

18	DALLAS	95
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Date Work Began - July 19, 1965  
Date Work Completed - January 26, 1967

FIELD CHANGES:

- No.1 Eliminate detours at Line J, Line K & Line L.  
Add Items for Jack Bore or Tunnel Pipe 5 feet  
(27 In R.C. (CI III), 60 In R.C. (CI III), 60 In R.C. (CI III))
- No.2 Construct Type III Inlets at Station 136+30,  
138+66, & 142+20 and connect to Line D  
with 15" Pipe Sewer.

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-3	PROJECT LAYOUT
4-6	TYPICAL SECTIONS
7-9	GENERAL NOTES & SPECIFICATION DATA
10-11	ESTIMATE & QUANTITY
12-13	MISCELLANEOUS SUMMARY
14-29	PLAN PROFILE SHEETS
30-33	INTERSECTION LAYOUTS
34-36	DRAINAGE AREA MAP
37-55	DRAINAGE LAYOUT SHEETS
56-57	DRAINAGE SUMMARY
58-59	INLET & MANHOLE DETAILS
60	CPCD-61 (MOD)
61	CH-11
62-63	BW-61 (1) & (2)
40A	PLAN SHEETS ADDED
42A	BY: FIELD CHANGE NO. 1
44A	
51A	
43A Rev 10/20/66	PLAN SHEETS ADDED
44 " "	BY: FIELD CHANGE NO. 2
59 " "	

# STATE OF TEXAS STATE HIGHWAY DEPARTMENT PLANS OF PROPOSED STATE HIGHWAY IMPROVEMENT

STATE PROJECT  
C 92-7-32 PD 0370

PLAN 1 IN = 20 FT  
PROFILE 1 IN HOR = 20 FT 1 IN VERT = 5 FT  
CROSS SECTIONS 1 IN HOR AND VERT = 5 FT  
OTHERS AS NOTED

SCALES

SECTION LENGTH OF PROJECT = 8988.50 FT 1.702 MI

TYPE: GRADING, STORM SEWERS & CONCRETE PAVEMENT  
**DALLAS COUNTY**  
STATE HIGHWAY 356  
FROM EAST LEVEE TRINITY RIVER  
TO I.H. 35 E CONNECTION

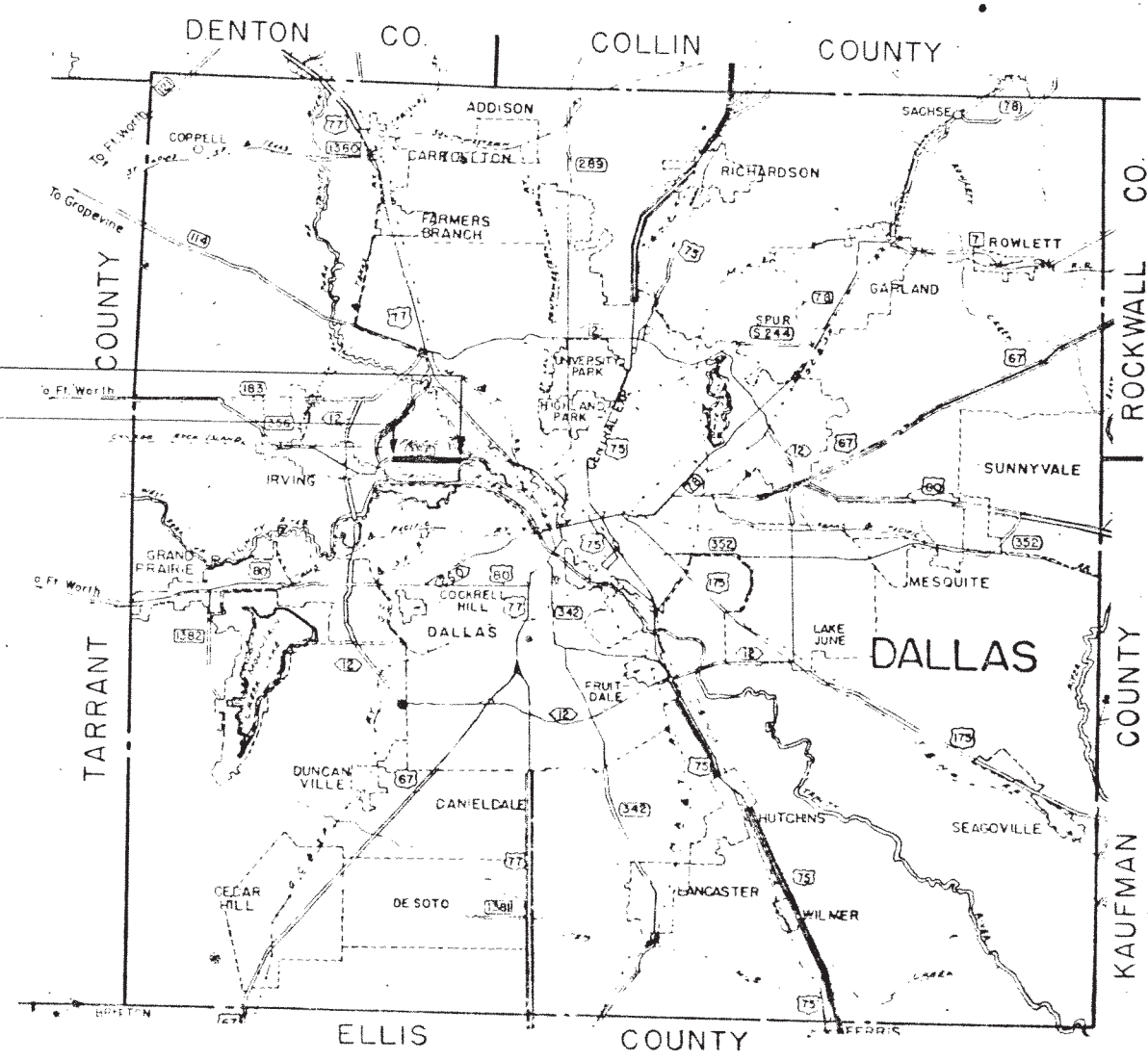
DELIVERY POINTS FOR MATERIALS  
DALLAS - ALL RAILROADS  
The Contractor shall make his own investigation and arrangements for trackage facilities.

