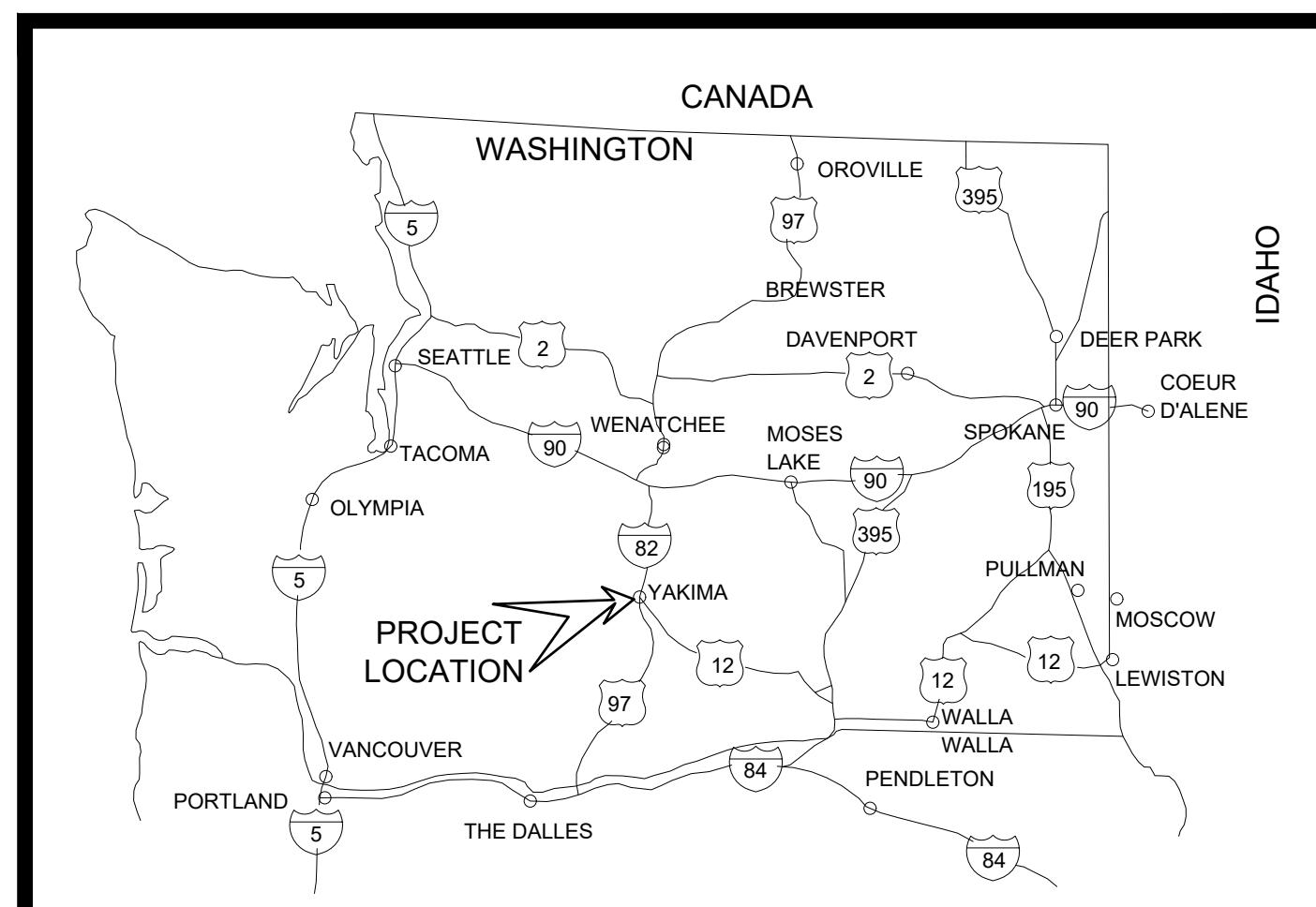


YAKIMA AIR TERMINAL / MCALLISTER FIELD

YAKIMA, WASHINGTON



AREA MAP



VICINITY MAP

OWNER
CITY OF YAKIMA
129 N SECOND ST
YAKIMA, WA 98901

TAXILANE CHARLIE EXTENSION

PROJECT NO. 45-21-021

J-U-B ENGINEERS, INC.

W. 422 Riverside Avenue, Suite 304, Spokane, WA 99201
p 509 458 3727 f 509 458 3762 w www.jub.com

OTHER J-U-B COMPANIES



THE LANGDON GROUP



GATEWAY MAPPING INC.

Know what's below.
Call before you dig.

CALL 2 BUSINESS DAYS IN ADVANCE BEFORE
YOU DIG, GRADE, OR EXCAVATE FOR THE
MARKING OF UNDERGROUND MEMBER UTILITIES

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| G-003 | GENERAL LEGEND |
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| E6.01 | FUTURE LAYOUT DIAGRAM |

