

STATE OF TEXAS STATE HIGHWAY DEPARTMENT

PLAN OF PROPOSED STATE HIGHWAY IMPROVEMENT

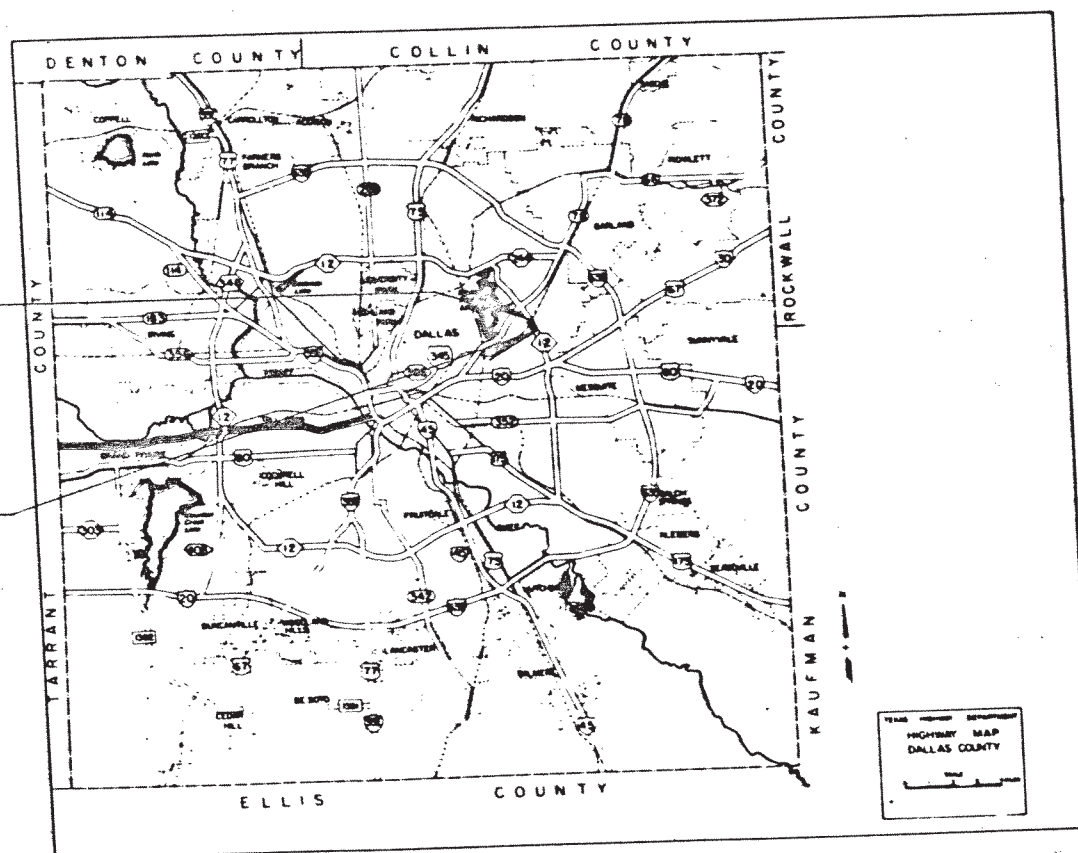
FEDERAL AID PROJECT.

UG-1089(37)

DALLAS COUNTY

LOOP 12
FROM: SOUTH OF G.C. & S.F. RAILROAD
TO: NORTH OF G.C. & S.F. RAILROAD

SCALE: PLAN: 1 IN. = 40 FT.
PROFILE: 1 IN. = 5 FT.
CROSS-SECTION: 1 IN. = 5 FT.
NET LENGTH OF PROJECT = 1200.00 FT. = .227 MI.
TYPE: GRADING, STORM SEWERS, RETAINING WALLS,
RAILROAD UNDERPASS STRUCTURE, AND
CONCRETE PAVEMENT.



STA. 713+00
End Project
Control 581-I-52
Project UG-1089(37)

STA. 701+00
Begin Project
Control 581-I-52
Project UG-1089(37)

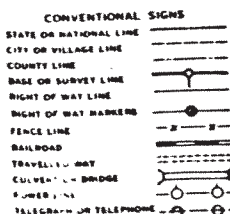
EQUATIONS: NONE
EXCEPTIONS: NONE

SPECIFICATIONS ADOPTED BY THE STATE HIGHWAY DEPARTMENT
OF TEXAS JANUARY 3, 1972 AND SPECIFICATION ITEMS LISTED
AND DATED AS FOLLOWS SHALL GOVERN ON THIS
PROJECT. REQUIRED CONTRACT PROVISIONS ALL
FEDERAL-AID CONSTRUCTION CONTRACTS,
(FORM PR-1273, MAY, 1971).

INDEX OF SHEETS

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ADDITIONAL SHEETS ADDED
35A, 40A, 41A, 42A, 43A, 47A, 50A,
51A, 52A, 53A, 54A, 55A



FINAL PLANS

DATE WORK BEGAN: AUGUST 21, 1972
DATE WORK COMPLETED: APRIL 26, 1974

FIELD CHANGE NO. 1
CHANGE ELEVATION TOP ABUTMENTS AND PIERS
FIELD CHANGE NO. 2 & SUPPLEMENTAL AGREEMENT NO. 1
BACKFILL ABUTMENT WITH STABILIZED MATERIAL

DELIVERY POINTS FOR MATERIALS:
DALLAS, ALL RAILROADS

Note: The Contractor shall make his own investigation and
arrangements for trackage facilities.

