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FEDERAL AID PROJECT.
135-5051 403
PLAN: 1 IN. = 100 FT.
PROFILE: 1 IN. HOR. = 100 FT. 1 IN. VERT. = 10 FT.
SCALES: CROSS-SECTIONS: 1 IN. HOR. AND VERT. = 5 FT.
OTHERS AS NOTED.
NET LENGTH OF PROJECT = 51,385.4 FT. = 166.04 MI.

DENTON COUNTY

FROM COOKE COUNTY LINE TO 2.4 MILES NORTH OF S.H. 24
CONCRETE PAVEMENT, FLEXIBLE BASE & A.C.P. & 2 CRS. SURF TREAT.

CLASS AT 11:30 AM
L. G. S. - 75 AF -

GENERAL NOTES:
See attached sheets BW-541182 for Barrisade, &
warning sign locations. Other signs seem to may
be deemed to be English, to be understood, to
relieve the public shall be placed at locations
approved by the Engineer.

The α and β rays are in effect the same as the γ rays. The nature of these rays are given the intrinsic edge of the particle structure.

APPROVED: 1-14 1 1958

P. Wilson
MAYOR - CITY OF SANGE

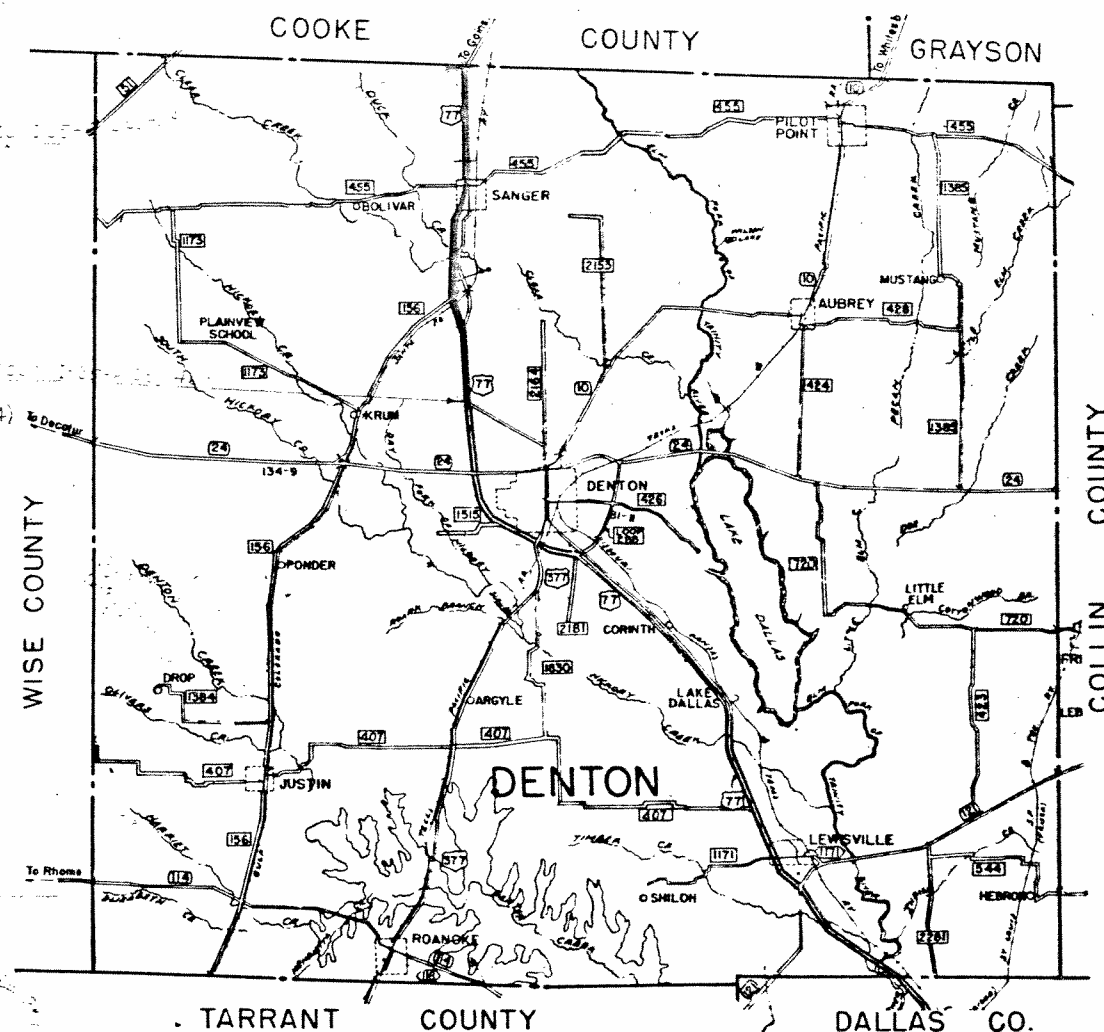
STATE HIGHWAY DEPARTMENT

1-29-5

127 12

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS

APPROVED _____
DESIGN ENGINEER



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14	CURB OPENING DETAILS
15	R-10-40 MIX
16	R-12A-40 MIX
17	R-11
18	CH-11.45
19	SW-12
20	GP-30
21-22	BR-14-10

FEDERAL AID PROJECT.