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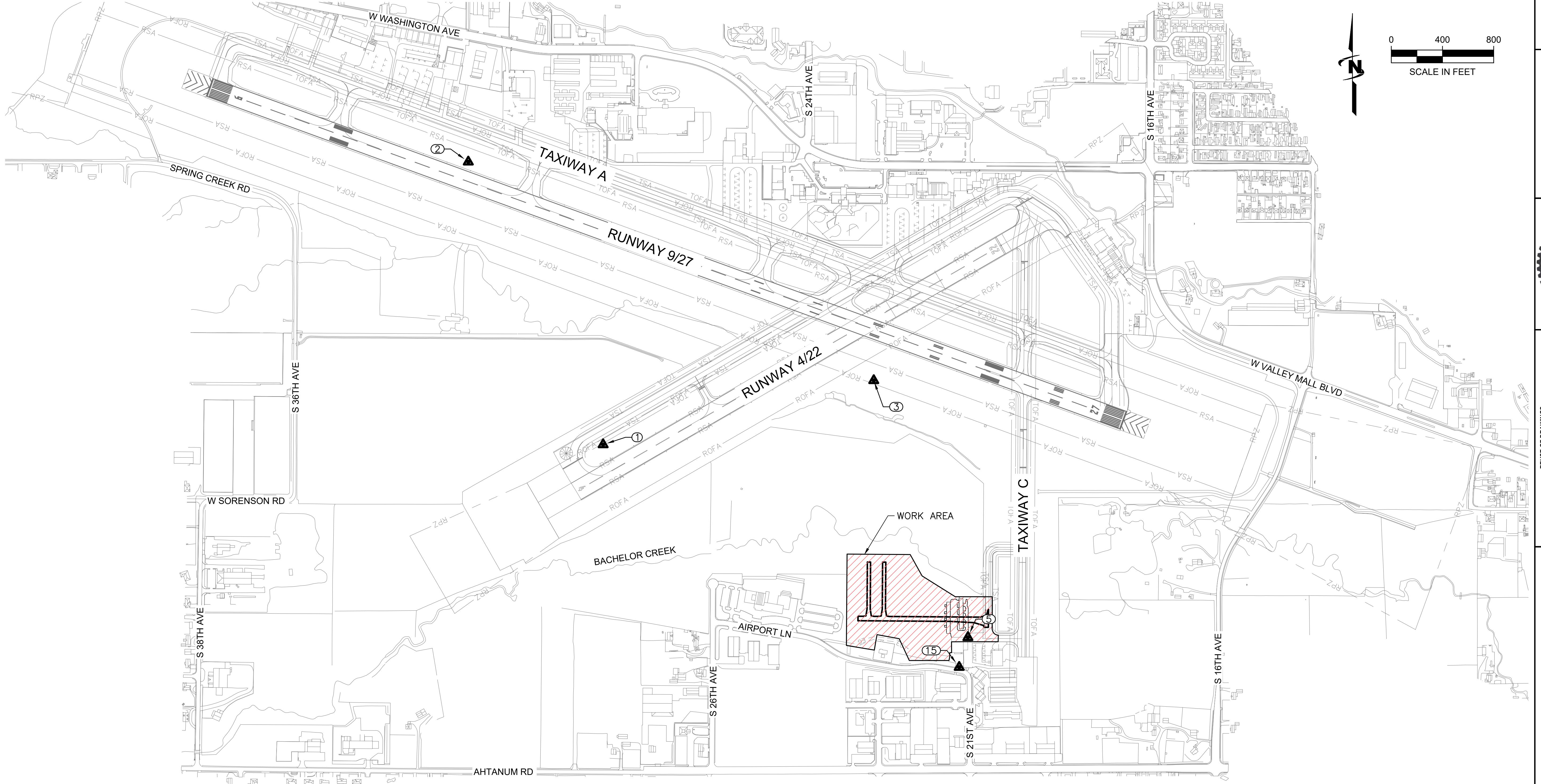
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LAST UPDATED: 2/9/2023

SHEET NUMBER:

G-001



| SURVEY CONTROL | | | | |
|----------------|-----------|------------|-----------|--|
| POINT # | NORTHING | EASTING | ELEVATION | DESCRIPTION |
| 1 | 449111.94 | 1628519.14 | 1074.08 | PACS-YKM D_PID AD9562_STAINLESS STEEL ROD |
| 2 | 451320.99 | 1627465.87 | 1084.54 | SACS-YKM AP STA A_PID AC5529_BRASS CAP |
| 3 | 449612.94 | 1630633.30 | 1058.50 | SACS-YKM C_PID AC5531_BRASS CAPAP STA A_PID C-BC |
| 5 | 447605.71 | 1631368.70 | 1055.00 | MAG NAIL IN ASPHALT |
| 15 | 447372.62 | 1631300.68 | 1053.94 | RB5/JUB CAP_5/8" IRON REBAR W/RED JUB CAP |

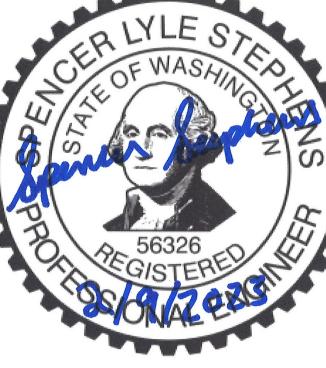
BASIS OF COORDINATES AND ELEVATIONS:

BASIS OF BEARINGS:
THE BASIS OF BEARINGS FOR THIS SURVEY IS THE 2011 ADJUSTMENT OF THE NORTH AMERICAN DATUM OF 1983 (NAD 83/2011, EPOCH 2010), WASHINGTON STATE PLANE COORDINATE SYSTEM, SOUTH ZONE (4602) BASED UPON PUBLISHED DATA SHEET VALUES FOR THE NATIONAL GEODETIC SURVEY (NGS) PRIMARY AND SECONDARY AIRPORT CONTROL STATIONS. ALL BEARINGS ARE GRID, ALL DISTANCES ARE GRID EXPRESSED IN US SURVEY FEET.

VERTICAL DATUM:
VERTICAL DATUM IS THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) USING GEOID MODEL 12B. BASED UPON THE PUBLISHED DATA SHEET ELEVATION FOR THE NATIONAL GEODETIC SURVEY (NGS) PRIMARY AIRPORT CONTROL STATION YKM D. THE PROJECT BENCH MARK IS YKM D (PACS). A STAINLESS-STEEL ROD IN A MONUMENT BOX, ELEVATION BEING 1074.08 FEET.

LEGEND

| | |
|------------------------|--|
| | WORK AREA LIMITS |
| — RSA — | RUNWAY SAFETY AREA |
| — ROFA — | RUNWAY OBJECT FREE AREA |
| — RPZ — | RUNWAY PROTECTION ZONE |
| — X — | EXISTING PERIMETER FENCE |
| ▲ | SURVEY CONTROL POINT |
| — ONE INCH — | AT FULL SIZE IF NOT ONE INCH SCALE ACCORDINGLY |
| LAST UPDATED: 2/9/2023 | |
| SHEET NUMBER: | G-002 |



J-U-B ENGINEERS, INC.
W. 422 Riverside Ave.
Suite 304
Spokane, WA 99201
Phone: 509.456.3727
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REVISION

NO. DESCRIPTION BY APR. DATE

TAXILANE CHARLIE EXTENSION
YAKIMA AIR TERMINAL - MCCALLISTER FIELD
AIP 3-53-0089-052-2002
GENERAL PLAN & SURVEY CONTROL

FILE #: 45-21-021_G-002
JUB PROJ. #: 45-21-021
DRAWN BY: WDR
DESIGN BY: ??
CHECKED BY: ???