Specifications adopted by the State Highway Department of Texas January 2, 1962 shall govern on this project. Special Labor Provisions for State Projects Adopted August 11, 1946.

Barricades and warning signs will be erected as shown on Standard B16-1(1) & (2)

Sta 188+88.50 End Project C92-7-32  
Control C92-7-32  
Sta 99+00 Begin Project C92-7-32  
Control C92-7-32

NO EQUATIONS  
NO EXCEPTIONS



DALLAS COUNTY

APPROVED April 22, 1965  
*K. H. H. H.*  
DIRECTOR OF PUBLIC WORKS

CITY OF DALLAS

RECOMMENDED FOR APPROVAL APRIL 21, 1965  
*W. S. M. M.*  
DIRECTOR OF PUBLIC WORKS

APPROVED APRIL 21, 1965  
*W. S. M. M.*  
ASSISTANT CITY MANAGER

STATE HIGHWAY DEPARTMENT

CORRECT April 5, 1965  
*Robert L. Hildner*  
RESIDENT ENGINEER

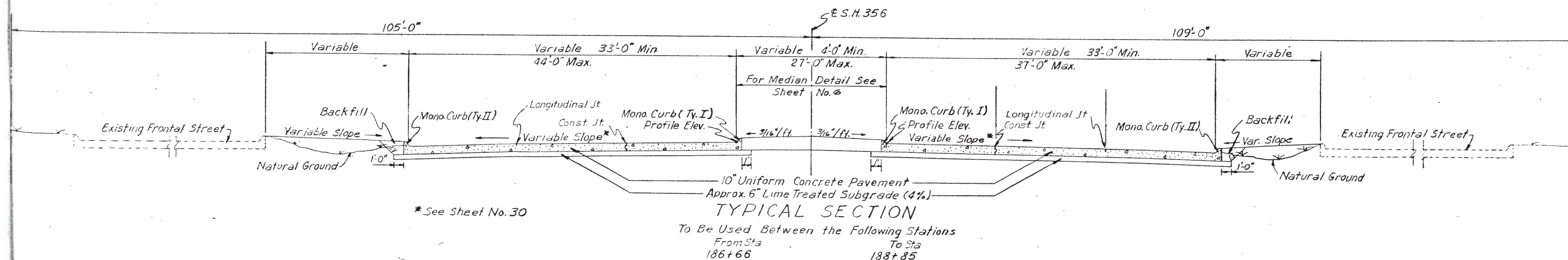
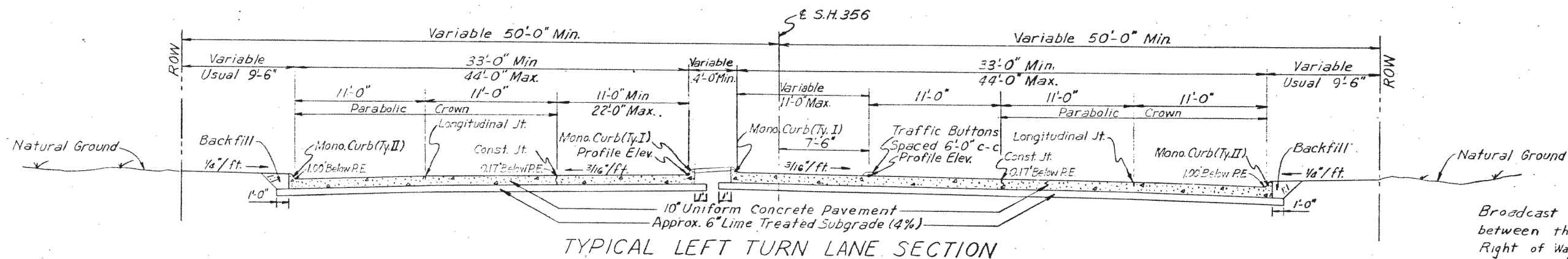
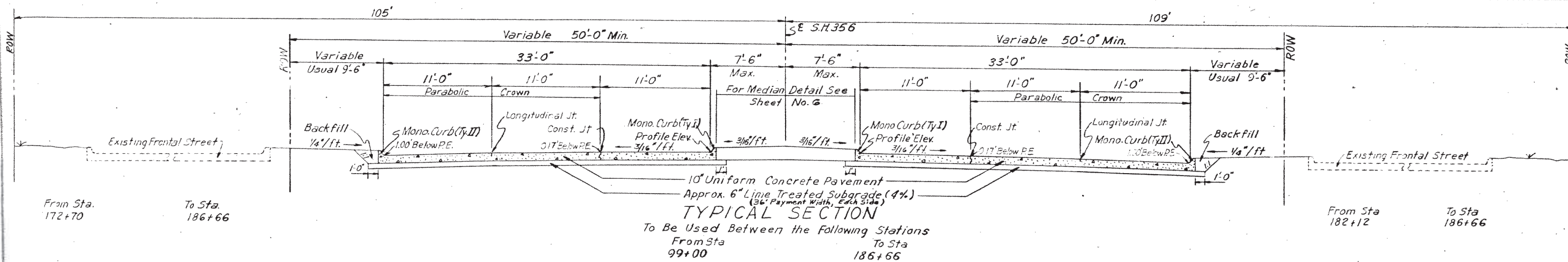
CORRECT 4-5-1965  
*John R. Hildner*  
DISTRICT DESIGN ENGINEER