13E-5051 AF2

PLAN: 1 IN. = 100 FT.
 PROFILE: 1 IN. HOR. = 100 FT. 1 IN. VERT. = 10 FT.
 CROSS-SECTIONS: 1 IN. HOR. AND VERT. = 5 FT.
 OTHERS AS NOTED.

NET LENGTH OF PROJECT = 53,395.4 FT. = 10.624 M

DENTON COUNTY












1. DATE 04-08-2014
 2. TIME 05-2-14
 3. NAME [illegible]

7-10-68
 7-10-68
 7-10-68

LETT ONE

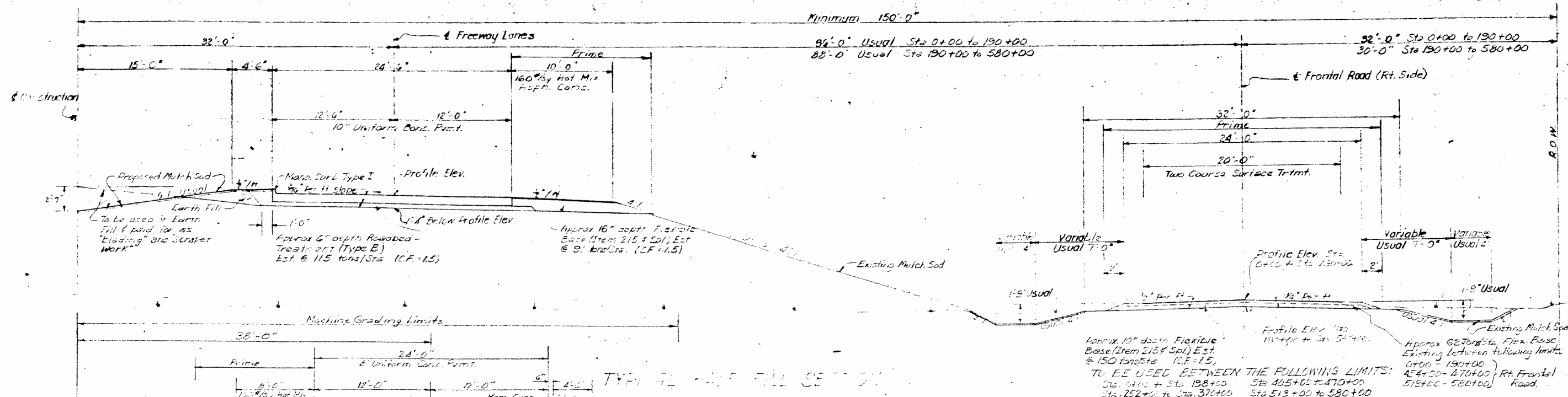
1. 215 + 325 Br - 325 + 215 Ar = - 225 Br
 2. 350 + 015 Br - 350 + 36 Ar = - 500 Br
 3. 350 + 001 Br - 450 + 950 Ar = - 550 Br
 4. 350 + 000 Br - 350 + 000 Ar = - 500 Br

TOTAL EXCEPTIONS = 1972.64 FT

CONVENTIONAL SIGNS	
STATE OR NATIONAL LINE	
OR VILLAGE LINE	
COUNTY LINE	
OR SURVEY LINE	
OUT OF WAY LINE	
OUT OF WAY BARRIERS	
RAILROAD	
UNPAVED ROAD	
INVERT OR BRIDGE	
POWER LINE	
TELEGRAPH OR TELEPHONE	

