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STATE OF TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

RES. NO. 051

DESIGN SPEED 70

STATE	COUNTY	FEDERAL AID PROJECT NO.	SHEET NO.
TEXAS	CHAMBERS	MA-IR 10-8 (132) 813	1
20	CHAMBERS	508-3-62	1H10

PLANS OF COMPLETED STATE HIGHWAY IMPROVEMENT

FEDERAL AID PROJECT.

MA-IR 10 - 8 (132) 813

IH 10 CHAMBERS COUNTY CONTROL 508-3-62

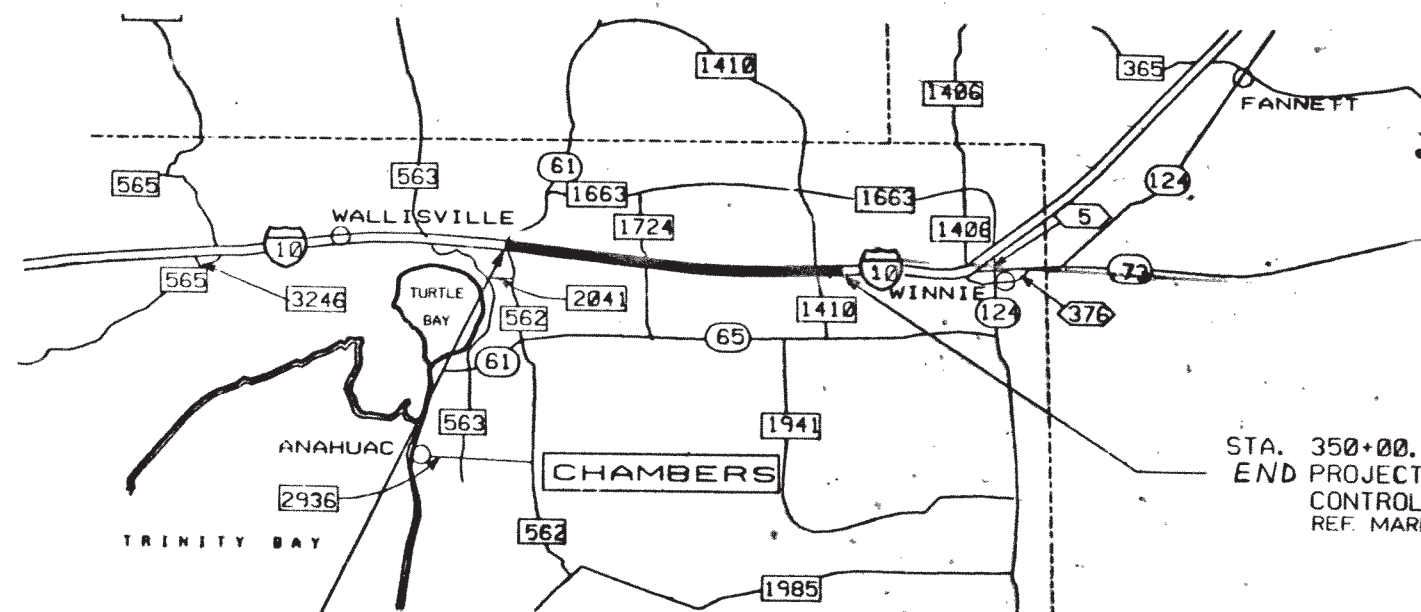
LIMITS: FROM SH 61
TO 0.887 MI EAST OF FM 1410

TYPE: RECONSTRUCT ROADWAY

8092
05

NET LENGTH OF PROJECT

ROADWAY:	56,846.28 FT.	18.766 MI
BRIDGES:	153.72 FT.	0.029 MI
TOTAL:	57,000.000 FT.	18.795 MI



STA. 350+00.00 = FM 1406
END PROJECT MA-IR 10-8 (132) 813
CONTROL 508-3-62
REF. MARK. 822.186

STA. 920+00.00
BEG. PROJECT MA-IR 10-8 (132) 813
CONTROL 508-3-62
REF. MARK. 812.981

PROJECT CONSTRUCTED AND FINAL
PLANS PREPARED BY:
Quinn S. Blawie 12/15/92 DATE
DATE WORK BEGAN: 11-1-88
DATE WORK COMPLETED: 8-31-92
CONTRACTOR: WILLIAMS BROTHERS CONSTRUCTION
PROJECT COST: \$3,919,328.69

For Field Changes and
Extra Work Orders:
See SHS 2B+2C.

