

11590, 28-13-17

FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.
6	TEXAS	156(14)
STATE DIST. NO.	COUNTY	STATE CONTROL NO.
20	Jefferson	28-13

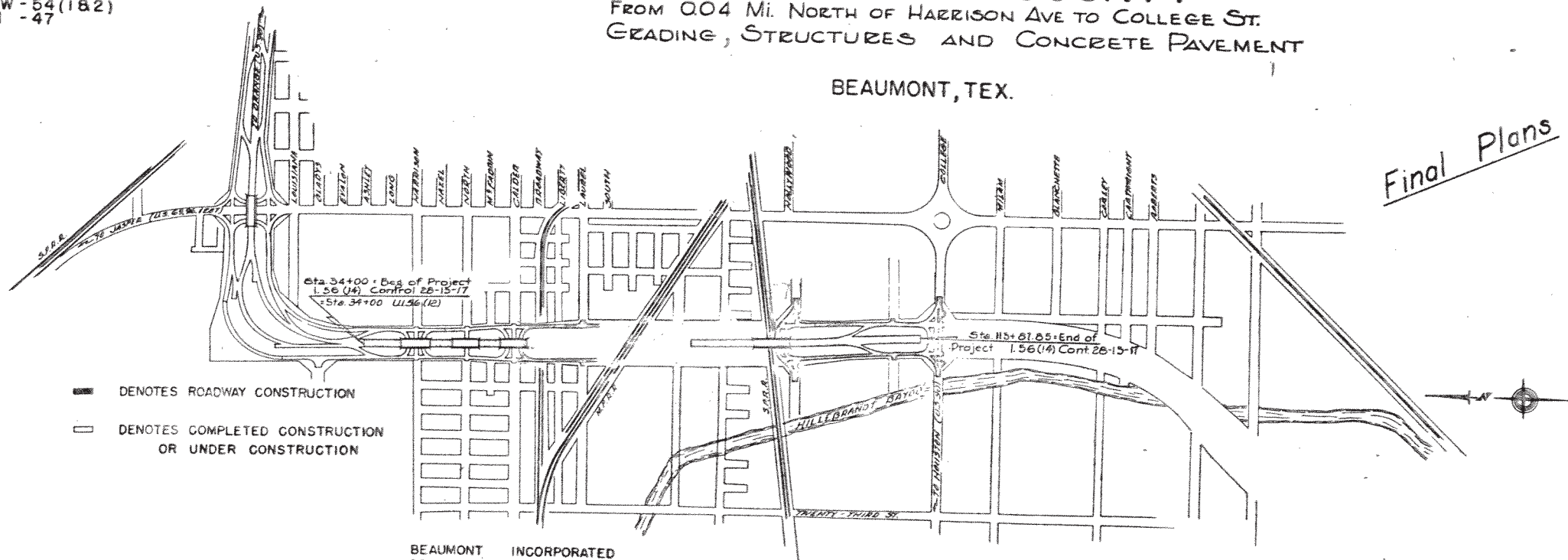
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STATE OF TEXAS
STATE HIGHWAY DEPARTMENT
PLANS OF COMPLETED
STATE HIGHWAY IMPROVEMENT

FEDERAL AID PROJECT.
156(14)
PLAN: 1 IN. = 20 FT.
PROFILE: 1 IN. HOR. = 20 FT., 1 IN. VERT. = 5 FT.
OTHERS AS NOTED.
NET LENGTH OF PROJECT = 6250.67 FT. = 1.188 MI. Bridges = 765.0' = 0.144 MI.
Roadway = 5515.67' = 1.044 MI.

JEFFERSON COUNTY
FROM 0.04 MI. NORTH OF HARRISON AVE TO COLLEGE ST.
GRADING, STRUCTURES AND CONCRETE PAVEMENT

BEAUMONT, TEX.



— DENOTES ROADWAY CONSTRUCTION
- - - DENOTES COMPLETED CONSTRUCTION OR UNDER CONSTRUCTION

BEAUMONT INCORPORATED
POPULATION 94,014 1950

Equations:
None

Exceptions:
Sta 36+34.99 to Sta 38+66.49 - 231.50'
Sta 43+55.04 to Sta 45+86.54 - 231.50'
Sta 65+45 to Sta 77+89.18 - 1244.18'
Total - 1707.18'

DELIVERY POINT FOR MATERIALS
Beaumont, Tex. - All Railroads

LAYOUT SCALE: 1 IN. = 1000 FT.