APPROVED: 1965  
*B. A. J. Berry*  
CHIEF ENGINEER OF HIGHWAY DESIGN



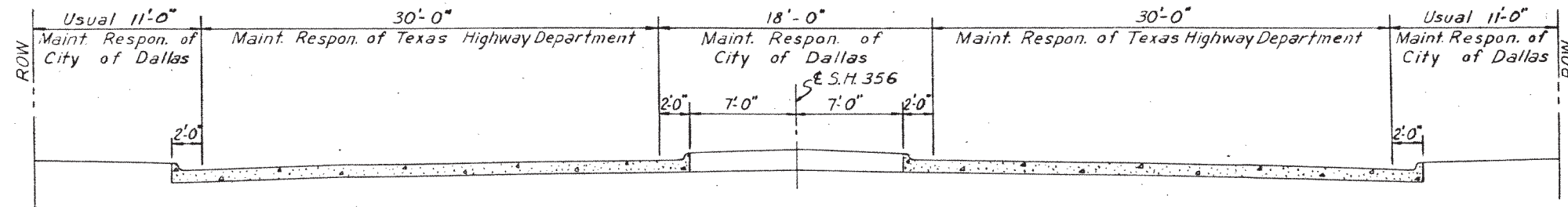
HWY NO. 1  
LETTERING DATE  
DATE ACCEPTED

STATE HIGHWAY DEPARTMENT	1
CITY OF DALLAS	1
COUNTY ENGINEER	1
BASE LAYOUT	1
RIGHT OF WAY	1
RIGHT OF WAY MARKING	1
STREET LIGHTS	1
RAILROADS	1
TRAVEL LANE	1
CURB AND GUTTER	1
PAVEMENT	1
TELEPHONE UTILITY	1



TYPICAL SECTIONS





## MAINTENANCE AGREEMENT

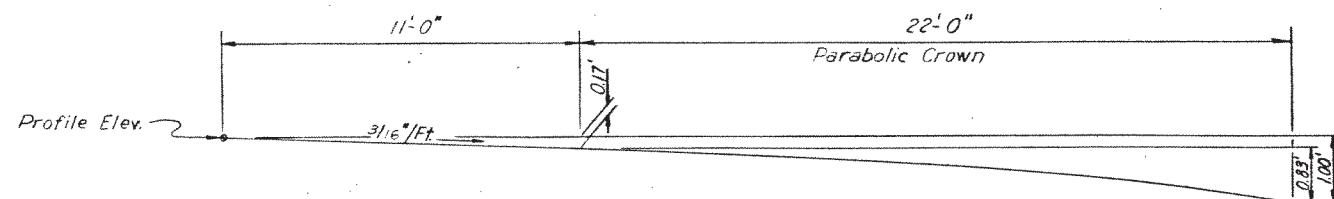
Showing the respective maintenance responsibilities of the Texas Highway Department and the City of Dallas on State Highway 356.

