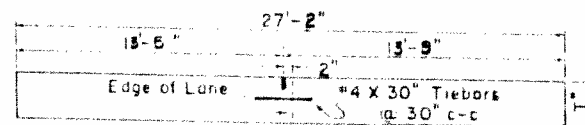
DATE	11/20/93	PROJECT NO.	FR 539 (49)	US75
BY	COLLIN	DATE	47	6
REVISION		DATE	6	5A



THREE LANE PAVEMENT PLAN
(12 ft. & 24 ft. Placement) **

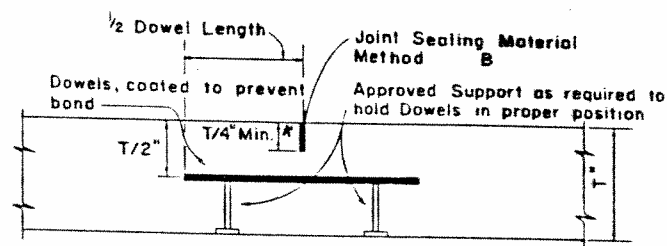


TWO LANE PAVEMENT PLAN

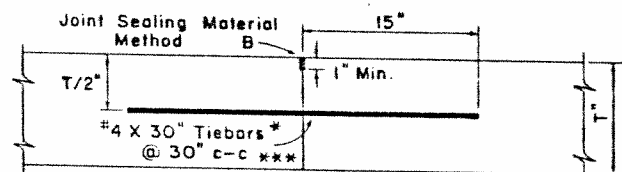


TYPICAL SECTION
(27'-2" Placement) **

*** At locations where the pavement width is greater than 40 feet but less than 60 feet the tie bar spacing shall be 17" center to center. At locations where pavement width is 60 feet and greater #5 tie bars 36" long shall be 15" center to center. WHERE PAVEMENT WIDTH IS LESS THAN 40 FEET, THE TIE BAR SPACING SHALL BE 25" CENTER TO CENTER.

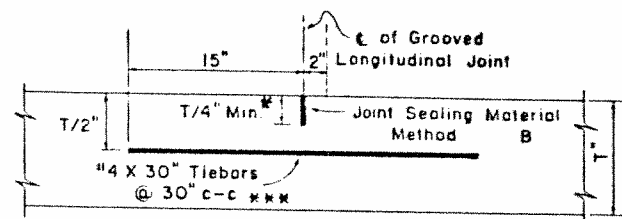


TRANSVERSE CONTRACTION JOINT
Section X-X
* T/2 FOR SILICONE GRAVEL



LONGITUDINAL CONSTRUCTION JOINT
Section Y-Y

* WITH THE APPROVAL OF THE ENGINEER, MULTIPLE PIECE TIEBARS (THREADED COUPLING OR OTHER ADEQUATE DEVICE) MAY BE USED TO FACILITATE CONSTRUCTION PROVIDED THE SYSTEM DEVELOPS A FORCE EQUAL TO 1 1/2 TIMES THE MINIMUM FORCE OF THE TIEBAR SHOWN. THE SPACINGS FOR THE SYSTEM SHALL BE LESS THAN OR EQUAL TO THE SPACING ALLOWED FOR BARS OF SIMILAR YIELD STRENGTH.



GROOVED LONGITUDINAL JOINT
Section Z-Z
* T/2 FOR SILICONE GRAVEL

** Lane widths are for illustrative purposes only and should not be used if in conflict with typical cross sections shown elsewhere in the plans.

GENERAL NOTES

- NO EXPANSION JOINTS WILL BE USED EXCEPT AT STRUCTURE ENDS OR FIXED OBJECTS AS SHOWN ELSEWHERE IN THE PLANS.
- FOR FURTHER INFORMATION REGARDING THE PLACEMENT OF CONCRETE AND LOAD TRANSFER DEVICES REFER TO THE GOVERNING SPECIFICATIONS FOR "CONCRETE PAVEMENT".
- DETAILS AS TO PAVEMENT WIDTH, PAVEMENT THICKNESS, AND THE CROWN CROSS-SLOPE SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
- JOINT GROOVE AND SEAL DETAILS SHALL BE AS SHOWN ELSEWHERE IN THE PLAN.
- TIEBARS SHALL BE SECURED PARALLEL TO THE PAVEMENT SURFACE AND PERPENDICULAR TO THE CENTERLINE BY:
 - USE OF BAR CHAIRS
 - ACCURATELY PLACED IN POSITION ON THE SCREEDED CONCRETE BY MEANS OF AN APPROVED TEMPLATE AND FORCED TO THE PROPER POSITION WITH A SUITABLE TOOL; OR
 - BY ANY OTHER MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- DOWEL BARS SHALL BE SECURED PARALLEL TO THE PAVEMENT SURFACE AND CENTERLINE BY A DOWEL BAR CHAIR.
- WHEN WORK IS STOPPED DUE TO BREAKDOWN OR OTHER CAUSE, CONCRETE SHALL BE REMOVED BEYOND LAST CONTRACTION JOINT IN PLACE AND A HEADER INSTALLED.
- WHERE A MONOLITHIC CURB IS SPECIFIED, THE JOINT IN THE CURB SHALL COINCIDE WITH PAVEMENT JOINTS AND MAY BE FORMED BY ANY MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- CONSTRUCTION JOINTS MAY BE FORMED BY USE OF METAL OR WOOD FORMS EQUAL IN DEPTH TO THE NOMINAL DEPTH OF THE PAVEMENT, OR BY OTHER MEANS WHICH HAVE BEEN APPROVED BY THE ENGINEER PRIOR TO THEIR USE.
- LONGITUDINAL AND TRANSVERSE STEEL SPACING SHALL NOT VARY MORE THAN ONE TWELFTH OF THE SPACING SHOWN HEREON.
- THE TIEBAR SPACINGS SHOWN ARE FOR ASTM DESIGNATIONS: A-615, OR A-616, GRADE 60, TIEBARS, WHICH SHALL NOT BE BENT. IF TIEBARS ARE TO BE BENT, THEY SHALL BE STEEL CONFORMING TO ASTM DESIGNATION: A-615, GRADE 40, WITH A CENTER TO CENTER SPACING REDUCED BY ONE THIRD.

DEPTH OF PAVEMENT (INCHES)	DOWELS (SMOOTH BARS)		
	SIZE AND LENGTH	AVERAGE SPACING (INCHES)	WEIGHT PER FOOT OF JOINT (LBS.)
8	1" X 18"	12	4.01
9	1 1/8" X 20"	12	5.63
10	1 1/8" X 22"	12	7.65
11	1 3/8" X 24"	12	10.10
12	1 1/2" X 26"	12	13.02

NOTE: TIEBARS USED IN LONGITUDINAL JOINTS SHALL NOT BE PLACED WITHIN 15" OF TRANSVERSE JOINTS.

242



1/5/94
W. C. S. J. L.



STATE DEPARTMENT OF HIGHWAYS
AND PUBLIC TRANSPORTATION

CONCRETE PAVEMENT DETAILS CONTRACTION DESIGN CPCD - 80 (1) (MOD)

IN.	CHANG.	DATE	REV.	STATE	FEDERAL PROJECT NO.	SHEET NO.
1	ORIGINAL			TEXAS	FR 539(49)	252
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