NO R.R. CROSSINGS
NO EXCEPTIONS
NO EQUATIONS

THE CONTRACTOR SHALL PROVIDE AND ERECT BARRICADES AND
CONSTRUCTION SIGNS IN ACCORDANCE WITH BC (1-7)-88 AND
THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AT
POINTS AS SHOWN ON THE TITLE SHEET AND PLAN SHEETS AND
AS DIRECTED BY THE ENGINEER.

SPECIFICATIONS ADOPTED BY THE STATE DEPARTMENT OF HIGHWAYS
AND PUBLIC TRANSPORTATION SEPTEMBER 1, 1982 AND SPECIFICATION
ITEMS, LISTED AND DATED AS FOLLOWS, SHALL GOVERN ON THIS PROJECT;
REQUIRED CONTRACT PROVISIONS, FEDERAL-AID CONSTRUCTION CONTRACTS
(FORM FHWA 1275, OCTOBER 1987).

LAYOUT SCALE: 1 IN. = 3.0 MI.

STATE DEPARTMENT OF HIGHWAYS
AND PUBLIC TRANSPORTATION

CORRECT: 12/15/92

APPROVED

DISTRICT DESIGN ENGINEER

RECOMMENDED FOR APPROVAL: 6-16-98

APPROVED FOR LETTERS

DISTRICT ENGINEER

RECOMMENDED FOR APPROVAL

APPROVED FOR LETTERS

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REV. 8-24-88
REV. 8-15-88

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

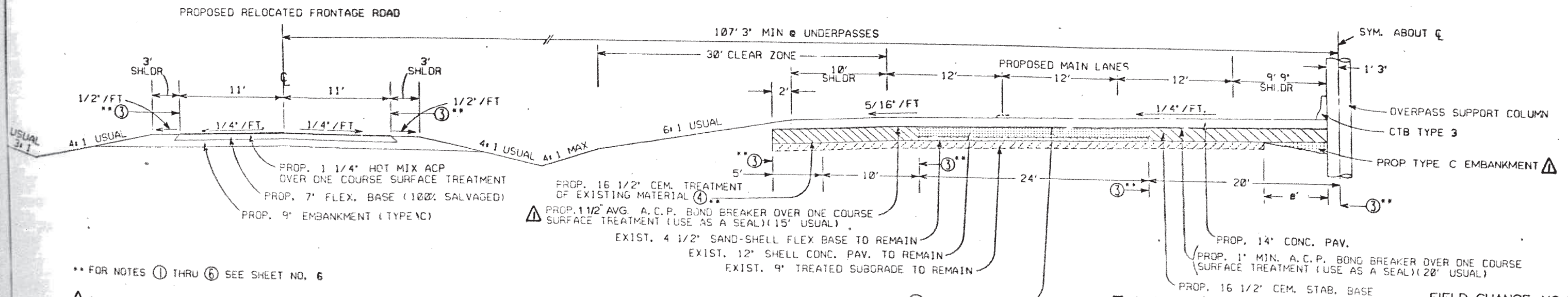
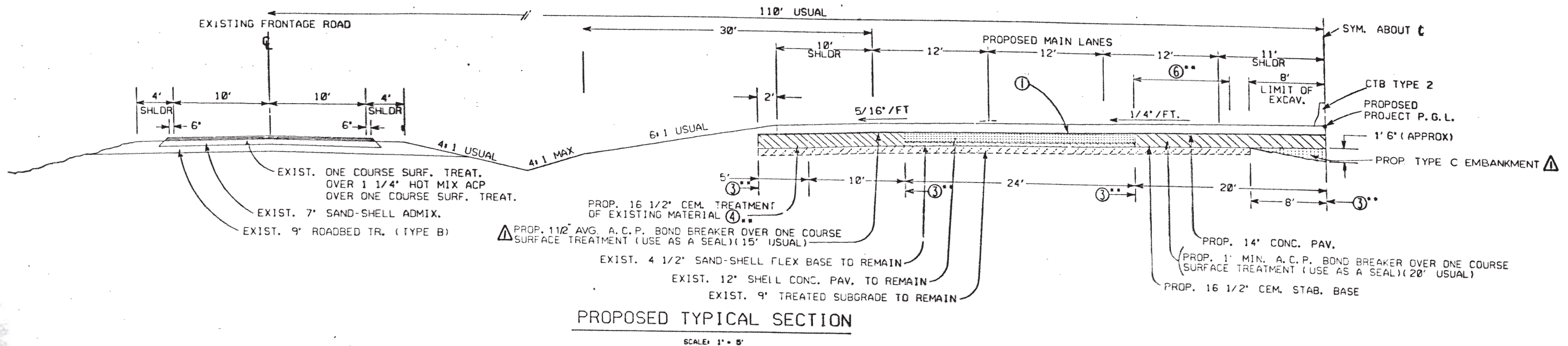
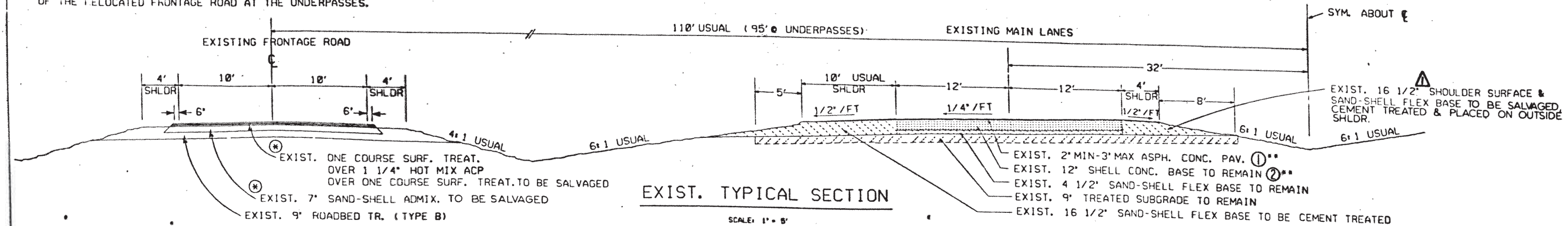
APPROVED