LAST UPDATED: 2/9/2023

SHEET NUMBER:

G-002

GENERAL LINE LEGEND

| LINE DESCRIPTION | PROPOSED LINE | EXISTING LINE |
|---------------------------------------|---------------|---------------|
| POWER / COMMUNICATIONS | | |
| OVERHEAD POWER | — OHP — | — OHP — |
| UNDERGROUND POWER | — UP — | — UP — |
| OVERHEAD TELEPHONE | — OHT — | — OHT — |
| UNDERGROUND TELEPHONE | — UT — | — UT — |
| FIBER OPTIC | — F/O — | — F/O — |
| CABLE TELEVISION | — CTV — | — CTV — |
| UNDERGROUND POWER, TEL, CABLE TV | | — P,T,CTV — |
| UNDERGROUND POWER, TEL, CABLE TV, GAS | | — P,T,CTV,G — |
| STORM DRAIN | | |
| STORM DRAIN (GENERAL) | — SD — | — SD — |
| STORM DRAIN | — X*SD — | — X*SD — |
| UNDER DRAIN | — UD — | — UD — |
| WATER | | |
| WATER (GENERAL) | — W — | — W — |
| WATER (SPECIFIED SIZE) | — X*W — | — X*W — |
| WATER SERVICE | — WS — WS — | — WS — WS — |

NOTE:
THE LOCATION OF ALL EXISTING KNOWN UNDERGROUND UTILITIES IS SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION. IN THE EVENT OF DAMAGE TO EXISTING UTILITIES OR CABLES, THE ENGINEER AND THE OWNER ARE TO BE NOTIFIED IMMEDIATELY. THE CONTRACTOR SHALL REPAIR ANY SUCH DAMAGE TO UTILITIES, AS DIRECTED BY THE ENGINEER, IMMEDIATELY AND AT THE CONTRACTOR'S EXPENSE.

| LINE DESCRIPTION | PROPOSED LINE | EXISTING LINE |
|--------------------|---------------|---------------|
| SITE | | |
| FENCE | — X — | — X — |
| MAJOR CONTOUR | — 2521 — | — — — |
| MINOR CONTOUR | | — — — |
| GRADE BREAK | | — GB — |
| TOP OF BANK | | — TOB — |
| TOE OF SLOPE | | — TOE — |
| CUT LIMITS | — — — | |
| FILL LIMITS | — | |
| DITCH | — — — | — — — |
| STORM SWALE | — — — | — — — |
| ROADWAY | | |
| ACCESS ROUTE | — - - - - | |
| ROAD SHOULDER | | — — — |
| ROAD CENTERLINE | | — — — |
| ROAD ASPHALT | | — EP — |
| ROAD GRAVEL | | — EG — |
| TOP BACK OF CURB | | |
| LIP OF GUTTER | | |
| LANDSCAPING LIMITS | — LS — | — LS — |

GENERAL SYMBOL LEGEND

| SITE | | |
|-----------------------|---|---|
| BOLLARD | ■ | ■ |
| BOULDER | ○ | ● |
| DRINKING FOUNTAIN | □ | □ |
| FLAGPOLE | ○ | ○ |
| GATE | — | — |
| MAIL BOX | ■ | ■ |
| PARKING METER | □ | □ |
| POST | ○ | ● |
| SIGN | — | — |
| SPOT ELEVATION | ○ | ○ |
| TREE (SHRUB) | □ | □ |
| TREE (STUMP) | △ | △ |
| TREE (CONIFEROUS) | ■ | ■ |
| DOMESTIC WATER | | |
| FIRE HYDRANT | ○ | ● |
| SPIGOT | ● | ● |
| YARD HYDRANT | ○ | ● |
| WATER MANHOLE | ○ | ● |
| WATER METER | ■ | ■ |
| WATER VALVE | ■ | ■ |
| ELECTRIC | | |
| ELEC. MANHOLE | ○ | ● |
| ELEC. METER | □ | □ |
| ELEC. TRANS. | □ | □ |
| TEST HOLE | ■ | ■ |
| WELL | ■ | ■ |
| WELL (MONITORING) | ■ | ■ |
| SANITARY SEWER | | |
| CLEANOUT | ○ | ○ |
| SEWER STUB | ○ | ○ |
| SS MANHOLE | ○ | ○ |
| IRRIGATION | | |
| IRRIGATION VALVE | ■ | ■ |
| IRRIGATION VALVE BOX | ○ | ○ |
| SPRINKLER | △ | △ |
| STORM DRAIN | | |
| CATCH BASIN | ■ | ■ |
| DRY WELL | ○ | ○ |
| FLARE END | ▽ | ▽ |
| GREASE TRAP | ○ | ○ |
| SD MANHOLE | ○ | ● |