DESIGN SPEED 50 M

- APPROVED FIELD CHANGES
- | No. | DESCRIPTION |
|-----|--|
| 1 | REVISION OF ENTRANCE RAMP AT STA 120+00. RELOCATING INLET CI 24" PROVIDING 6' RETURN ON FANNIN |
| 2 | CONSTRUCTING TYPE 'F' INLET BETWEEN HARRISON & NORTH ST. & TWO 21" RC AT LIBERTY ST. |
| 3 | REVISION OF EXIT RAMP AT STA 103. |
| 4 | REVISION OF PAVEMENT WIDTH ON C ST. & CONSTRUCTING TYPE 'B' INLET STA 11+92 (COLLEGE ST.) |
| 5 | REVISION OF PAVEMENT WIDTH ON C ST. & CONSTRUCTING TYPE 'B' INLET STA 12+34 (COLLEGE ST.) |
- EXTRA WORK ORDERS
1. PLACING NON-MET. CONT. AT COLLEGE FOR FUTURE SIGNAL SYSTEM.

NOTE:—
The contractor shall provide and erect Barricades and Warning Signs in accordance with BW-54 at points as directed by the Engineer and as shown on Layout Sheet No. 1.
Class "E" Barricades shall be provided at all cross streets when ordered by the Engineer.
See Special Provisions, Description of Project, Scope of Contract, Traffic Service and Sequence of Work.

PROJECT CONSTRUCTED AND FINAL PLANS PREPARED BY
[Signature]
SUPV. RES. ENGR.

RECOMMENDED FOR APPROVAL
10-26-1956
[Signature]
City Engineer of Beaumont, Texas

APPROVED
10-26-1956
[Signature]
Mayor, City of Beaumont, Texas

CORRECT: October 11, 1956

[Signature]
DISTRICT ENGINEER

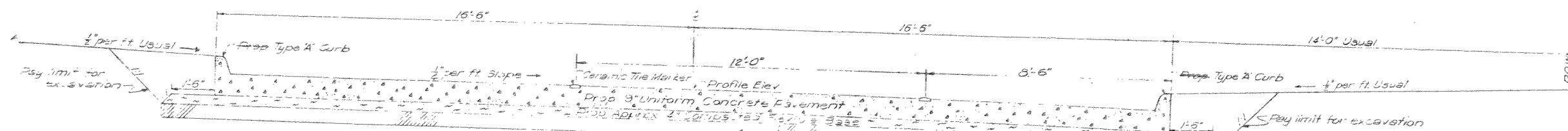
RECOMMENDED FOR APPROVAL
10-19-56

[Signature]
DISTRICT ENGINEER

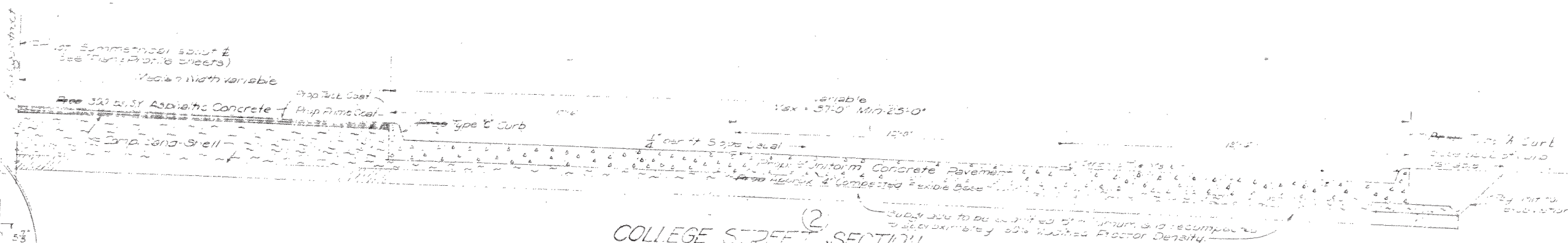
DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
APPROVED:	DATE
DISTRICT ENGINEER	DATE