The Contractor shall provide and erect barricades and warning
signs in accordance with BC(1)72, BC(2)72, BC(3)72, BC(4)72,
BC(5)72, BC(6)72, at points indicated and at other
points as directed by the Engineer.
Construction Identification Signs for Federal Aid Projects shall be
erected in accordance with CIS-71.
Barricades Class I(C) with signs G20-6, W20-1C, W20-1B, W20-1A,
G20-1, G20-2, C-2, and R10-6 will be required at each
end of the project.

DALLAS COUNTY

CITY OF DALLAS

APPROVED: May 4 1971
James S. Smith
DIRECTOR OF PUBLIC WORKS

RECOMMENDED April 20 1971
FOR APPROVAL

Monroe J. P. Smith
Asst. DIRECTOR OF PUBLIC WORKS

APPROVED April 22 1971

Asst. CITY MANAGER

TEXAS HIGHWAY DEPARTMENT

CORRECT 22 Feb 1971

W. R. Vacher
S.J.P. RESIDENT ENGINEER

CORRECT 4-9-1971

J. R. Kirkpatrick
DISTRICT DESIGN ENGINEER

CORRECT 4-12-71

N. D. Poling
S.J.P. RESIDENT ENGINEER

RECOMMENDED 4-12-71

John L. Keller
DISTRICT ENGINEER

APPROVED: 1/20/72

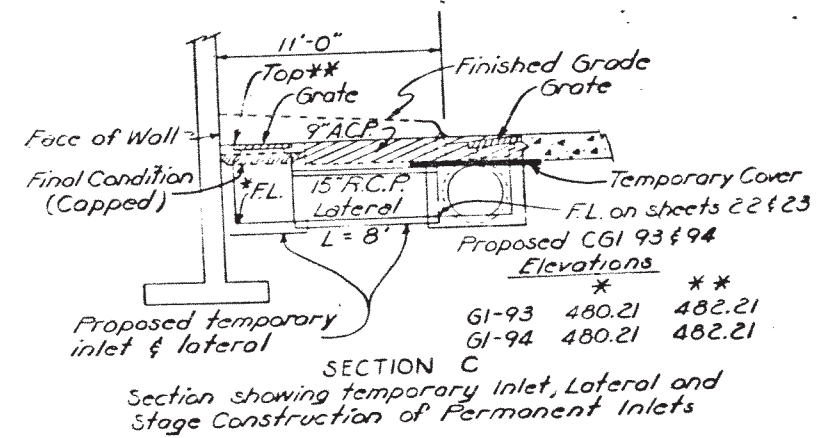
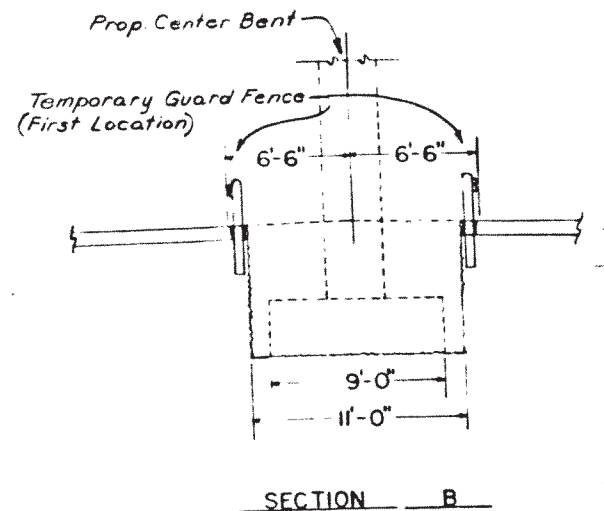
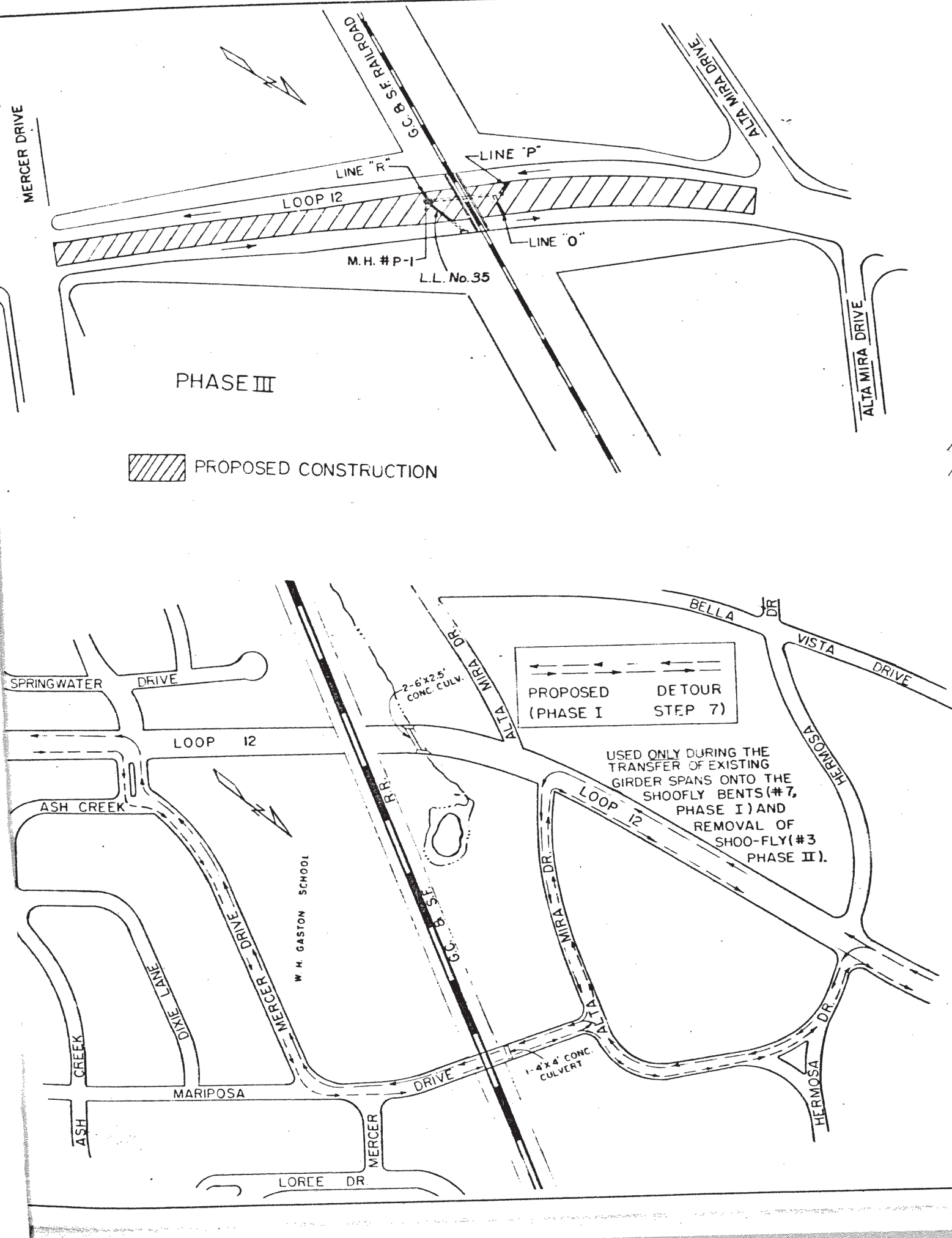
Steve Thacker
BRIDGE ENGINEER