Approved APRIL 21 1961

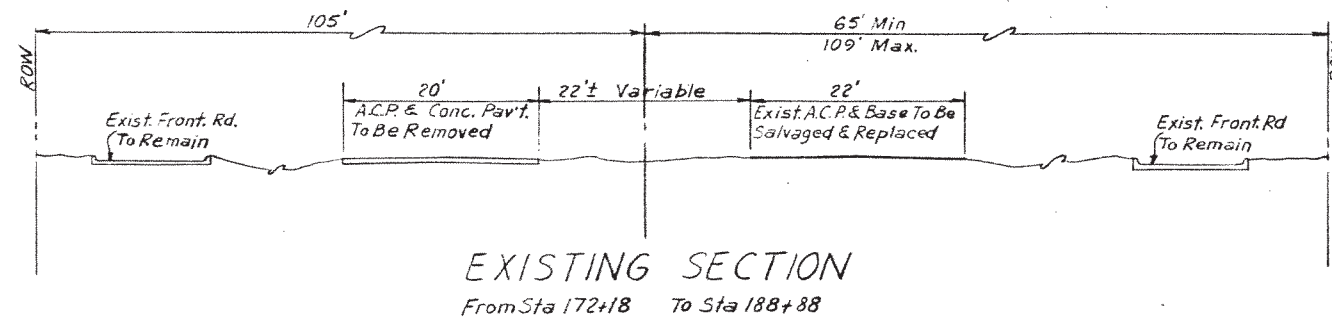
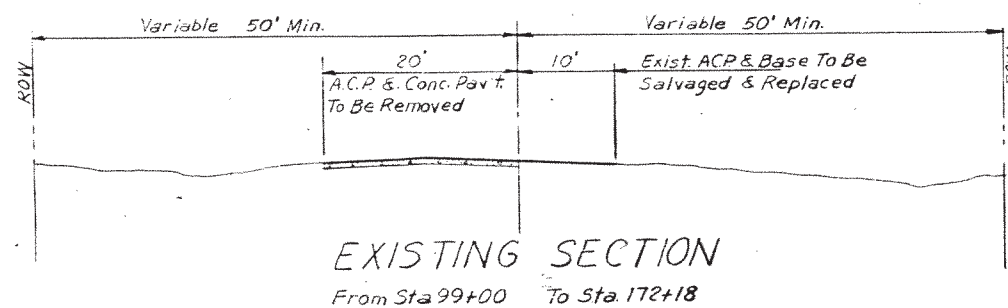
*[Signature]*  
Director of Public Works