COUNTY PROJ. NO.
HWY. NO. LETTING DATE
DATE ACCEPTED

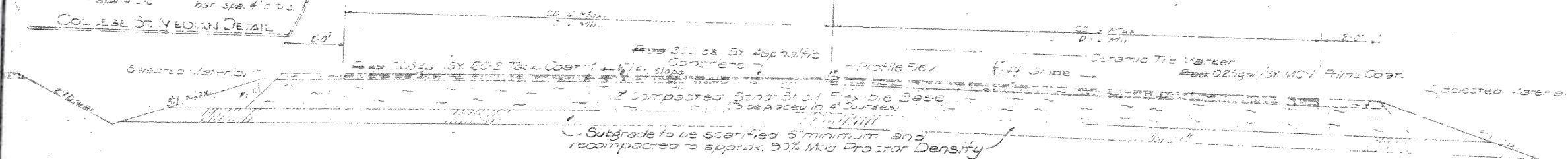
CONVENTIONAL SIGNS	
STATE OR NATIONAL LINE	—
CITY OR VILLAGE LINE	—
COUNTY LINE	—
BASE ON SURVEY LINE	—
RIGHT OF WAY LINE	—
RIGHT OF WAY MARKERS	—
FENCE LINE	—
RAILROAD	—
TRAVELLED WAY	—
CULVERT OR BRIDGE	—
POWER LINE	—
TELEGRAPH OR TELEPHONE	—



FRONTAGE ST. SECTION I
To be used approximately 70 Stations

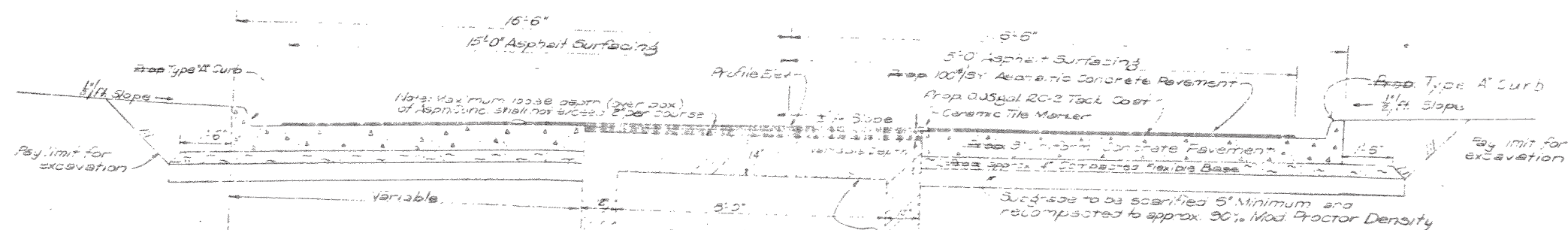


COLLEGE STREET SECTION
To be used approximately 14 Stations



3 TYPICAL FLEXIBLE BASE SECTION

TEMPORARILY CONNECTION AT LAUREL ST.
To be used approximately 14 Stations



LEFT FRONTAGE ST. SECTION
To be used approximately 3 Stations

TYPICAL CROSS-SECTIONS
DETAILS TYPE 'A' CURB
DETAILS COLLEGE ST. MEDIAN

GENERAL NOTES

Figure 1. Schematic representation of the experimental design. The subjects were divided into two groups: the control group (CG) and the experimental group (EG). The CG was divided into two subgroups: the control group (CG) and the control group (CG). The EG was divided into two subgroups: the experimental group (EG) and the experimental group (EG). The subjects were divided into two groups: the control group (CG) and the experimental group (EG). The CG was divided into two subgroups: the control group (CG) and the control group (CG). The EG was divided into two subgroups: the experimental group (EG) and the experimental group (EG).