APPROVED: 1/20/72

R. L. Lewis
CHIEF ENGINEER OF HIGHWAY DESIGN

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

DATE ACCEPTED: JUNE 21, 1972
DATE ACCEPTED: JUNE 20, 1974



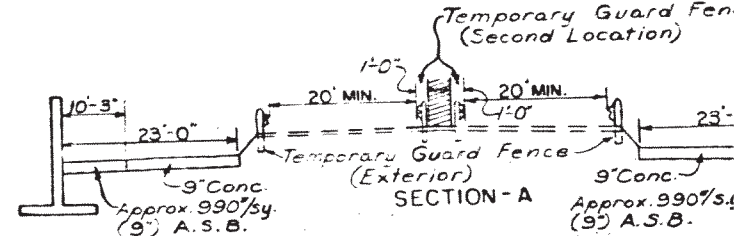
NOTE: INLET CGI-93 AND CGI-94 TO BE CONSTRUCTED IN STAGE I AND II. UNDER STAGE I (PHASE II-5) THE INLETS SHALL BE CONSTRUCTED TO A POINT ABOVE THE TOP OF THE LATERAL PIPE AND LATERAL WHICH WILL ALLOW A TEMPORARY COVER AS DIRECTED BY THE ENGINEER. THE INLETS SHALL THEN BE COVERED WITH A.S.B. DURING DETOUR USAGE. STAGE II CONSTRUCTION SHALL BE PERFORMED DURING PHASE III-3.

Note: Construct Spring Reservoir with the north retaining wall. Connect the 12" R.C. Overflow pipe into Line "O" during this phase. Place the 6" C.I. Pipe during the grading phases, laying short sections as allowed by the exposed roadway widths.

- SEQUENCE OF CONSTRUCTION
- PHASE I.
1. CONSTRUCT TEMPORARY DRAINAGE STRUCTURE RIGHT FROM STA. 707+50 TO STA. 708+80, WHICH INCLUDES JACKING OR BORING UNDER EXISTING RAIL AND CONSTRUCTION OF TEMPORARY SUMP HOLE.
 2. CONSTRUCT TEMPORARY DRAINAGE PIPE UNDER SHOOF-FLY AND SHOOF-FLY EMB
 3. EXCAVATE AND INSTALL PANELS NOS. 19R AND 31L.
 4. BACKFILL AGAINST 19R AND 31L TO DESIRED SLOPE. INSTALL TEMPORARY FENCE ADJACENT TO MEDIAN (SECTION B).
 5. CONSTRUCT SHOOF-FLY BENTS NOS. 1 THRU 5. COMPLETE SHOOF-FLY TRESTLE
 6. LAY BALLAST AND TRACKS ON SHOOF-FLY WHERE POSSIBLE (R. R. FORCES)
 7. DURING SUNDAY DAYLIGHT HOURS, ROUTE TRAFFIC AROUND PROJECT (VIA ALTA MIRA DRIVE AND MERCER DRIVE). MOVE EXISTING GIRDER SPANS A TRACK FROM EXISTING STRUCTURE ONTO SHOOF-FLY BENTS AND COMPLETE SHOOF-FLY STRUCTURE. UPON COMPLETION OF RELOCATING GIRDER SPANS, ROUTE TRAFFIC BACK TO LOOP 12.
 8. REMOVE EXISTING ABUTMENTS AND CENTER PIER.
 9. EXCAVATE OUT AND INSTALL NEW ABUTMENTS AND PANELS NOS. 17R AND 17L.
 10. INSTALL CENTER BENT NO. 2 OF THE PROPOSED NEW RAILROAD STRUCTURE

NOTE: DURING THE CONSTRUCTION OF BOTH CENTER BENTS (SHOOF-FLY AND PERM) THE CONTRACTOR WILL BE PERMITTED TO CLOSE ONLY ONE EXISTING TRAIL LANE DURING THE HOURS BETWEEN 9:00 A. M. AND 4:00 P. M.