Approved APRIL 21 1961

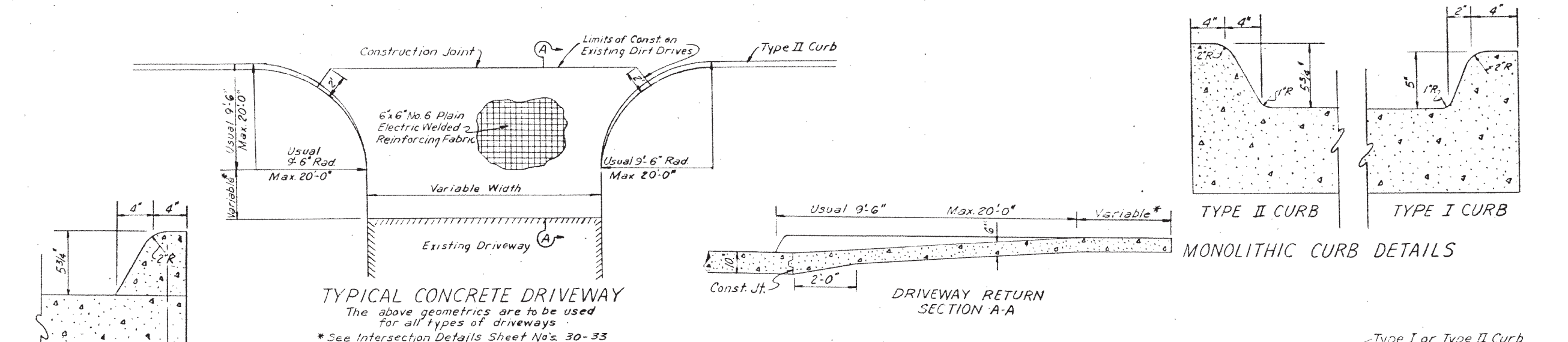
*[Signature]*  
Asst. City Manager



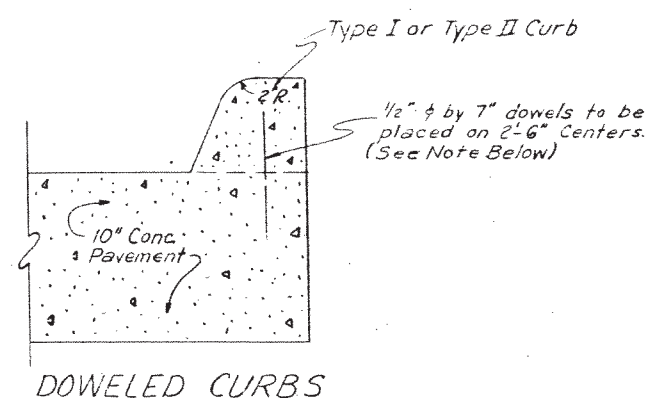
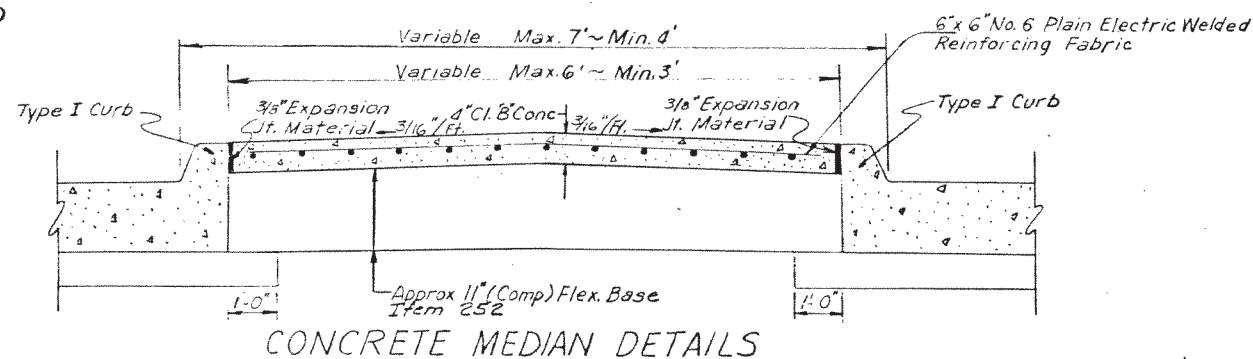