DIVISION ADMINISTRATOR

DATE

274b (32+100) 11/25/88

(*) TO BE 100% SALVAGED AND USED FOR PROPOSED 7" FLEX BASE OF THE RELOCATED FRONTAGE ROAD AT THE UNDERPASSES.



** FOR NOTES ① THRU ⑥ SEE SHEET NO. 6

△ CHANGES AS PER F.C. 1

PROPOSED TYPICAL SECTION AT UNDERPASSES

SCALE: 1" = 5'

R. 3-15-08

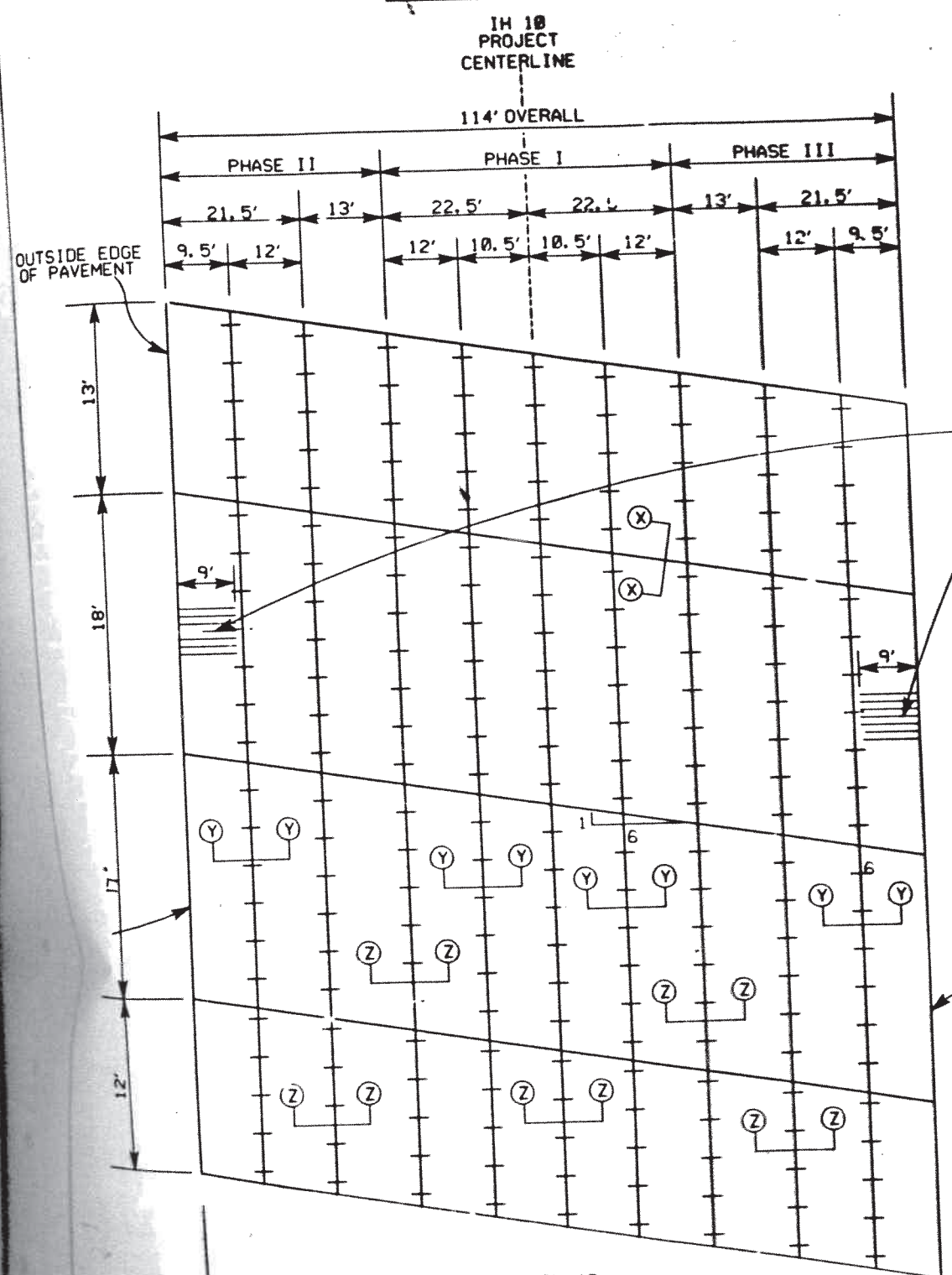
IH 10
TYPICAL SECTIONS

FIELD CHANGE NO. 1
SHEET 1 OF 4