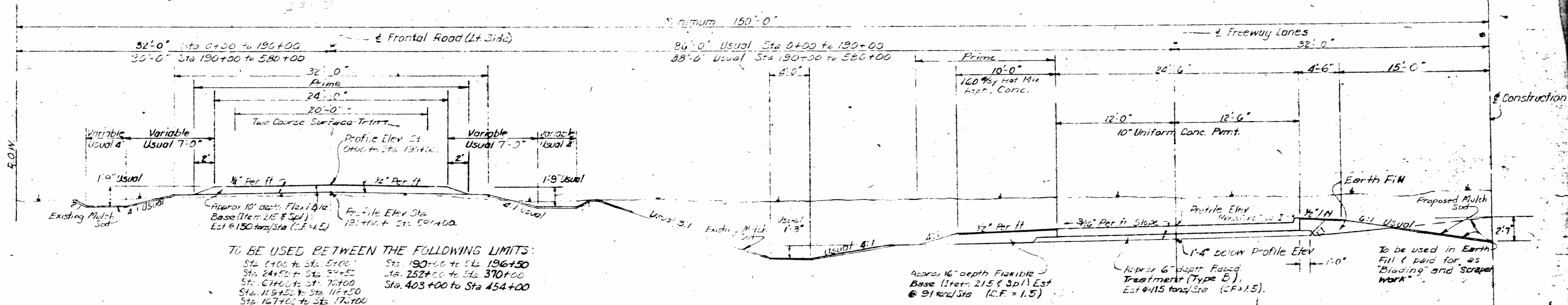
0 1 2 3 4 5 6 7 8 9 10
SCALE IN MILES

COUNTY Durham PROJ. NO. 12-1-10
 HWY. NO. 101 LETTING DATE 12-1-10
 DATE ACCEPTED 12-1-10



Note: All curves shall be left frontal road that have been used for the 1-35 section shall receive a two course surface treatment after the traffic is diverted to the freeway lanes.

It shall be the responsibility of the Engineer to establish the grade in the field. The Frontal Road grade shall be such that the difference in elevation of frontal road finished grade and adjacent property shall be 12 inches or less.



Note: The Contractor will be required to provide temporary connections at each end of the project between the freeway lanes and the existing highway. The geometrics and locations will be as directed by the Engineer. The connections will consist of a 24' roadway of 100% 1.5:1 Hot Mix Asphalt Conc. Pmt. on approx. 12" Flex. Base (Item 215 Sp.) Est. All work involved shall be measured and paid for under the various bid items involved with the exception of any drainage structures necessary, which shall be considered subsidiary to the other items of work involved.

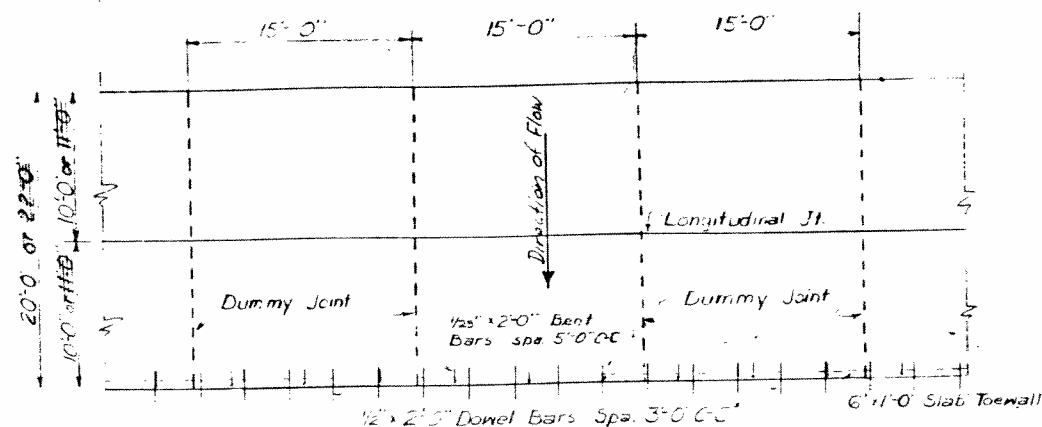
Mulch Sod to be replaced on regraded sections. All area with existing or proposed Mulch Sod to be seeded. All curves shall be superelevated in accordance with SWC 39 at rate of super-elevation and super-elevation runoff as given in A.A.S.H.O., "A Policy on Geometric Design of Rural Highways", 1954.

Maximum rate of super-elevation shall be 0.05' per ft. Frontal Road Curves to be super-elevated in accordance with Table II of SWC 39 or as directed by the Engineer.

TYPICAL SECTION GRADE SEPARATION CONNECTIONS
 (Sta 0+00 to Sta 190+00)

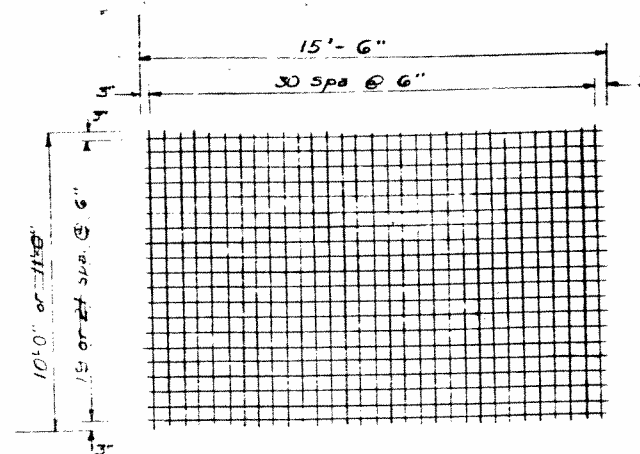
TYPICAL SECTIONS

STATE	COUNTY	CONTRACT NO.	SHEET NO.	TOTAL SHEETS
TX	DENTON	1-35-6 (2.3)	463	2
18				



NOTE
Dowel Bars to be used to anchor Expansion and Turnarounds.