CROWN DIAGRAM



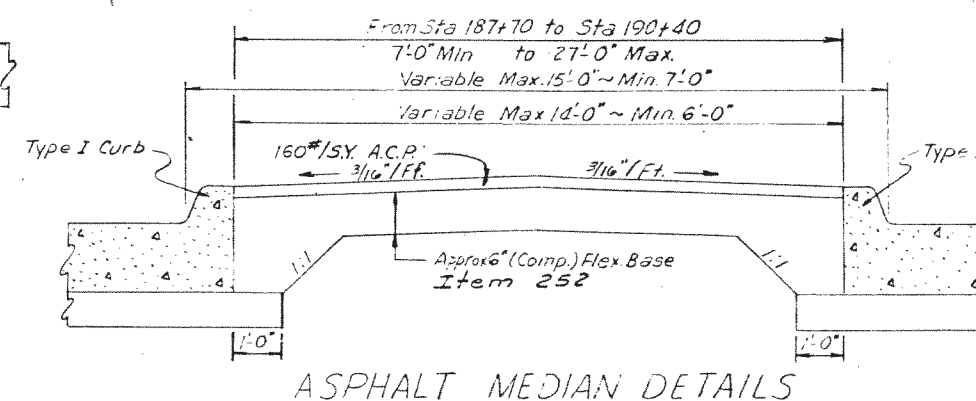
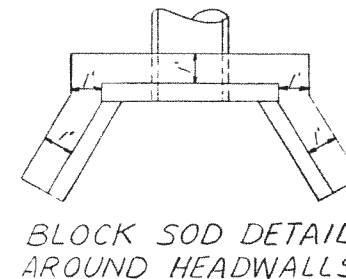
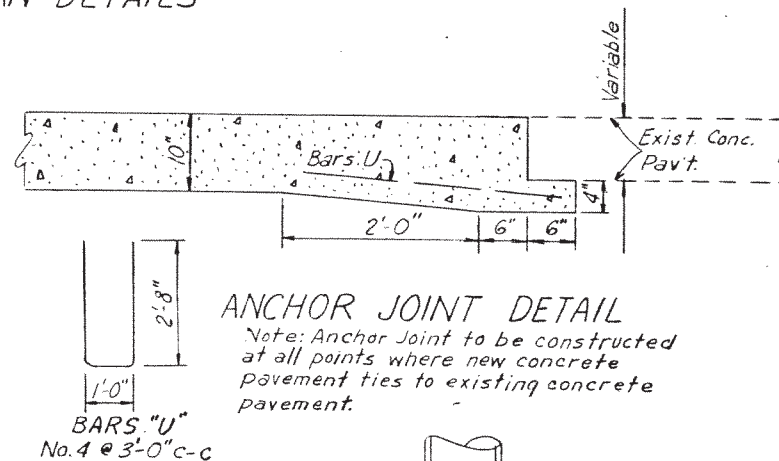
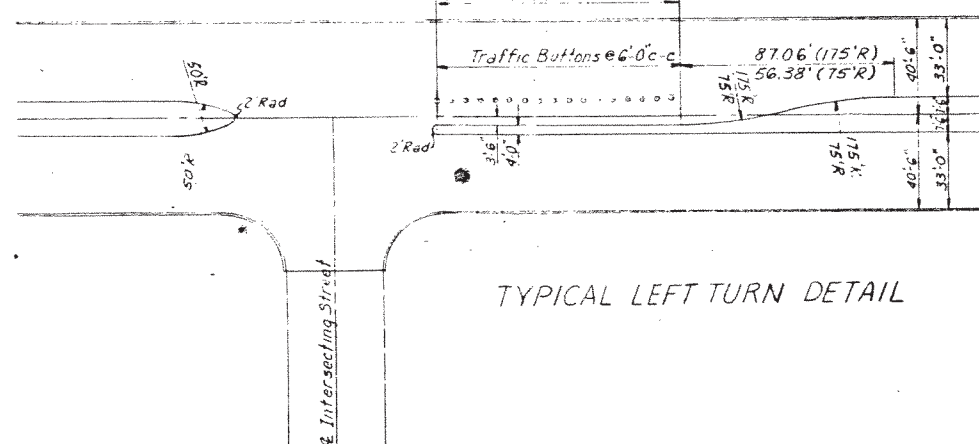
TYPICAL SECTIONS