FED. ROAD DIST. NO.	FED. AID PROJECT NO.
6	MA-IR 10-B(132) B13
STATE	COUNTY
TEXAS	CHAMBERS
DATE	EDT. DATE
20	02/23/08
DESIGNED BY	DRAWN BY

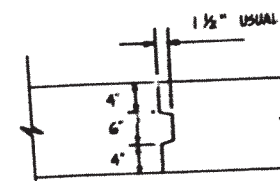
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PAVEMENT PLAN

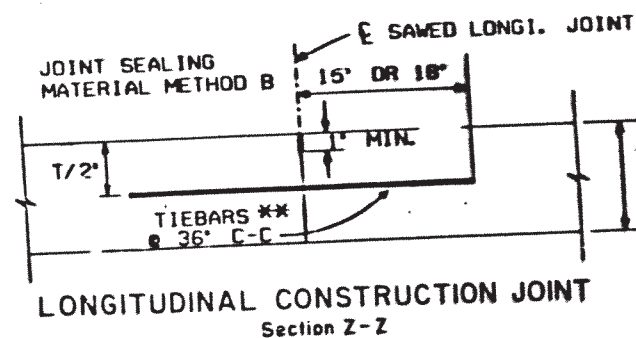
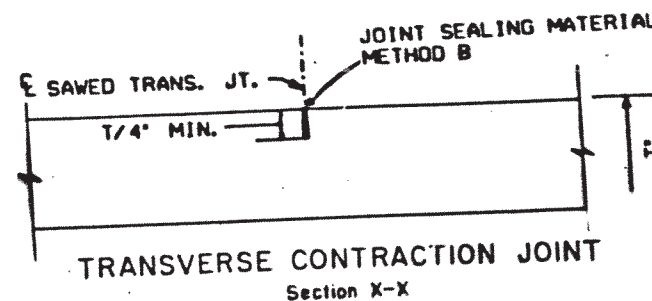


PROPOSED ONE RUMBLE STRIP PER EACH 18' SHOULDER SLAB. THERE WILL BE NO DIRECT PAYMENT FOR RUMBLE STRIPS.