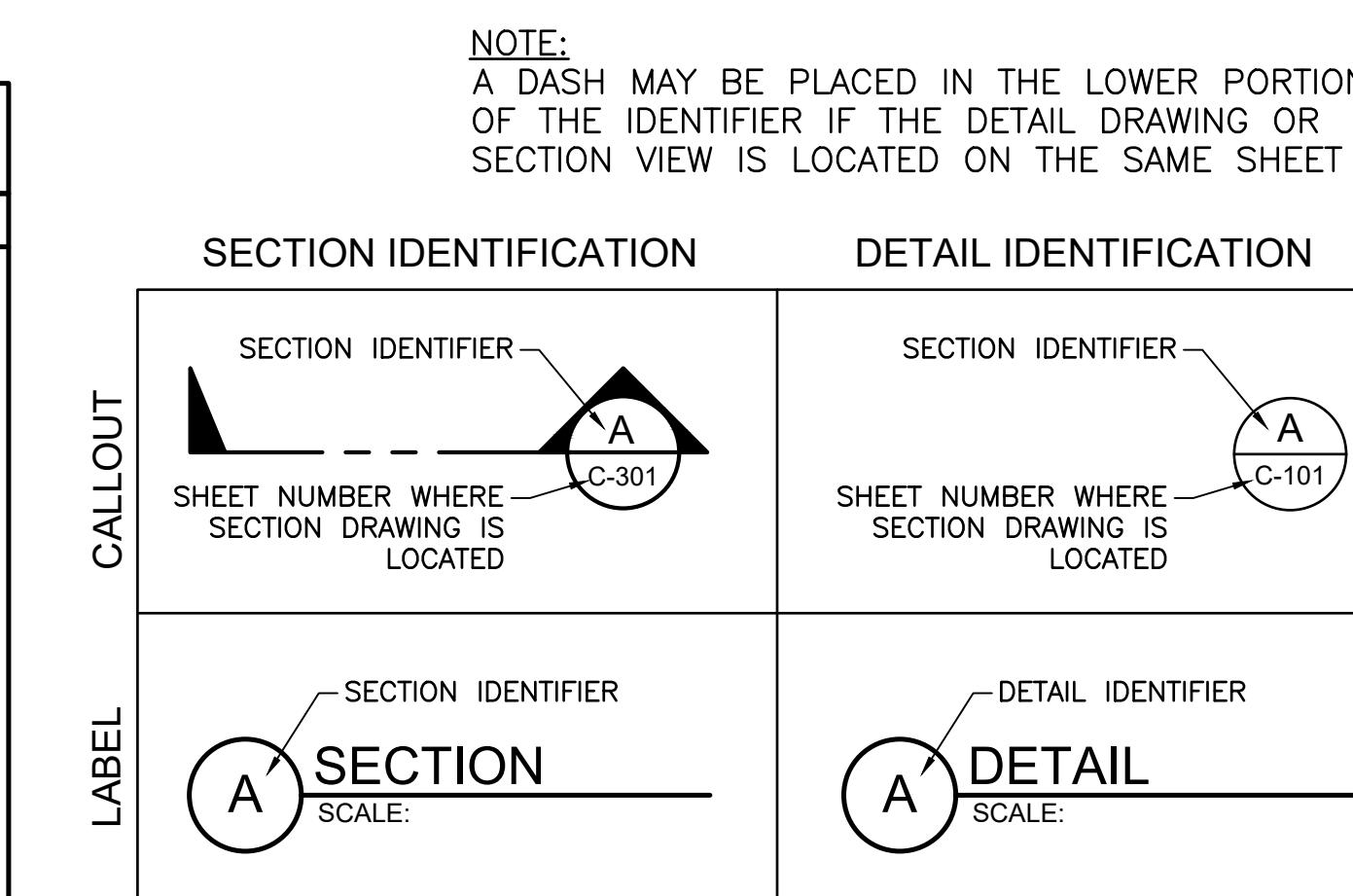
AIRPORT LEGEND

| LINE DESCRIPTION | PROPOSED LINE | EXISTING LINE |
|------------------------------|---------------|---------------|
| AIRPORT LINETYPES | | |
| AIRPORT PROPERTY LINE | — - - | — - - |
| BUILDING RESTRICTION LINE | — BRL — | — BRL — |
| RUNWAY OBJECT FREE AREA | — ROFA — | — ROFA — |
| RUNWAY OBJECT FREE ZONE | — ROFZ — | — ROFZ — |
| RUNWAY PROTECTION ZONE | — RPZ — | — RPZ — |
| RUNWAY SAFETY AREA | — RSA — | — RSA — |
| RUNWAY VIZABILITY ZONE | — RVZ — | — RVZ — |
| TAXIWAY SAFETY AREA | — TSA — | — TSA — |
| TAXIWAY OBJECT FREE AREA | — TOFA — | — TOFA — |
| PRECISION OBSTACLE FREE ZONE | — POFZ — | — POFZ — |

| DESCRIPTION | PROPOSED | EXISTING |
|--------------------------|----------|----------|
| AIRFIELD PAVEMENT | | |
| AIRFIELD PAVEMENT | | |
| PAVEMENT TO BE REMOVED | | |

| LINE DESCRIPTION | PROPOSED LINE | EXISTING LINE |
|---|---------------|---------------|
| AIRPORT SYMBOLS | | |
| AIRFIELD LIGHT (RUNWAY) | ○ | ○ |
| LIGHT JUNCTION CAN | ○ | ○ |
| AIRFIELD LIGHT (TAXIWAY) | □ | □ |
| THRESHOLD LIGHTS | — — — | — — — |
| TAXIWAY REFLECTOR | ○ | ○ |
| REIL | ▼ | ▽ |
| LIGHTED GUIDANCE SIGN | — — — | — — — |
| VASI/PAPI | ■ ■ | □ □ |
| AIRCRAFT TIEDOWN | — — — | — — — |
| AIRPORT REFERENCE POINT | ○ | ○ |
| AIRPORT BEACON | □ | □ |
| ELECTRICAL JUNCTION BOX (LIGHT TRAFFIC) | □ | □ |
| ELECTRICAL JUNCTION BOX (HEAVY TRAFFIC) | J | J |
| WIND CONE | ○ | ○ |
| SEGMENTED CIRCLE | ○ | ○ |
| TETRAHEDRON | △ | △ |

SECTION AND DETAIL IDENTIFIERS



EXAMPLE
SHEET NUMBER

C-101

 DISCIPLINE DESIGNATOR
SHEET TYPE DESIGNATOR
SHEET SEQUENCE NUMBER

| DISCIPLINE DESIGNATORS | | |
|------------------------|------------|----------------|
| DISCIPLINE | DESIGNATOR | DESCRIPTION |
| GENERAL | G | ALL GENERAL |
| SURVEY/MAPPING | V | ALL SURVEY |
| CIVIL | C | ALL CIVIL |
| ELECTRICAL | E | ALL ELECTRICAL |

| SHEET TYPE DESIGNATORS | | |
|------------------------|--|--|
| DESIGNATOR | SHEET TYPE | |
| 0 | GENERAL (SYMBOLS, LEGENDS, NOTES, ETC.) | |
| 1 | TYPICAL SECTIONS, PLAN SHEETS | |
| 2 | GRADING SHEETS | |
| 3 | PLAN AND PROFILES | |
| 4 | MARKING SHEETS | |
| 5 | DETAILS OR COMBINED DETAILS AND SECTIONS | |

TAXILANE CHARLIE EXTENSION YAKIMA AIR TERMINAL - MCALLISTER FIELD
GENERAL LEGEND

AIP 3-53-0089-052-2002
GENERAL LEGEND

FILE #: 45-21-021 G-003
JUB PROJ. #: 45-21-021
DRAWN BY: WDR
DESIGN BY: ???.
CHECKED BY: ???.
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LAST UPDATED: 4/14/2022
SHEET NUMBER:
G-003