- PHASE II.
1. COMPLETE CONSTRUCTION OF NEW STRUCTURE.
 2. SWITCH TRAIN TRAFFIC ONTO NEW STRUCTURE.
 3. DURING SUNDAY DAYLIGHT HOURS, ROUTE TRAFFIC AROUND PROJECT (VIA ALTA MIRA DRIVE AND MERCER DRIVE) AND REMOVE SHOOF-FLY GIRDER: COMPLETION OF GIRDER REMOVAL, ROUTE TRAFFIC BACK TO LOOP 12.
 4. REPLACE MEDIAN PAVEMENT AND RELOCATE TEMPORARY GUARD FENCE (SECTION A).
 5. CONSTRUCT STORM SEWER LINE P, CGI-91, LL #39. TEMPORARY AND ST. STRUCTURE OF PERMANENT INLET AT CGI-94 LOCATION (SECT. C). CC LINE R WHERE POSSIBLE AND UNDERDRAINS UNDER OUTER SOUTHBOUND I TEMPORARY A.S.B. WIDENING. CONSTRUCT LINE O, CGI-78, LL #35. RARY AND STAGE CONSTRUCTION OF PERMANENT INLET AT CGI-93 LOCATION (SECT. C), STAGE I OF CGI-76, DI #77, LINE O, DI-92 AND ALL P UNDERDRAINS UNDER OUTER NORTHBOUND LANE AND TEMPORARY WIDENIN
 6. LENGTHEN CULVERT AT STA. 710+12 LT AND RT.
 7. CONSTRUCT REMAINING RETAINING WALLS AND RIPRAP FLUME.
 8. CONSTRUCT TEMPORARY EXTERIOR GUARD FENCE AS SHOWN ON SECTION COMPLETE DRAINAGE INSTALLATION. GRADING AND PAVING OF OUTSIDE BOTH DIRECTIONS WITH 9" CONC PAVEMENT (31' TO 43'-9" LT. AND APPROXIMATELY 990#/S.Y. A. S. B. (P.D. AREA - 1ST COURSE) AND TRAFFIC ONTO THESE LANES.
 9. REMOVE AND REPLACE EXTERIOR GUARD FENCE TO FINAL LOCATION.

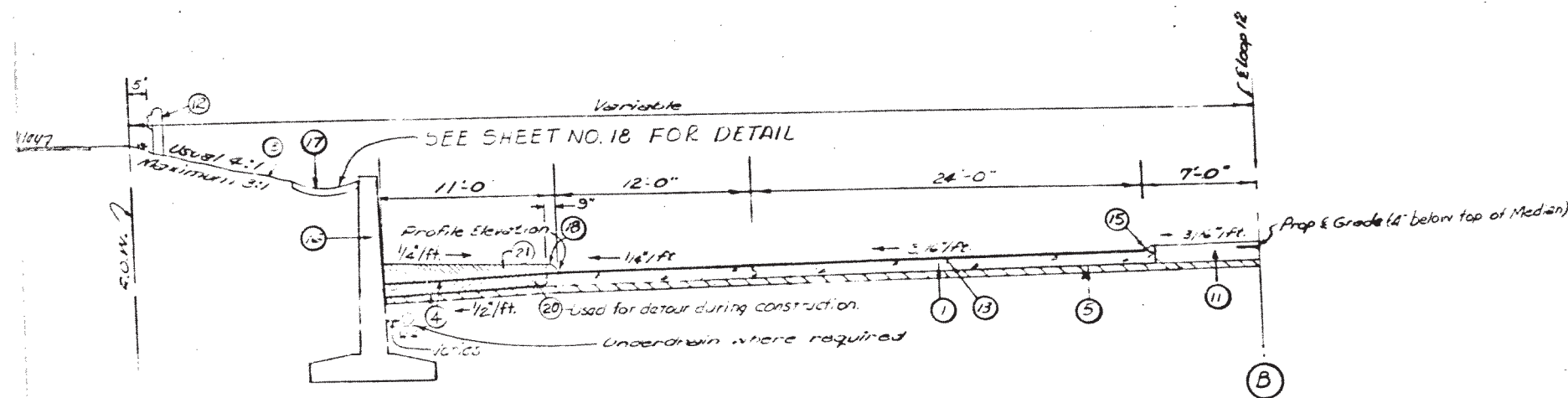
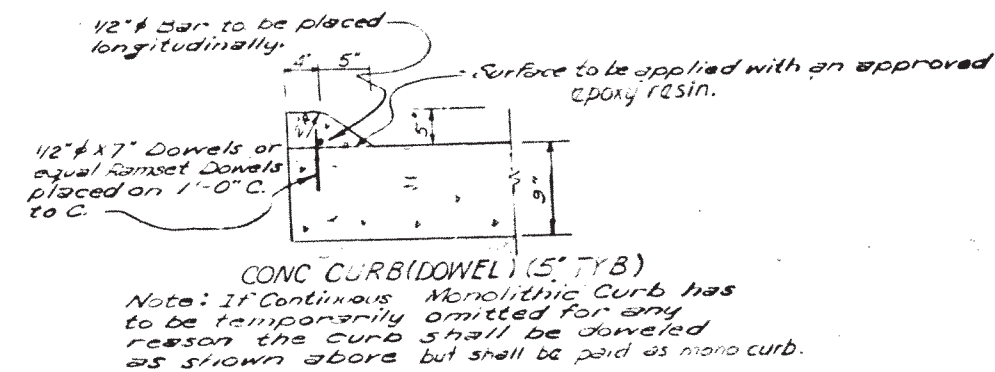
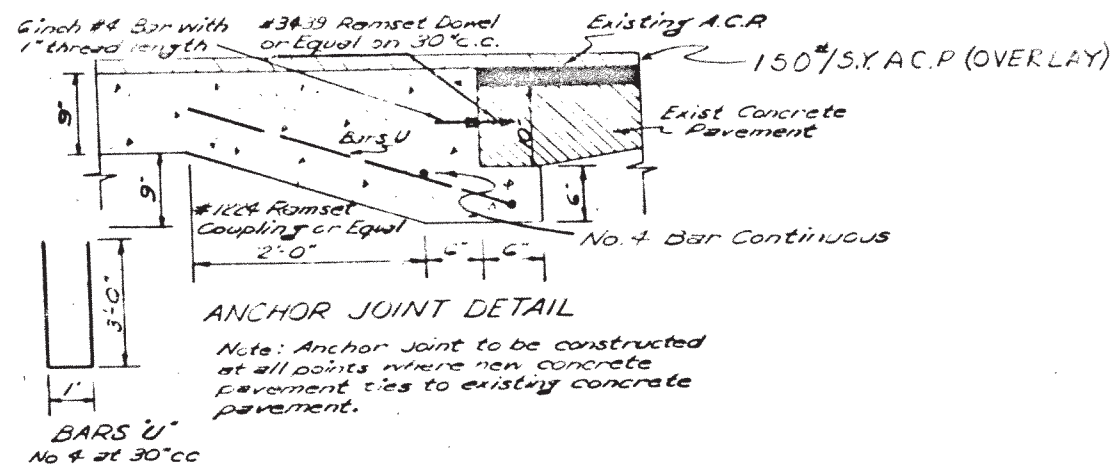