1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100. 101. 102. 103. 104. 105. 106. 107. 108. 109. 110. 111. 112. 113. 114. 115. 116. 117. 118. 119. 120. 121. 122. 123. 124. 125. 126. 127. 128. 129. 130. 131. 132. 133. 134. 135. 136. 137. 138. 139. 140. 141. 142. 143. 144. 145. 146. 147. 148. 149. 150. 151. 152. 153. 154. 155. 156. 157. 158. 159. 160. 161. 162. 163. 164. 165. 166. 167. 168. 169. 170. 171. 172. 173. 174. 175. 176. 177. 178. 179. 180. 181. 182. 183. 184. 185. 186. 187. 188. 189. 190. 191. 192. 193. 194. 195. 196. 197. 198. 199. 200. 201. 202. 203. 204. 205. 206. 207. 208. 209. 210. 211. 212. 213. 214. 215. 216. 217. 218. 219. 220. 221. 222. 223. 224. 225. 226. 227. 228. 229. 230. 231. 232. 233. 234. 235. 236. 237. 238. 239. 240. 241. 242. 243. 244. 245. 246. 247. 248. 249. 250. 251. 252. 253. 254. 255. 256. 257. 258. 259. 260. 261. 262. 263. 264. 265. 266. 267. 268. 269. 270. 271. 272. 273. 274. 275. 276. 277. 278. 279. 280. 281. 282. 283. 284. 285. 286. 287. 288. 289. 290. 291. 292. 293. 294. 295. 296. 297. 298. 299. 300. 301. 302. 303. 304. 305. 306. 307. 308. 309. 310. 311. 312. 313. 314. 315. 316. 317. 318. 319. 320. 321. 322. 323. 324. 325. 326. 327. 328. 329. 330. 331. 332. 333. 334. 335. 336. 337. 338. 339. 340. 341. 342. 343. 344. 345. 346. 347. 348. 349. 350. 351. 352. 353. 354. 355. 356. 357. 358. 359. 360. 361. 362. 363. 364. 365. 366. 367. 368. 369. 370. 371. 372. 373. 374. 375. 376. 377. 378. 379. 380. 381. 382. 383. 384. 385. 386. 387. 388. 389. 390. 391. 392. 393. 394. 395. 396. 397. 398. 399. 400. 401. 402. 403. 404. 405. 406. 407. 408. 409. 410. 411. 412. 413. 414. 415. 416. 417. 418. 419. 420. 421. 422. 423. 424. 425. 426. 427. 428. 429. 430. 431. 432. 433. 434. 435. 436. 437. 438. 439. 440. 441. 442. 443. 444. 445. 446. 447. 448. 449. 450. 451. 452. 453. 454. 455. 456. 457. 458. 459. 460. 461. 462. 463. 464. 465. 466. 467. 468. 469. 470. 471. 472. 473. 474. 475. 476. 477. 478. 479. 480. 481. 482. 483. 484. 485. 486. 487. 488. 489. 490. 491. 492. 493. 494. 495. 496. 497. 498. 499. 500. 501. 502. 503. 504. 505. 506. 507. 508. 509. 510. 511. 512. 513. 514. 515. 516. 517. 518. 519. 520. 521. 522. 523. 524. 525. 526. 527. 528. 529. 530. 531. 532. 533. 534. 535. 536. 537. 538. 539. 540. 541. 542. 543. 544. 545. 546. 547. 548. 549. 550. 551. 552. 553. 554. 555. 556. 557. 558. 559. 560. 561. 562. 563. 564. 565. 566. 567. 568. 569. 570. 571. 572. 573. 574. 575. 576. 577. 578. 579. 580. 581. 582. 583. 584. 585. 586. 587. 588. 589. 590. 591. 592. 593. 594. 595. 596. 597. 598. 599. 600. 601. 602. 603. 604. 605. 606. 607. 608. 609. 610. 611. 612. 613. 614. 615. 616. 617. 618. 619. 620. 621. 622. 623. 624. 625. 626. 627. 628. 629. 630. 631. 632. 633. 634. 635. 636. 637. 638. 639. 640. 641. 642. 643. 644. 645. 646. 647. 648. 649. 650. 651. 652. 653. 654. 655. 656. 657. 658. 659. 660. 661. 662. 663. 664. 665. 666. 667. 668. 669. 670. 671. 672. 673. 674. 675. 676. 677. 678. 679. 680. 681. 682. 683. 684. 685. 686. 687. 688. 689. 690. 691. 692. 693. 694. 695. 696. 697. 698. 699. 700. 701. 702. 703. 704. 705. 706. 707. 708. 709. 710. 711. 712. 713. 714. 715. 716. 717. 718. 719. 720. 721. 722. 723. 724. 725. 726. 727. 728. 729. 730. 731. 732. 733. 734. 735. 736. 737. 738. 739. 740. 741. 742. 743. 744. 745. 746. 747. 748. 749. 750. 751. 752. 753. 754. 755. 756. 757. 758. 759. 760. 761. 762. 763. 764. 765. 766. 767. 768. 769. 770. 771. 772. 773. 774. 775. 776. 777. 778. 779. 780. 781. 782. 783. 784. 785. 786. 787. 788. 789. 790. 791. 792. 793. 794. 795. 796. 797. 798. 799. 800. 801. 802. 803. 804. 805. 806. 807. 808. 809. 810. 811. 812. 813. 814. 815. 816. 817. 818. 819. 820. 821. 822. 823. 824. 825. 826. 827. 828. 829. 830. 831. 832. 833. 834. 835. 836. 837. 838. 839. 840.