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REGISTERED
BY APR. DATE
NO. DESCRIPTION
NO. DESCRIPTION

GENERAL SYMBOL LEGEND

| COMMUNICATION | | |
|-----------------------|----|----|
| TELE. MANHOLE | ① | ● |
| TELE. PEDASTAL | ① | ○ |
| TELE. POLE | ○ | ● |
| TV PEDASTAL | IV | IV |
| GUY WIRE | ↓ | ↑ |
| DOMESTIC WATER | | |
| FIRE HYDRANT | ○ | ● |
| SPIGOT | ● | ● |
| YARD HYDRANT | ○ | ● |
| WATER MANHOLE | ○ | ● |
| WATER METER | ■ | ■ |
| WATER VALVE | ■ | ■ |
| ELECTRIC | | |

SAFETY NOTES AND REQUIREMENTS:

- ALL OPERATIONS DURING CONSTRUCTION SHALL BE SUBJECT TO AC 150/5370-2G. "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- THE CONSTRUCTION SHALL, AS MUCH AS POSSIBLE, MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY.
- THE CONTRACTOR SHALL MAINTAIN CLEAR ROUTES FOR AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) OPERATIONS TO ACTIVE AIRPORT OPERATIONS AND SAFETY AREAS INCLUDING THE AIRPORT GENERAL AVIATION AREA. THE CONTRACTOR SHALL NOTIFY AIRPORT PERSONNEL OF ANY PROPOSED ACTIVITIES THAT MAY INTERFERE WITH ARFF OPERATIONS; I.E. EQUIPMENT OPERATING NEAR HYDRANTS OR NEED TO SHUT OFF HYDRANTS.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR OR ENGINEER AT LEAST 48 HOURS PRIOR TO WORK BEING INITIATED WITHIN THE AIRPORT OPERATIONS AREA (AOA).
- THE OWNER WILL INITIATE AND CANCEL ALL NOTICES TO AIRMAN (NOTAM) BASED ON CONTRACTOR SCHEDULES. THE CONTRACTOR SHALL COORDINATE ALL ACTIVITIES WITH THE ENGINEER. NO WORK SHALL BEGIN UNTIL THE NOTAM FOR THAT WORK HAS BEEN POSTED WITH THE FAA.
- ALL CONSTRUCTION EQUIPMENT AND VEHICLES SHALL BE MARKED AND LIGHTED PER SECTION 70-08 OF THE FAA GENERAL CONDITIONS. EQUIPMENT AND VEHICLE FLAGGING AND LIGHTING SHALL BE IN ACCORDANCE WITH AC 150/5210-5D, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT", CURRENT EDITION.
- THE CONTRACTOR MAY STORE ALL EQUIPMENT AND MATERIALS IN THE DESIGNATED STAGING AREA WHEN NOT IN USE. STOCKPILED MATERIALS NECESSARY FOR CONSTRUCTION SHALL BE OF LOW PROFILE (10-FOOT MAX HEIGHT).
- WORKER'S PRIVATE VEHICLES WILL BE PERMITTED WITHIN THE SECURITY FENCED AREA. CONSTRUCTION PERSONNEL SHALL BE REQUIRED TO PARK IN THE DESIGNATED STAGING AREA OR OTHER AREAS AS APPROVED BY THE OWNER. TRANSPORTATION TO THE PROJECT SITES WITHIN THE SECURITY FENCED AREAS SHALL MEET THE REQUIREMENTS OF SAFETY NOTE 6, NO EXCEPTIONS.
- REMOVE ALL GENERATED DEBRIS FROM THE WORK AREA AT THE CONCLUSION OF WORK IN THE GIVEN AREA. BURNING OF GENERATED DEBRIS SHALL NOT BE PERMITTED, NO EXCEPTIONS.
- THE CONTRACTOR SHALL CONTROL DUST BY VACUUMING OR WATERING THE DISTURBED AREAS OR BY OTHER EROSION STABILIZATION METHODS APPROVED BY THE ENGINEER. DUST / EROSION CONTROL OPERATIONS SHALL BE CONTINUOUS, INCLUDING HOLIDAYS AND WEEKENDS.
- THE CONTRACTOR SHALL SUPPLY LIGHTED LOW PROFILE BARRICADES INCLUDING FLAGS FOR THIS PROJECT. PLACEMENT BY THE CONTRACTOR SHALL BE IN THE PRESENCE AND WITH THE APPROVAL OF AIRPORT PERSONNEL AND THE ENGINEER DURING AOA CONSTRUCTION OPERATIONS PER SECTION 70-08 OF THE SPECIFICATIONS AND AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PLACE AND FILL THE BARRICADES WITH WATER AND MAINTAIN FOR THE CONTRACT DURATION. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AND REPLACING BARRICADE LIGHT BATTERIES AND FLAGS AS REQUIRED DURING CONSTRUCTION. BARRICADES DAMAGED BEYOND USE DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE OWNER.
- ALL AIRPORT GATES AND FENCING USED FOR CONSTRUCTION ACCESS SHALL BE MAINTAINED BY THE CONTRACTOR TO CONTROL AND/OR DETER HUMAN AND ANIMAL INTRUSIONS INTO THE AOA.
- IN ORDER TO MAINTAIN A CLEAR OBSTACLE FREE ZONE, CONTRACTOR EQUIPMENT HEIGHT SHALL BE LIMITED TO 45 FEET AND NO ACTIVITY WILL BE ALLOWED WITHIN 400 FEET OF RUNWAY 9/27 CENTERLINE OR WITHIN 250 FEET OF RUNWAY 4/22 CENTERLINE. ADDITIONALLY, NO ACTIVITY WILL BE ALLOWED WITHIN AN ACTIVE TAXIWAY SAFETY AREA. THE OWNER MAY CONSIDER EXCEPTIONS FOR SHORT DURATION ACTIVITIES ON A LIMITED BASIS. SUBMIT REQUEST A MINIMUM OF 10 WORKING DAYS IN ADVANCE FOR OWNER'S CONSIDERATION.
- CONTRACTOR SHALL HOLD, AT A MINIMUM, WEEKLY CONSTRUCTION MEETINGS WHICH MAY INCLUDE REPRESENTATIVES FROM THE AIRPORT, FBO'S, AIR TRAFFIC CONTROL TOWER, ARFF, AND ENGINEER TO REVIEW SCHEDULE, WORK AND SAFETY ISSUES. IF A SAFETY ISSUE ARISES, THE AIRPORT DIRECTOR OR ENGINEER MAY REQUIRE MORE FREQUENT MEETINGS. ON-SITE INSPECTIONS OF PROJECT CONDITIONS WILL BE PERFORMED BY THE CONTRACTOR WITH THE AIRPORT OPERATOR THROUGHOUT THE DURATION OF THE PROJECT. ANY DEFICIENCIES FOUND SHALL BE CORRECTED IMMEDIATELY BY THE CONTRACTOR. IF REQUESTED BY THE OWNER OR ENGINEER, THE CONTRACTOR SHALL PROVIDE MINUTES OF ALL SAFETY MEETINGS CONDUCTED. THE MINUTES SHALL INCLUDE A LIST OF ATTENDEES. PAYMENT WILL NOT BE MADE FOR CONSTRUCTION SAFETY (SP-1.2) UNTIL REQUESTED MEETING MINUTES ARE SUBMITTED.
- THE CONTRACTOR SHALL HAVE, AT A MINIMUM, ONE PERSON ON SITE AT ALL TIMES WHO HAS THE AUTHORITY TO MAKE DECISIONS AND DIRECT CONTRACTOR AND SUBCONTRACTOR STAFF IN A TIMELY MANNER. THIS PERSON SHALL ALSO HAVE AN AIRPORT SECURITY BADGE TO ACT AS ESCORT FOR THE CONTRACTOR'S PERSONNEL. IT IS RECOMMENDED THAT THE CONTRACTOR HAVE ADDITIONAL SECURITY BADGED INDIVIDUALS TO COVER CONTINGENCIES. THE BADGED INDIVIDUAL(S) SHALL REMAIN WITH THE UNBADGED PERSONNEL AT ALL TIMES THEY ARE IN THE AOA. ACCESS TO THE SECURITY IDENTIFICATION DISPLAY AREAS (SIDA), SHALL ONLY BE UNDER DIRECT ESCORT PROVIDED BY THE OWNER OR A CONTRACTOR INDIVIDUAL WHO HAS MET THE ADDITIONAL REQUIREMENTS TO OBTAIN A SIDA SECURITY BADGE. A CONTRACTOR'S SIDA BADGED INDIVIDUAL PROVIDING SIDA AREA ESCORT SHALL NOT BE PERMITTED TO PERFORM ANY WORK ACTIVITIES, NO EXCEPTIONS.
- THE CONTRACTOR SHALL PROVIDE CONTRACT SECURITY COMPANY PERSONNEL AT ALL DESIGNATED SECURITY ACCESS GATES AT ALL TIMES DURING CONSTRUCTION ACTIVITIES. THIS INDIVIDUAL SHALL VERIFY IDENTIFICATION OF AND APPROVE ALL ENTERING PERSONNEL TO BE ON THE PROJECT.
- EXISTING AIRPORT PAVEMENTS SHALL NOT BE USED AS HAUL ROUTES UNLESS SHOWN/NOTED ON THE PLANS AND/OR PRIOR PERMISSION HAS BEEN GRANTED BY THE AIRPORT AND THE ENGINEER.
- ALL EXCAVATIONS WITHIN RUNWAY OR TAXIWAY SAFETY AREAS SHALL BE BACKFILLED, COMPAKTED, AND GRADED PRIOR TO OPENING THE RUNWAY OR TAXIWAY FOR OPERATIONS. THE GRADING SHALL LEAVE NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS AS DETERMINED BY THE ENGINEER.