CONC. CURB DOWELED TYPE III (MOD.)



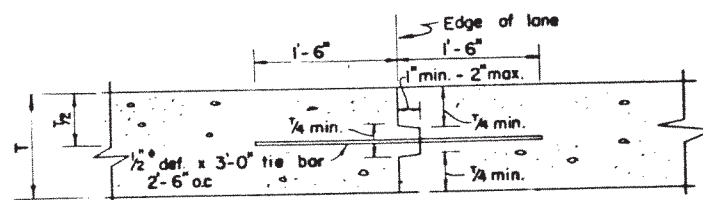
- 52' - 6 Traffic Buttons
- 40' - 7 Traffic Buttons
- 43' - 8 Traffic Buttons
- 46' - 8 Traffic Buttons
- 49' - 9 Traffic Buttons
- 50' - 9 Traffic Buttons
- 53' - 9 Traffic Buttons
- 60' - 11 Traffic Buttons
- 80' - 14 Traffic Buttons
- 85' - 15 Traffic Buttons
- 100' - 17 Traffic Buttons
- 200' - 34 Traffic Buttons



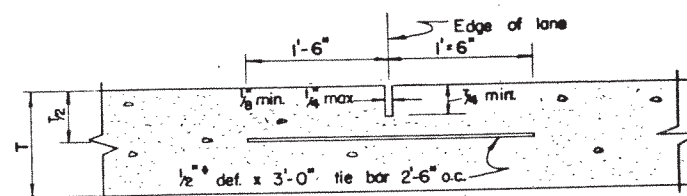
TYPICAL SECTIONS



## LONGITUDINAL JOINTS

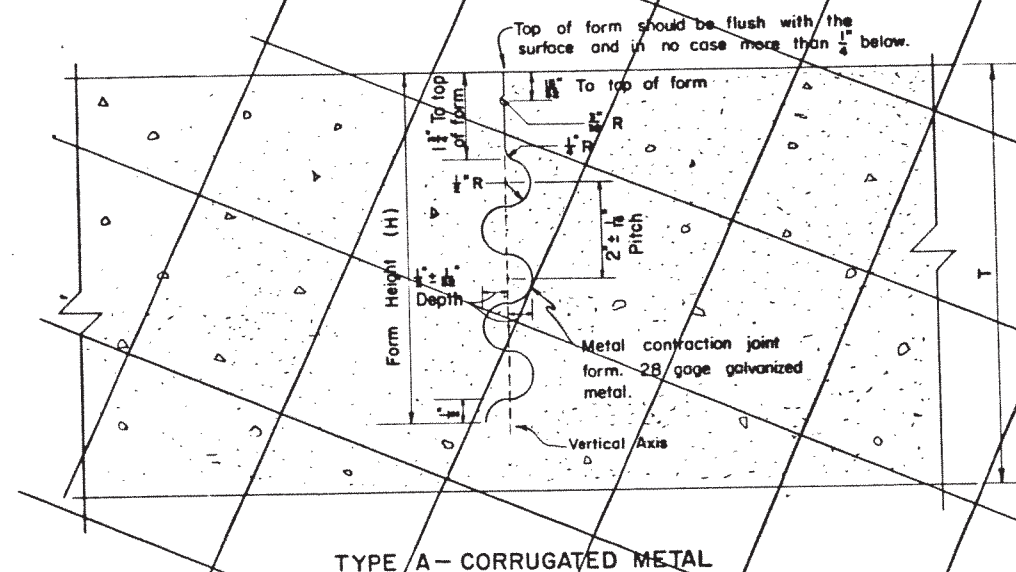


**TYPE 1 - CONSTRUCTION**  
(Tongue and Groove with Tie Bars)

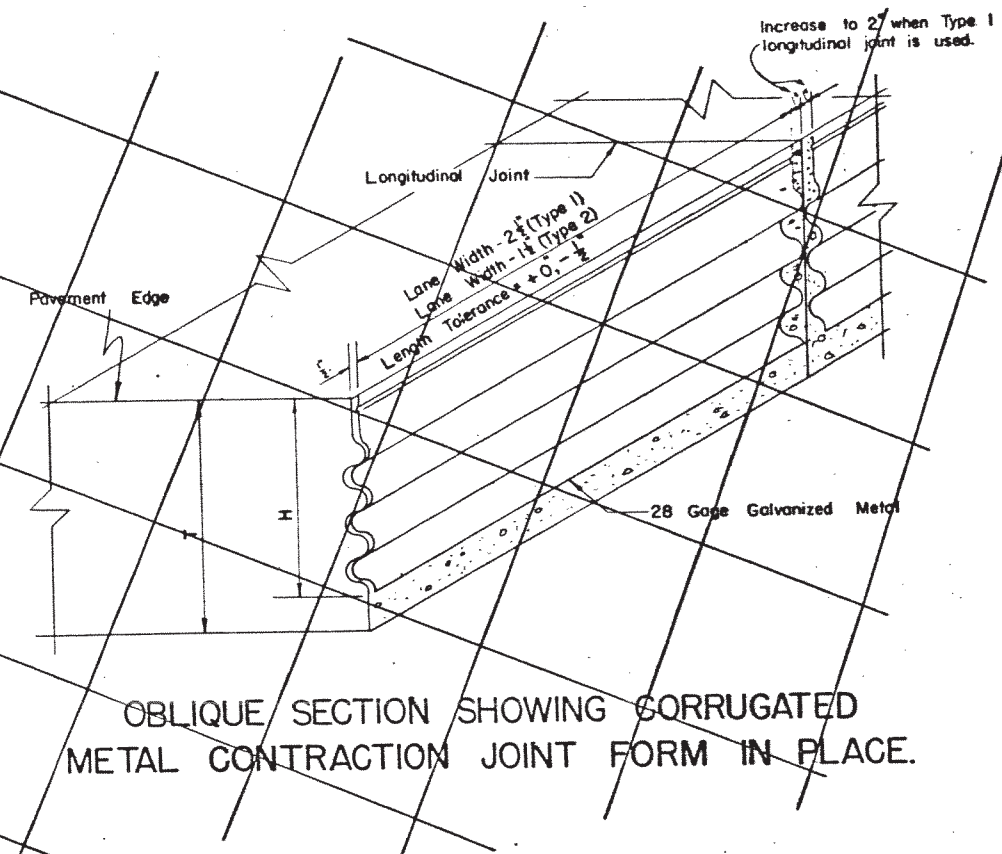


**TYPE 2 - GROOVED**  
(Sawed with Tie Bars)

## TRANSVERSE CONTRACTION JOINTS



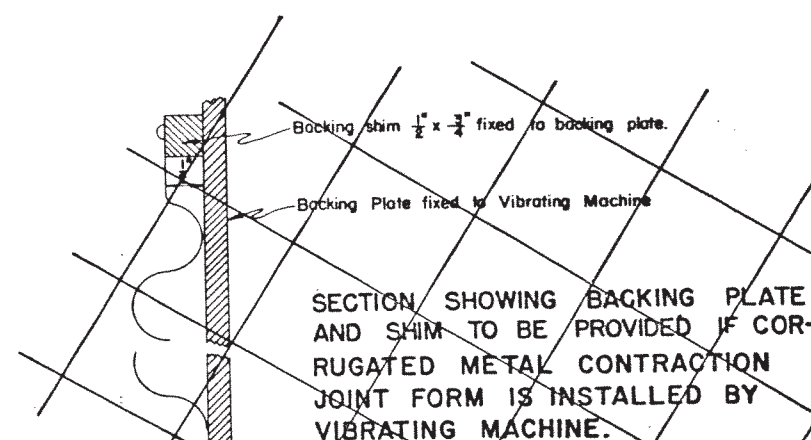
**NOTE:**  
Permissible Fabrication Tolerance - The semicircular segments shall be connected at a common point of tangency on the vertical axis of the joint or by a segment not exceeding one fourth of one inch ( $\frac{1}{4}$ ) in length at right angles to and symmetrical with the vertical axis of the joint.