For more detailed info of above items
kindly consider visiting the website
of the above in these links

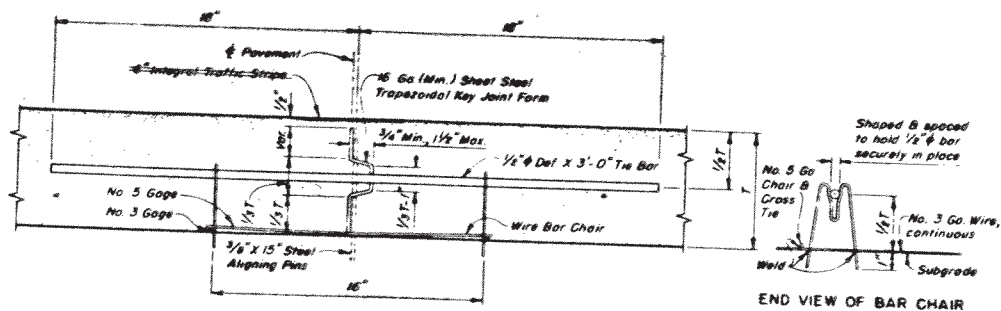
of 2007 is 3.9% and is projected to be 4.5% in 2008. The 2007-2008 fiscal year is projected to be 4.5% and is projected to be 4.5% in 2008.

EXPOSITIONS OF DIFFERENT WINGS RESPECT TO
A INTERSECTIONS WITH INTERSECTIONS OF
THE DIFFERENTIAL EQUATIONS OF THE

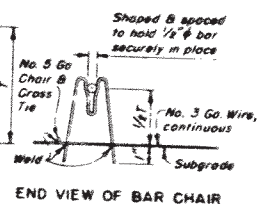
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Backfilling of excavated areas shall not be paid for directly but shall be considered outside to bid item "Common Road Excavation".

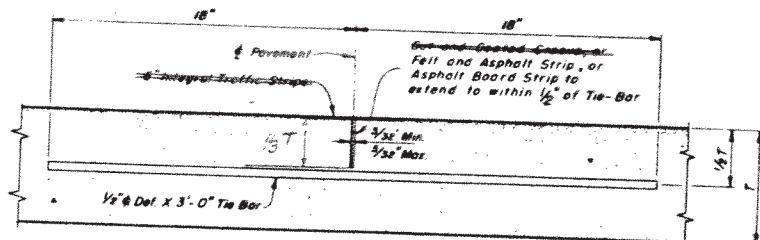
| | | | | | |
|-----------|---------|------|---------------------|-----------|-------------------------|
| DR. P. | DRAWING | DATE | TR. ROAD
DR. NO. | STATE | FEDERAL AID PROJECT NO. |
| CR. DR. 1 | | | 6 | TEXAS | |
| DR. 2 | | | STATE
DIST. NO. | COUNTY | CONTRACT
NO. |
| CR. DR. 3 | | | 20 | ROBERTSON | 10 |
| TR. 4 | | | | | |
| CR. TR. 5 | | | | | |