- PHASE III.
1. COMPLETE STORM SEWER CONNECTIONS (LINES O, P, R, MH #P-1, L UNDERDRAINS.
 2. COMPLETE GRADING AND PAVING OF INSIDE TWO LANES AND MEDIAN.
 3. ROUTE TRAFFIC ONTO INSIDE LANES. REMOVE TEMPORARY DRAINAGE COMPLETE CURB AND PEDESTRIAN AREA.

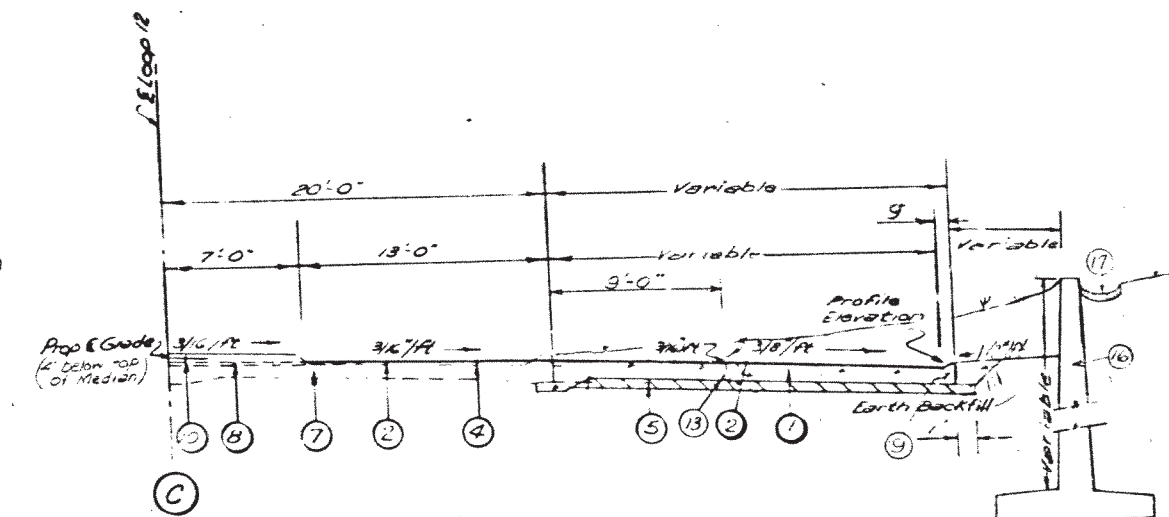
PROJECT LAYOUT SHEET

SHEET 2 of 2

STATE	FEDERAL
6 TEXAS	100
STATE	COUNTY
100	100



This section to be used between the following stations.
 Rt 705+00 - 710+84
 Lt & Rt 710+84 - 711+25 (Transition to 33' Roadway)
 from sta 710+84 to 713+34



This section to be used between the following stations

701+00 to 702+50 (Begin Retaining Wall Sect. © on Left at Sta. 702+05)
 *702+50 to 705+00 (Transition to 36' Roadway as dimensioned on Sect. ©)
 (Begin Retaining Wall on Rt. at Sta. 704+85)
 *711+25 to 713+00 (Transition from 36' Roadway Sect. © to Sect. ©)

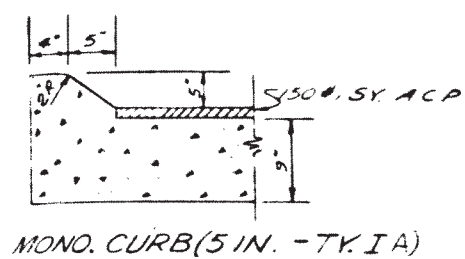
- LEGEND**
- ① 9" JOINTED CONCRETE PAVEMENT (CP)
 - ② Approx. 150#/SY A.C.P. (Overlay)
 - ③ Straw Match Sanding & Fe
 - ④ TACK COAT
 - ⑤ APPROXIMATELY 6" STABILIZED SUBGRADE (4% LIME)
 - ⑥ EXISTING FLEXIBLE BASE SHOULDER TO BE SALVAGED AND MIXED WITH 6% OF SUBGRADE BEFORE STABILIZING WITH LIME.
 - ⑦ EXISTING CONCRETE PAVEMENT
 - ⑧ EXISTING 2" A.C.P.
 - ⑨ MONO CURB (5 IN. - TY. 1A)
 - ⑩ CLASS "B" CONCRETE MEDIAN
 - ⑪ Approx. 1430#/SY. A.S.B.
 - ⑫ Galv. Steel Beam Guard Fence
 - ⑬ SAWED JOINT
 - ⑮ MONO. CURB (4 IN. - TY. 11)
 - ⑯ RETAINING WALL
 - ⑰ CLASS "B" CONCRETE FLUME
 - ⑱ Conc Curb (Dowel) (5" TYB)
 - ⑲ 4" Concrete Sidewalk
 - ⑳ Approx. 990#/sy. A.S.B.
 - ㉑ Variable A.S.B. (973#/SY)