TRANSVERSE CONSTRUCTION HEADER JOINT



(SEE NOTE # 7)

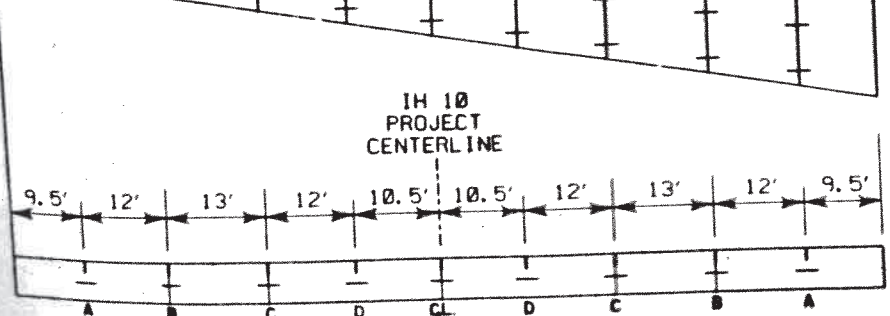


** WITH THE APPROVAL OF THE ENGINEER, MULTIPLE PIECE TIEBARS (THREADED COUPLING OR OTHER ADEQUATE DEVICE) MAY BE USED TO FACILITATE CONSTRUCTION PROVIDED THE SYSTEM DEVELOPS A FORCE EQUAL TO 1 1/2 TIMES THE MINIMUM FORCE OF THE TIEBAR SHOWN. THE SPACINGS FOR THE SYSTEM SHALL BE LESS THAN OR EQUAL TO THE SPACING ALLOWED FOR BARS OF SIMILAR YIELD STRENGTH.

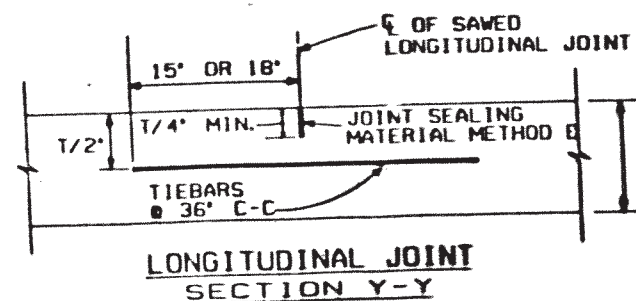
OUTSIDE EDGE OF PAVEMENT

TIEBAR SIZES AND SPACING

- #5 BARS - 36" SPACING AT D
- #5 BARS - 30" SPACING AT C
- #5 BARS - 30" SPACING AT B
- #5 BARS - 36" SPACING AT A
- #5 BARS SHALL BE 36" IN LENGTH



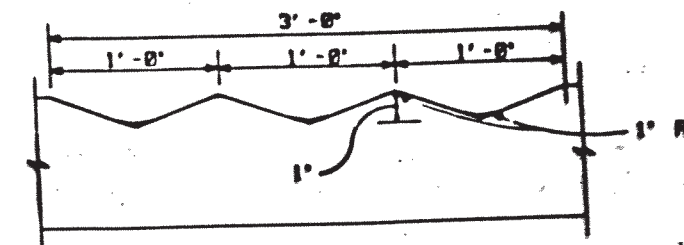
TYPICAL SIX LANE SECTION WITH SHOULDERS*