CULTURAL MONITORING REQUIREMENTS:

- DU TO POTENTIAL PRESENCE OF CULTURALLY SIGNIFICANT ITEMS IN THE PROJECT AREA, THE CONTRACTOR MUST USE CAUTION DURING PROJECT RELATED GROUND DISTURBING ACTIVITIES. IF CULTURALLY SIGNIFICANT ITEMS ARE LOCATED, THE CONTRACTOR MUST IMMEDIATELY HALT ALL GROUND DISTURBING ACTIVITIES IN THE VICINITY AND NOTIFY THE OWNER/ENGINEER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR HALTING WORK IN DESIGNATED AREAS UNTIL NEEDED COORDINATION AND CLEARANCE IS DONE. A CULTURAL MONITOR WILL BE ON SITE DURING GROUND DISTURBING ACTIVITIES IN THE DESIGNATED CULTURAL MONITORING AREAS. THE CULTURAL MONITOR WILL BE PROVIDED BY THE OWNER/ENGINEER. CONTRACTOR SHALL NOTIFY THE OWNER/ENGINEER A MINIMUM OF 1 WEEK PRIOR TO GROUND DISTURBING ACTIVITY IN THE DESIGNATED CULTURAL MONITORING AREAS.

DEFINITIONS:

- AIR OPERATIONS AREA (AOA):** FOR THE PURPOSE OF THE SPECIFICATIONS AND AS NOTED ON THIS SAFETY PLAN, THE TERM AIR OPERATIONS AREA (AOA) SHALL MEAN ANY AREA OF THE AIRPORT USED OR INTENDED TO BE USED FOR LANDING, TAKEOFF, OR SURFACE MANEUVERING OF AIRCRAFT. AN AIR OPERATIONS AREA SHALL INCLUDE SUCH PAVED OR UNPAVED AREAS THAT ARE USED OR INTENDED TO BE USED FOR THE UNOBSTRUCTED MOVEMENT OF AIRCRAFT IN ADDITION TO ITS ASSOCIATED RUNWAY, TAXIWAY, APRON, ROFA, TOFA, RSA, TSA, AND RPZ.
- OBJECT FREE AREA (ROFA/TOFA):** AN AREA ON THE GROUND PROVIDED TO ENHANCE THE SAFETY OF AIRCRAFT OPERATIONS BY HAVING THE AREA FREE OF OBJECTS.
- SAFETY AREA (RSA/TSA):** A DEFINED SURFACE ALONGSIDE THE RUNWAY OR TAXIWAY PREPARED OR SUITABLE FOR REDUCING THE RISK OF DAMAGE TO AIRCRAFT UNINTENTIONALLY DEPARTING THE RUNWAY OR TAXIWAY.
- RUNWAY PROTECTION ZONE (RPZ):** AN AREA OFF THE RUNWAY END TO ENHANCE THE PROTECTION OF PEOPLE AND PROPERTY ON THE GROUND.
- OBSTACLE FREE ZONE (OFZ):** AIRSPACE ALONG RUNWAY THAT IS REQUIRED TO BE CLEAR OF ALL OBJECTS IN ORDER TO PROVIDE CLEARANCE PROTECTION FOR AIRCRAFT LANDING OR TAKING OFF. REFER TO DETAILS ON THIS SHEET.