TYPICAL SECTIONS

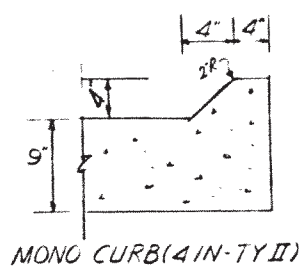
SCALE 1" = 5'

SHEET 1 OF 2

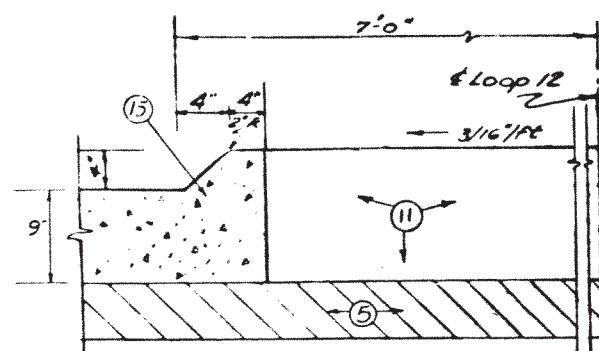
STATE	FEDERAL
TEXAS	UG
COUNTY	
DALLAS	



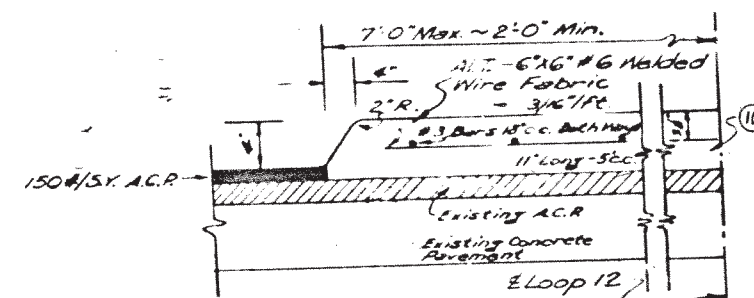
MONO CURB(5 IN. - TY. I A)



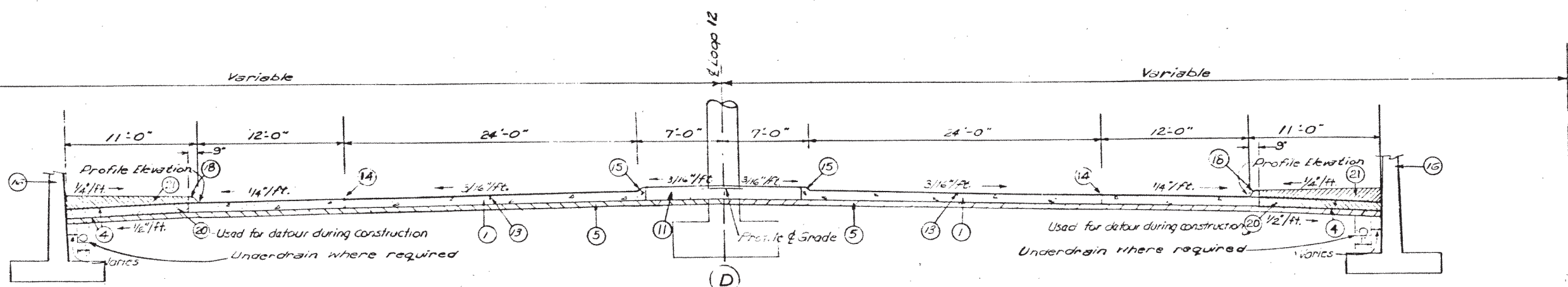
MONO CURB(4 IN. - TY. II)



HALF SECTION ~ A.S.B. MEDIAN
AT NEW CONSTRUCTION



HALF SECTION ~ CONCRETE MEDIAN
(CLASS "B" CONCRETE)



This Section to be used at
G.C. & S.F.R.R. Underpass

- LEGEND
- ① 9" JOINTED CONCRETE PAVEMENT (1)
 - ② 150# S.Y. A.C.P. (OVERLAY)
 - ③ Straw Mulch Seeding &
 - ④ TACK COAT
 - ⑤ APPROXIMATELY 6" STABILIZED SUBGRADE (4% LIME)
 - ⑥ EXISTING FLEXIBLE BASE SHOULDER TO BE SALVAGED AND MIXED WITH 6% OF SUBGRADE BEFORE STABILIZED WITH LIME.
 - ⑦ EXISTING CONCRETE PAVEMENT
 - ⑧ EXISTING 2" A.C.P.
 - ⑨ MONO CURB (5 IN. - TY. I A)
 - ⑩ CLASS "B" CONCRETE MEDIAN
 - ⑪ approx. 1450# S.Y. A.S.B.
 - ⑫ Galv. Steel Beam Guard Fe
 - ⑬ SAWED JOINT
 - ⑭ Construction Joint
 - ⑮ MONO CURB (4 IN. - TY. II)
 - ⑯ RETAINING WALL
 - ⑰ CLASS "B" CONCRETE FLUME
 - ⑱ Conc Curb (Don't 5 in Ty.
 - ⑲ 4" Concrete Sidewalk
 - ⑳ Approx. 990# S.Y. A.S.B.
 - ㉑ Variable A.S.B. / 973# S.