**OBLIQUE SECTION SHOWING CORRUGATED METAL CONTRACTION JOINT FORM IN PLACE.**



**TYPE B - GROOVED**  
(Sawed with Coated Dowels)



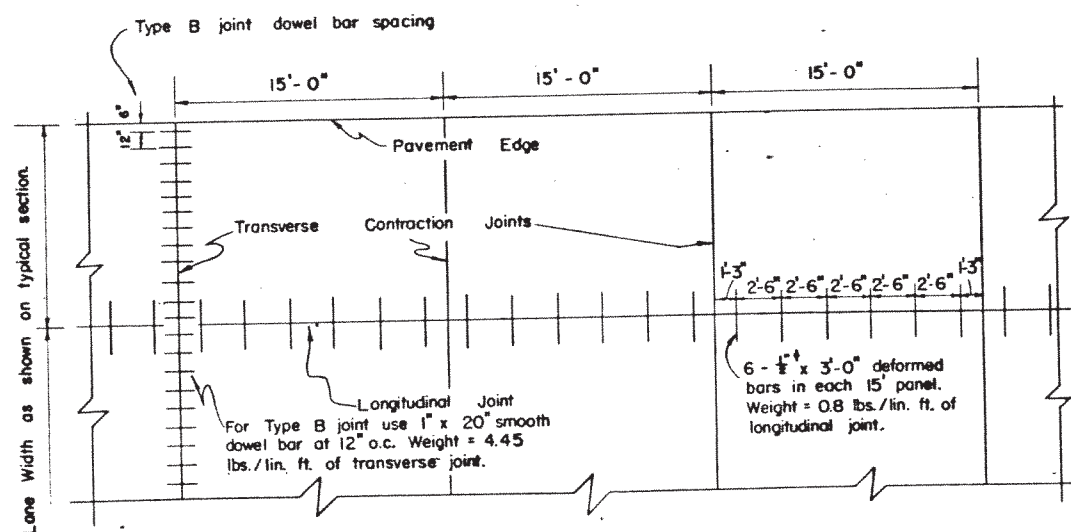
**TABLE OF FORM HEIGHTS (H)**

SLAB THICKNESS (T) (in.)	6	8	9	10	11	12
FORM HEIGHT (H) (in.)	5 1/4	5 1/4	5 1/4	6 1/4	7 1/4	8 1/4

The values shown above are applicable when form is installed by machine. When form is staked out in advance of concrete placement, these values are to be considered as minimum heights. \* Cannot be placed by machine.

### General Notes:

1. Crown, pavement width, and pavement thickness shall be as shown on typical sections elsewhere in plans. Where more than one section is shown, the Type 1 longitudinal joint shall be used as a construction joint.
2. Expansion joints shall be provided only at structure ends as shown elsewhere in plans.
3. The weakened plane in Types 1, 2, and B shall be true to line, vertical, and of depth shown. For Types 2 and B, the weakened plane shall be formed by sawing by an approved machine and filling with rubber joint sealing compound.
4. The contractor shall hold and save the State, its agents, and its employees harmless to liability of any kind, including costs and expenses, for or on account of any patent or unpatented invention, article, appliance, manufactured or used in accordance with the details of these plans.
5. Type 1 longitudinal joint may be formed by a metal form, wood form, or other means, which prior to its use, has been approved by the Engineer.
6. Where monolithic curb is specified, the joint in the curb shall coincide with pavement joints and be formed by any means which, prior to its use, has been approved by the Engineer.
7. Tie bars shall be secured parallel to the pavement surface and perpendicular to the center line by a bar chair or accurately placed on the screeded concrete by means of an approved template and forced to the proper position with suitable tool, or any of which prior to its use, has been approved by the Engineer.
8. Dowel bars shall be secured parallel to the pavement surface and centerline by a dowel bar chair or placed by a machine which accurately position the bars by vibrating them into the full depth concrete slab prior to the pass of the final finishing equipment.
9. When work is stopped due to breakdown or other cause, concrete shall be removed beyond last contraction joint in place and head.
10. Unless otherwise directed by the Engineer, all curbs will be monolithic.



**PLAN VIEW OF LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS.**

TEXAS HIGHWAY DEPARTMENT  
CONCRETE PAVEMENT  
CONTRACTION DESIGN  
CPCD-61 (MODIFIED)

FED. RD. DIV. NO.	STATE	PROJECT
8	TEXAS	C 92-7-32
STATE DIST. NO.	COUNTY	CONTRACT NO.
18	Dallas	92