TYPE 1 - STEEL TONGUE-AND-GROOVE FORM



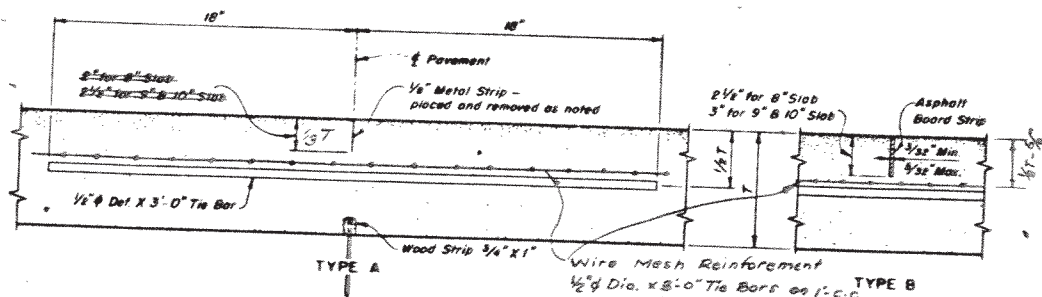
END VIEW OF BAR CHAIR



TYPE 2 - MACHINE CUT GROOVE

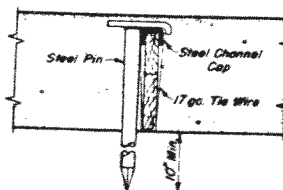
Top groove shall be cut by an approved machine and the vertical faces of the groove shall be smoothed. A continuous metal shield, shall be placed continuously in a groove cut in the concrete by an approved mechanical device operated in advance of the longitudinal float. The shield shall be true to line, vertical, and of the depth shown. The bars shall be installed as in Type 1, or accurately placed in position on the screeded concrete by means of an approved template and forced to the proper position with a suitable tool.

ALTERNATE TYPES OF LONGITUDINAL JOINTS



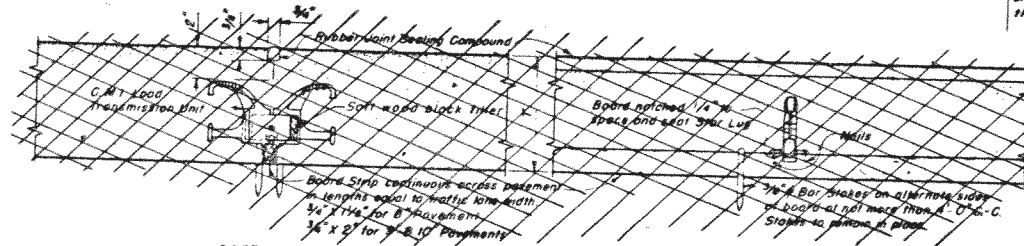
The 3/4 inch x 1 inch Wood Strip as shown for Type A shall be continuous for width of pavement, and shall be securely fastened to the subgrade by 40-penny wire nails driven through drilled holes on not more than 30 inch centers. The Bars shall be placed accurately in position, after screeding, by means of an approved template. The transverse finishing machine shall pass over the joint area after installing the bars. After longitudinal float has passed over, remove steel plate prior to finishing. Type B, Asphalt Board Strip - Asphalt board strip, held in an approved continuous metal shield, shall be placed continuously in a groove cut by an approved mechanical device operating in advance of the longitudinal float.

ALTERNATE TYPES OF TRANSVERSE WARPING JOINTS



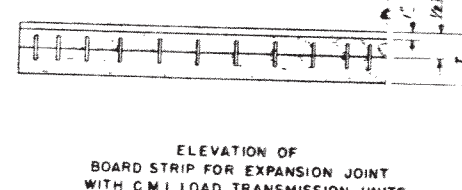
INSTALLING PIN FOR EXPANSION JOINT

Board Joint Filler of specified type shall be secured on subgrade in exact position and line as illustrated or by other approved device. Pins shall be removed after passage of finishing machine, then pavement resurfaced by second pass of finishing machine. After second passage of finishing machine remove concrete to 1 inch below top of board and nail 3/4 inch x 2 inch wood strip to top of board filler to form joint seal space. Replace concrete and finish with longitudinal float. The wood top strip shall not be removed until immediately prior to pouring joint seal.