TYPICAL SECTIONS

SCALE 1"=5'

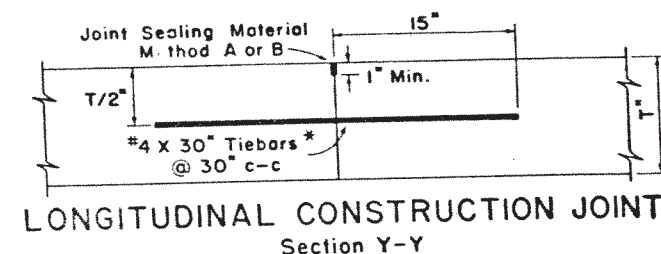
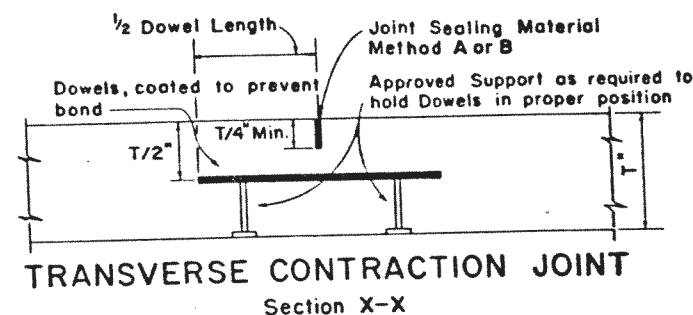
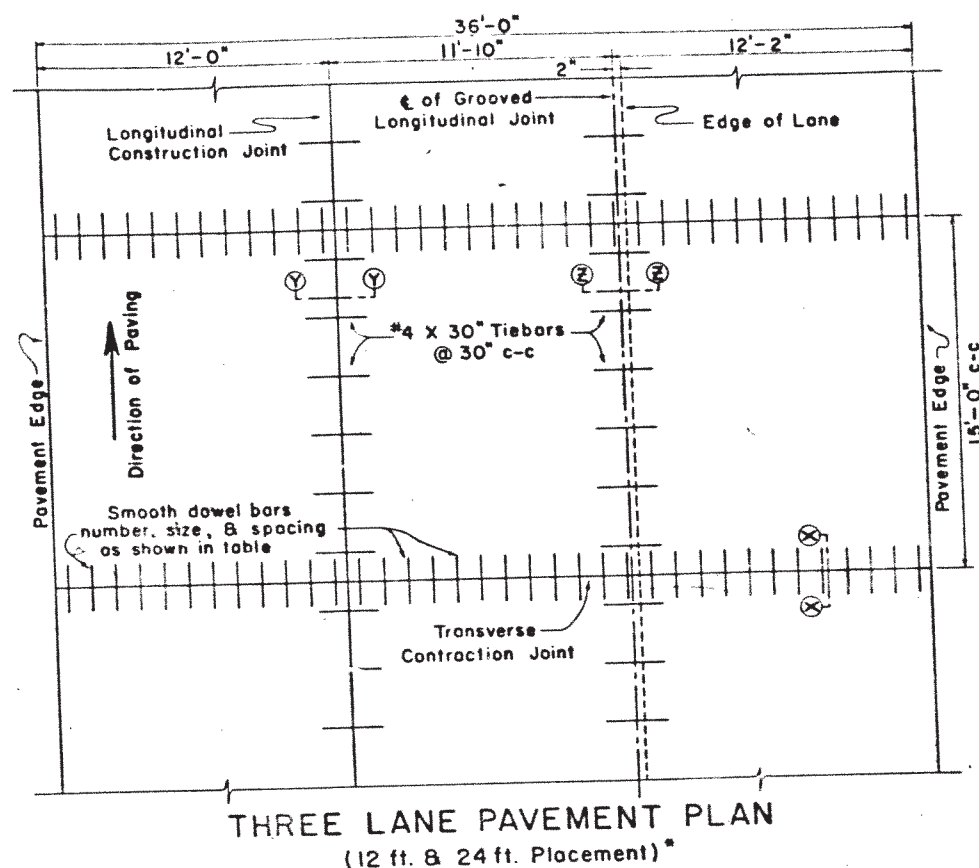
SHEET 2 OF 2

STATE	FEDERAL AID	PROJ.
6	TEXAS	U5-155
STATE	COUNTY	COUNT
4		

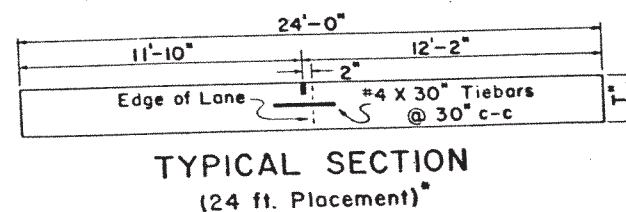
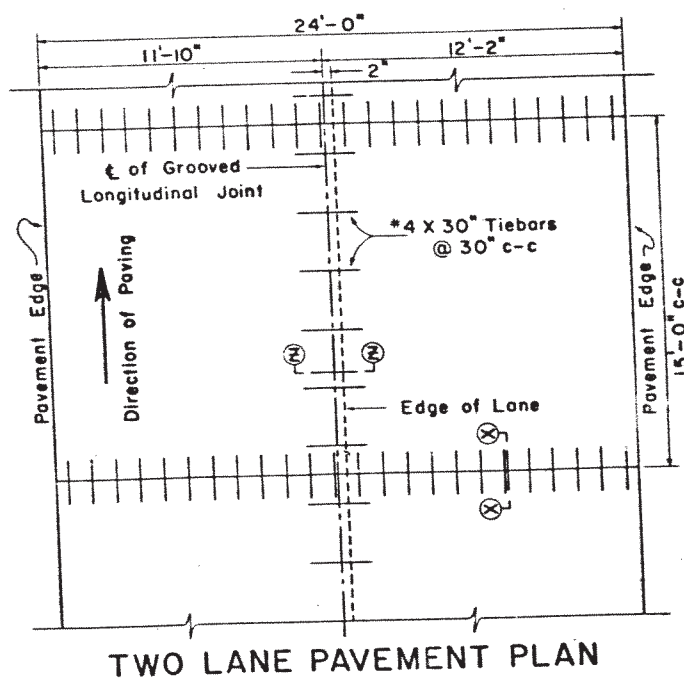
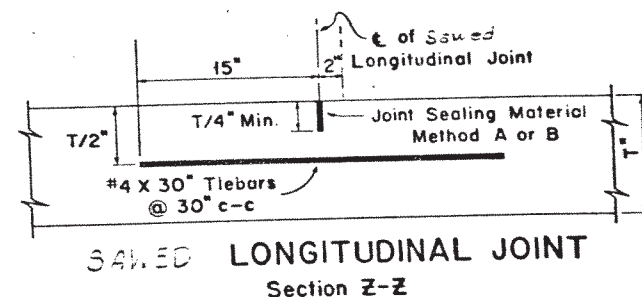
GENERAL NOTES