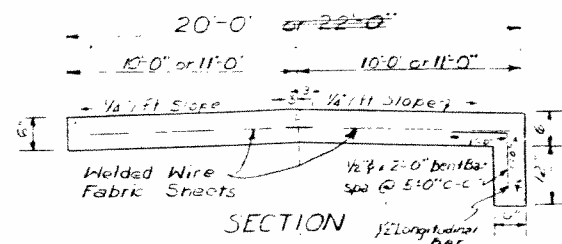
Welded Wire Fabric Reinforcement with 6" overlap across Longitudinal and Dummy Joints



WELDED WIRE FABRIC SHEET

Transverse wire No. 6 ga. @ 6"
Longitudinal wire No. 6 ga. @ 6"
Est. 31 Lb. per Sq. Yd. of Pavement

* TO BE USED BETWEEN THE FOLLOWING LIMITS:
Left Frontal Rd. Sta 334+00 to 339+00
Right Frontal Rd. Sta 334+50 to 339+50



GENERAL NOTES.

Transverse Dummy joints, Construction joints and Longitudinal joints shall be constructed in accordance with joint details shown.

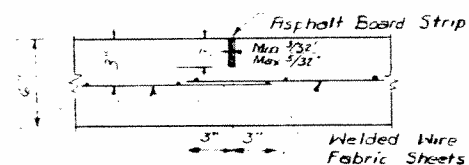
Construction joints shall be constructed at the end of each days pour or when pouring operation is interrupted for any reason.

The cost of furnishing and placing of welded wire Fabric Sheets, Dowels, and Joint material shall be included in Unit Price Bid per Cubic Yard for "Concrete Pavement (Class 'A' Concrete)"

Either wood or steel Forms may be used.

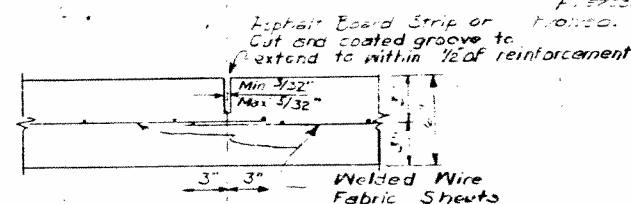
Either cotton mats or waterproofed paper or membrane Curing/Sprayed by hand, may be used.
Hand finishing of Slab will be permitted.

As specified required for the material shall be considered subsidiary to this other items of work.



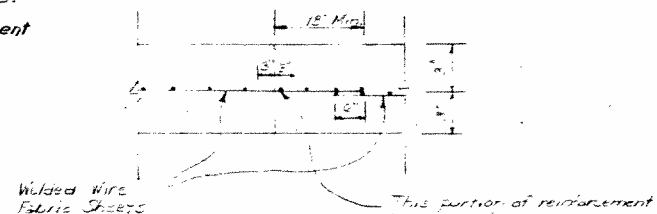
Asphalt Board Strip held in an approved continuous metal shield shall be placed continuously by an approved mechanical device operating in advance of the longitudinal float

DUMMY JOINT

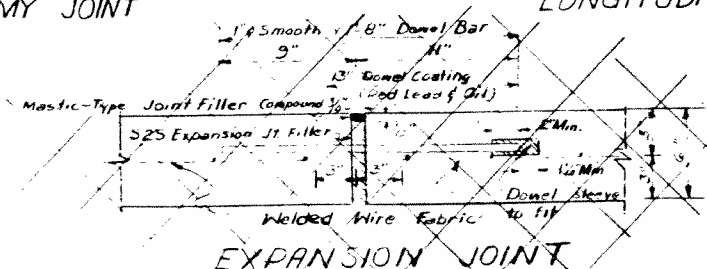


Top groove shall be cut by an approved machine and the vertical faces of the concrete coated with an approved concrete curing compound before closing and final finishing

LONGITUDINAL JOINT



TRANSVERSE CONSTRUCTION JOINT



EXPANSION JOINT

CONCRETE PAVEMENT DETAILS

FILE NO.	STATE	FEDERAL PROJECT NO.	SHEET NO.
6	TEXAS	1-35-6(25) 463	12
STATE	COUNTY	CONTRACT NO.	SHEET NO.
TX	DENTON	19A 2 16	14/35