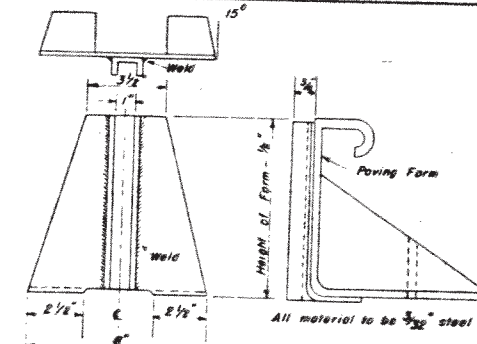


CAST MALLEABLE IRON CANTILEVER TYPE LOAD TRANSMISSION UNIT
D-14 1/2 "STAR LUG" as manufactured by Texas Foundries, Lufkin, Texas, or equal Load Transmission Unit

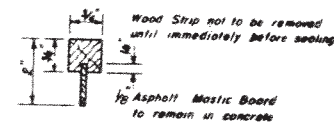
ALTERNATE TYPES OF TRANSVERSE CONTRACTION JOINTS



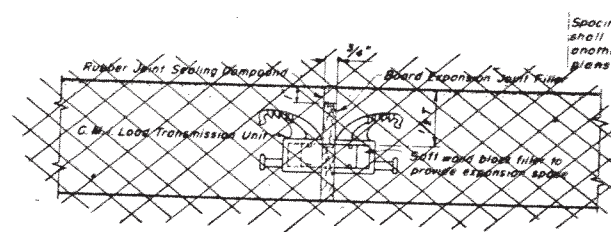
ELEVATION OF BOARD STRIP FOR EXPANSION JOINT WITH CMI LOAD TRANSMISSION UNITS



ACCEPTABLE CONTRACTION AND EXPANSION JOINT HOLDER
(Other types may be used if approved by engineer.)

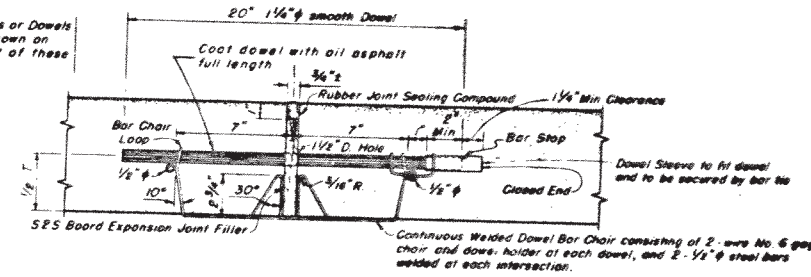


CONTRACTION JOINT SEAL FORM



CAST MALLEABLE IRON CANTILEVER TYPE LOAD TRANSMISSION UNIT
D-13 "STAR LUG" as manufactured by Texas Foundries, Lufkin, Texas, or equal Load Transmission Unit

ALTERNATE TYPES OF TRANSVERSE EXPANSION JOINTS



1/4" ROUND STEEL BAR DOWEL

GENERAL NOTES

Either of the alternate types of joints shown by these details may be constructed, at the option of the Contractor. If the Contractor desires to use any other alternate device, he shall, prior to its use, secure its approval by the Engineer.

Load Transmission Units or Dowels shall be secured parallel to the pavement surface and center line. All joints, including all materials, devices, and work required shall be considered subsidiary work and shall be included in the unit price bid for "Concrete Pavement." No direct payment will be made for any material, bar chair, steel, or any other device shown, nor for its installation.

"T" indicates center depth of thickened edge pavements or depth of uniform pavements. For thickened edge pavements the bottom edges of board expansion joint fillers shall be made to conform with the subgrade by the addition of wedges of the same material and thickness.

The Contractor shall hold and save the State, its officers, its agents, and its employees harmless to liability of any nature or kind, including costs and expenses, for or on account of any patent or unpatented invention, article or appliance manufactured or used in accordance with the details of these plans.

TEXAS HIGHWAY DEPARTMENT CONCRETE PAVEMENT JOINT DETAILS 8'-9'-10' SLABS 11"

C.P.J. - 52-2 (MOD)

REVISED FEB 7, 1952

| PER. NO. | STATE | FEDERAL AID PROJECT NO. | SHEET |
|----------|-----------|-------------------------|-------|
| 6 | TEXAS | 1-56/12 | 49 |
| STATE | COUNTY | SECTION | NO. |
| 25 | Jefferson | 28 | 13 |