- NO EXPANSION JOINTS WILL BE USED EXCEPT AT STRUCTURE ENDS OR FIXED OBJECTS AS SHOWN ELSEWHERE IN THE PLANS.
- FOR FURTHER INFORMATION REGARDING THE PLACEMENT OF CONCRETE AND LOAD TRANSFER DEVICES REFER TO THE GOVERNING SPECIFICATIONS FOR "CONCRETE PAVEMENT".
- DETAILS AS TO PAVEMENT WIDTH, PAVEMENT THICKNESS, AND THE CROWN CROSS-SLOPE SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
- JOINT GROOVE AND SEAL DETAILS SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
- TIEBARS SHALL BE SECURED PARALLEL TO THE PAVEMENT SURFACE AND PERPENDICULAR TO THE CENTERLINE BY:
 - USE OF BAR CHAIRS
 - ACCURATELY PLACED IN POSITION ON THE SCREEDED CONCRETE BY MEANS OF AN APPROVED TEMPLATE AND FORCED TO THE PROPER POSITION WITH A SUITABLE TOOL; OR
 - BY ANY OTHER MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- DOWEL BARS SHALL BE SECURED PARALLEL TO THE PAVEMENT SURFACE AND CENTERLINE BY A DOWEL BAR CHAIR.
- WHEN WORK IS STOPPED DUE TO BREAKDOWN OR OTHER CAUSE, CONCRETE SHALL BE REMOVED BEYOND LAST CONTRACTION JOINT IN PLACE AND A HEADER INSTALLED.
- WHERE A MONOLITHIC CURB IS SPECIFIED, THE JOINT IN THE CURB SHALL COINCIDE WITH PAVEMENT JOINTS AND MAY BE FORMED BY ANY MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- CONSTRUCTION JOINTS MAY BE FORMED BY USE OF METAL OR WOOD FORMS EQUAL IN DEPTH TO THE NOMINAL DEPTH OF THE PAVEMENT, OR BY OTHER MEANS WHICH HAVE BEEN APPROVED BY THE ENGINEER PRIOR TO THEIR USE.
- LONGITUDINAL AND TRANSVERSE STEEL SPACING SHALL NOT VARY MORE THAN ONE TWELFTH OF THE SPACING SHOWN HEREON.
- THE TIEBAR SPACINGS SHOWN ARE FOR ASTM DESIGNATIONS: A-615, OR A-616, GRADE 60, TIEBARS, WHICH SHALL NOT BE BENT. IF TIEBARS ARE TO BE BENT, THEY SHALL BE STEEL CONFORMING TO ASTM DESIGNATION: A-615, GRADE 40, WITH A CENTER TO CENTER SPACING OF 24 INCHES.

(REV)
- SEE RC (CPCR-71) FOR STEEL PLACING REQUIREMENTS IN THE AREA OF CONFLUENCE AT RAMP TERMINALS.



*WITH THE APPROVAL OF THE ENGINEER, MULTIPLE-PIECE TIEBARS (THREADED COUPLING OR OTHER ADEQUATE DEVICE) MAY BE USED TO FACILITATE CONSTRUCTION PROVIDED THE SYSTEM DEVELOPS A FORCE EQUAL TO 1 1/2 TIMES THE MINIMUM FORCE OF THE TIEBAR SHOWN. THE SPACINGS FOR THE SYSTEM SHALL BE LESS THAN OR EQUAL TO THE SPACING ALLOWED FOR BARS OF SIMILAR YIELD STRENGTH.



* Lane widths are for illustrative purposes only and should not be used if in conflict with typical cross sections shown elsewhere in the plans.

DEPTH OF PAVEMENT (INCHES)	DOWELS (SMOOTH BARS)		
	SIZE AND LENGTH	AVERAGE SPACING (INCHES)	WEIGHT PER FOOT OF JOINT (LBS)
8	1" X 18"	12	4.01
9	1 1/8" X 20"	12	5.63
10	1 1/4" X 22"	12	7.65
11	1 3/8" X 24"	12	10.10

TEXAS HIGHWAY DEPARTMENT

CONCRETE PAVEMENT DETAILS
CONTRACTION DESIGN

CPCD-71 (Rev.) (MOD)

DN:	DRAWING	DATE	FED. RD.	STATE	FEDERAL PROJECT NO.
CK. DN:	Original	Feb. 1969	6	TEXAS	16155713
DW:					
CK. DW:					
TR:					
CK. TR:					