REV. 8-15-88

GENERAL NOTES

- NO EXPANSION JOINTS WILL BE USED EXCEPT AT STRUCTURE ENDS OF FIXED OBJECTS AS SHOWN FIRSTHAND IN THE PLANS.
- FOR FURTHER INFORMATION REGARDING THE PLACEMENT OF CONCRETE REFER TO THE GOV-ERNING SPECIFICATIONS FOR "CONCRETE PAVEMENT".
- DETAILS AS TO PAVEMENT WIDTH, PAVEMENT THICKNESS, AND THE CROWN CROSS-SLOPE SHALL BE SHOWN ELSEWHERE IN THE PLANS.
- JOINT SEAL DETAILS SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
- TIEBARS SHALL BE SECURED PARALLEL TO THE PAVEMENT SURFACE AND PERPENDICULAR TO THE CENTERLINE BY:
 - USE OF BAR CHAIRS
 - ACCURATELY PLACED IN POSITION ON THE SCREEDED CONCRETE BY MEANS OF AN APPROVED TEMPLATE AND FORCED TO THE PROPER POSITION WITH A SUITABLE TOOL OR
 - BY ANY OTHER MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- TIEBAR SHALL NEVER CROSS SKewed TRANSVERSE JOINTS. TO INSURE THIS THE CON-TRACTOR MAY VARY THE SPACING OF TIEBARS NEAR THESE JOINTS ± 6 INCHES.
- WHEN WORK IS STOPPED DUE TO BREAKDOWN OR OTHER CAUSE, CONCRETE SHALL BE REMOVED BEYOND LAST CONTRACTION JOINT IN PLACE AND A HEADER INSTALLED.
- WHERE A MONOLITHIC CURB IS SPECIFIED, THE JOINT IN THE CURB SHALL COINCIDE WITH PAVEMENT JOINTS AND MAY BE FORMED BY MEANS WHICH, PRIOR TO ITS USE, HAS BEEN APPROVED BY THE ENGINEER.
- CONSTRUCTION JOINTS MAY BE FORMED BY USE OF METAL OR WOOD FORMS EQUAL IN DEPTH TO THE NOMINAL DEPTH OF THE PAVEMENT, OR BY OTHER MEANS WHICH HAVE BEEN APP-ROVED BY THE ENGINEER PRIOR TO THEIR USE.
- STEEL SPACING SHALL NOT VARY MORE THAN ONE TWELFTH OF THE SPACING SHOWN HERE.
- THE TIEBAR SPACINGS SHOWN ARE FOR ASTM DESIGNATIONS: A-615, OR A-616, GRADE TIEBARS, WHICH SHALL NOT BE BENT.
- JOINT SPACING OF 13'-18'-17'-12' SHALL BE A REPEATED PATTERN WHERE THE SK-ED CONCRETE PAVEMENT IS TERMINATED THE END OF THE LAST SLAB SHALL BE PERPENDI-TO THE CENTERLINE OF THE ROADWAY. THE SPACING OF THE TRANSVERSE JOINTS WILL ADJUST SO THAT THE MINIMUM SLAB LENGTH WILL BE 8 FEET.
- THE LONGITUDINAL AND TRANSVERSE JOINT SHALL BE SAWED. JOINT SEALING MATERIAL METHOD "B" SHALL BE USED AS SHOWN ON THE STANDARD JS-75.
- ALL JOINTS TO BE FILLED WITH CLASS 5 (LOW MODULAR SILICONE SEALANT) AS APP-ROVED BY THE ENGINEER.



TYPICAL RUMBLE STRIP

NOTE: RUMBLE STRIP SECTIONS MAY BE VARIED WITH WRITTEN APPROVAL OF THE ENGINEER.

MODIFICATIONS

- REVISED PAVEMENT PLAN
- ADDED TRANSVERSE CONST. HEADER JOINT
- ADDED RUMBLE STRIPS
- ELIMINATED METHOD "A" JOINT SEALING
- SPECIFY JOINT SEALANT
- ADDED TIEBAR SIZES & SPACING
- DELETED NOTE ALLOWING BENDING OF TIEBARS

STATE DEPARTMENT OF HIGHWAY AND PUBLIC TRANSPORTATION

CONCRETE PAVEMENT DATA

SKewed TRANSVERSE JOINTS (NO DOWELS)

CPSJ-75 (MOD)

FED. ROAD DIST. NO.	FEDERAL AID PROJECT NO.
6	10-8 (132) 8/3
STATE	COUNTY
20	CHAMBERS
TERMS	CON. SECT.
508	3