SAFETY AREA LIMITS:

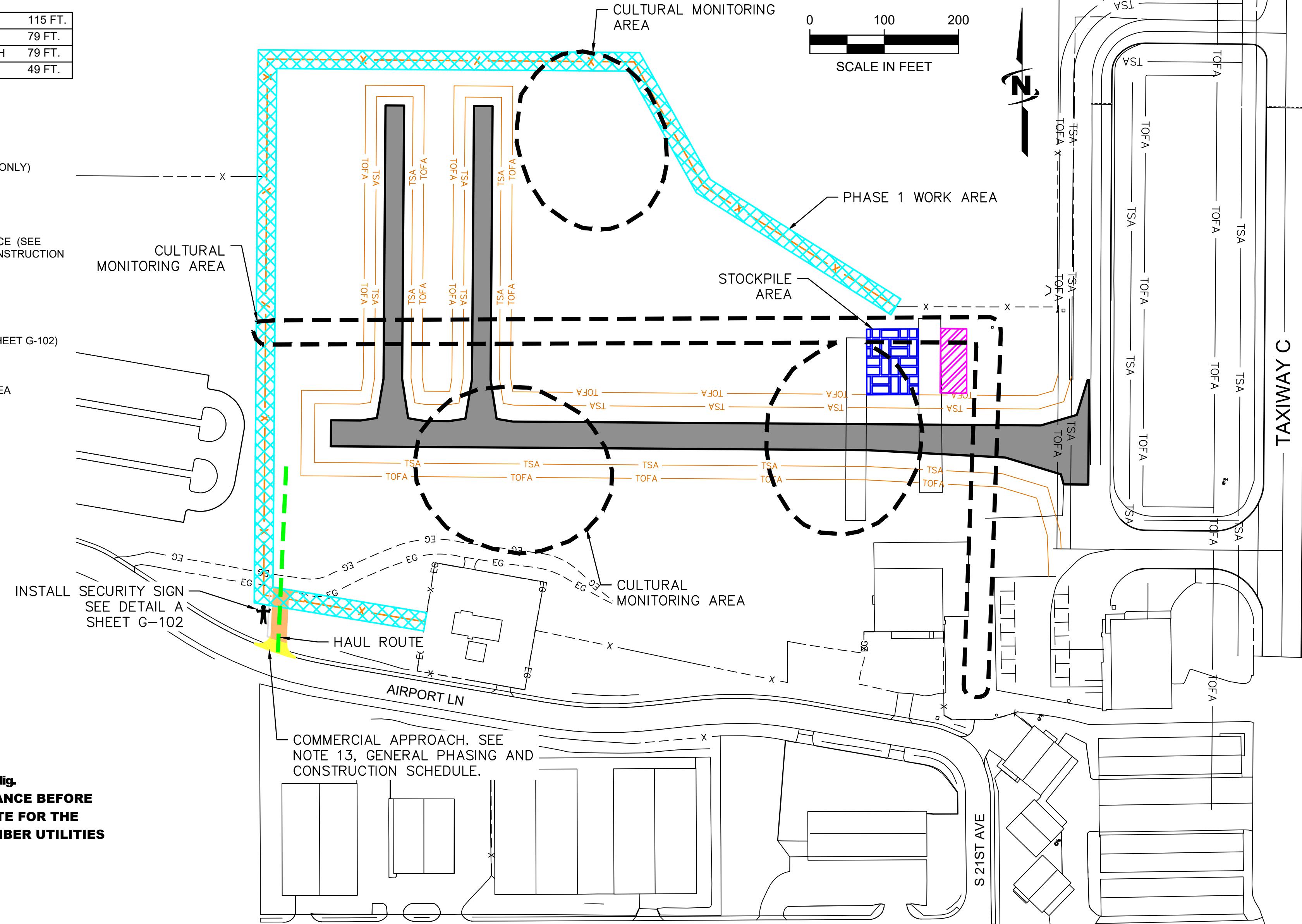
| | |
|--|---------|
| TAXILANE 1 - TAXILANE OBJECT FREE AREA WIDTH | 115 FT. |
| TAXILANE 1 - TAXILANE SAFETY AREA WIDTH | 79 FT. |
| TAXILANE 2 & 3 - TAXILANE OBJECT FREE AREA WIDTH | 79 FT. |
| TAXILANE 2 & 3 - TAXILANE SAFETY AREA WIDTH | 49 FT. |

LEGEND

- PHASE 1 WORK AREA LIMITS (FENCE ONLY)
- CONTRACTOR STAGING AREA
- STABILIZED CONSTRUCTION ENTRANCE (SEE NOTE 14, GENERAL PHASING AND CONSTRUCTION SCHEDULE)
- CONTRACTOR ACCESS ROUTE
- SECURITY ACCESS GUARD
- CLOSURE BARRICADE (SEE DETAIL SHEET G-102)
- TSA - TAXIWAY/TAXILANE SAFETY AREA
- TOFA - TAXIWAY/TAXILANE OBJECT FREE AREA
- X - EXISTING PERIMETER FENCE



Know what's below.
Call before you dig.
**CALL 2 BUSINESS DAYS IN ADVANCE BEFORE
YOU DIG, GRADE, OR EXCAVATE FOR THE
MARKING OF UNDERGROUND MEMBER UTILITIES**



OVERALL PHASING PLAN

GENERAL PHASING AND CONSTRUCTION SCHEDULE:

- CONSTRUCTION SHALL NOT DELAY OR DIVERT ANY REGULARLY SCHEDULED COMMERCIAL AIR OPERATIONS UNLESS PREVIOUSLY APPROVED BY THE ENGINEER AND/OR AIRPORT DIRECTOR.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS AND REGULATIONS AND CONSTRUCTION PERMITS THAT ARE PERTINENT TO THIS WORK. THE CONTRACTOR SHALL PURCHASE ALL PERMITS AND LICENSES PER SECTION 70-02 OF THE FAA GENERAL PROVISIONS.
- IF REQUIRED, DURING A RUNWAY OR TAXIWAY CLOSURE, THE CONTRACTOR SHALL OBSERVE THE SAFETY AREAS OF ALL OPEN RUNWAYS AND TAXIWAYS. WORK WITHIN THE SAFETY AREAS MAY BE PERMITTED ON A CASE BY CASE BASIS WITH APPROVAL OF THE DIRECTOR OF AIRPORTS AND ENGINEER.
- PERSONNEL ARE NOT PERMITTED TO ENTER ANY ACTIVE RUNWAY OBJECT FREE ZONES OR SAFETY AREAS WITHOUT AIRPORT ESCORT. WHEN PERMITTED TO WORK WITHIN A SAFETY AREA LIMITS OF AN OPEN RUNWAY OR TAXIWAY, THE CONTRACTOR MAY BE DIRECTED BY THE ENGINEER OR AIRPORT PERSONNEL TO RELOCATE TO ANOTHER PART OF THE AIRPORT OPERATIONS AREA (AOA) OR VACATE FROM THE FENCED CONFINES OF THE AOA FOR ARRIVALS AND / OR DEPARTURES OF AIRCRAFT. THE CONTRACTOR SHALL COMPLETE ITS RELOCATION IN, OR VACATING OF THE AOA BY 10 MINUTES PRIOR TO THE ARRIVAL AND SHALL NOT REMOBILIZE TO THE WORK AREA FOR 5 MINUTES FOLLOWING ANY ARRIVALS AND / OR DEPARTURE OF AIRCRAFT.
- AT ANYTIME, FOR EMERGENCY OPERATIONS, OR WHEN CONDITIONS MAY BE HAZARDOUS TO PUBLIC SAFETY, AT THE DIRECTION OF THE ENGINEER OR AIRPORT DIRECTOR THE CONTRACTOR SHALL CEASE OPERATIONS AND VACATE THE AOA. NO ADDITIONAL STANDBY TIMES WILL BE GRANTED OR PAID FOR THIS ITEM.
- ALL AREAS DISTURBED AS A RESULT OF THE CONTRACTOR'S STAGING, HAUL ROUTES, AND CONSTRUCTION OPERATIONS SHALL BE RESTORED EQUAL TO OR BETTER THAN ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE IN A TIMELY MANNER. ANY UTILITIES, REST ROOM FACILITIES, AND TEMPORARY FENCING SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES PRIOR TO THE START OF THE CONSTRUCTION. IN THE EVENT OF DAMAGE TO EXISTING UTILITIES AND CABLES, THE ENGINEER AND THE OWNER ARE TO BE NOTIFIED IMMEDIATELY. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO UTILITIES, AS DIRECTED BY THE ENGINEER, IMMEDIATELY, AT THE CONTRACTOR'S EXPENSE.
- STEEL PROTECTIVE PLATES SHALL BE PLACED ON ANY PAVEMENT EDGE CROSSINGS PRIOR TO STAGING AND CONSTRUCTION. 1/2" MINIMUM PLATE THICKNESS BY 12-FEET WIDE IS REQUIRED. PLATES SHALL BEGIN A MINIMUM OF 5-FEET BEFORE THE PAVEMENT EDGE AND SHALL EXTEND BEYOND PAVEMENT A MINIMUM OF 10-FEET. CONSTRUCT UNIFORM SLOPE AT SHOULDERS WHERE PLATES ARE PLACED TO MATCH EXISTING PAVEMENT SLOPE. ADDITIONAL CRUSHED AGGREGATE TOP COURSE MAY BE REQUIRED TO CONSTRUCT UNIFORM SHOULDER.
- THE CONTRACTOR SHALL CLEAN/MAINTAIN ALL PAVEMENT CROSSINGS BY BROOMING OR OTHER APPROVED METHODS AT THE END OF EACH WORK SHIFT AT A MINIMUM OR AS REQUIRED BY ENGINEER. CLEANING EQUIPMENT (INCLUDING A VACUUM/SWEEPER TRUCK) SHALL BE AVAILABLE ON SITE AT ALL TIMES.
- WATER FOR CONSTRUCTION SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF YAKIMA TO SETUP A METERED SOURCE. THE CONTRACTOR SHALL PAY ALL FEES ASSOCIATED WITH THE METER AND WATER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPHELDING ALL LOCAL, STATE AND FEDERAL REGULATIONS/RESTRICTIONS FOR PUBLIC HEALTH.
- THE CONTRACTOR SHALL DESIGNATE AND SUBMIT THE NAME OF A SUPERVISOR TO MONITOR HEALTH & WELL BEING OF THE CONSTRUCTION ACTIVITIES IN ACCORDANCE WITH WASHINGTON STATE LAW.
- CONTRACTOR SHALL CONSTRUCT A 24' WIDE "COMMERCIAL APPROACH - TYPE 2" IN ACCORDANCE WITH CITY OF YAKIMA STANDARD DETAILS TO REPLACE EXISTING CONCRETE SIDEWALK. CONTRACTOR SHALL OBTAIN REQUIRED PERMITTING FROM CITY OF YAKIMA PRIOR TO COMMENCING WORK.
- CONTRACTOR SHALL CONSTRUCT 24' WIDE STABILIZED CONSTRUCTION ACCESS (SEE DETAIL A SHEET G-301). CLEAR/GRUB AREA, EXCAVATE 6"-8" OF SOIL (AS DIRECTED) AND COMPACT PRIOR TO CONSTRUCTION OF STABILIZED CONSTRUCTION ENTRANCE. PAYMENT FOR STABILIZED CONSTRUCTION ENTRANCE SHALL BE UNDER THE LUMP SUM TEMPORARY EROSION AND SEDIMENTATION CONTROL BID ITEM.

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| FILE #: 45-21-021-G-10X | JUB PROJ. #: 45-21-021 |
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| CHECKED BY: TDJ | ONE INCH AT FULL SIZE, IF NOT ONE INCH, SCALE ACCORDINGLY |
| LAST UPDATED: 2/20/2023 | SHEET NUMBER: G-101 |
| REUSE OF DRAWINGS. COPYRIGHT AND OTHER RESERVED RIGHTS WITHOUT J-U-B'S PRIOR WRITTEN CONSENT SHALL NOT BE REUSED WITHOUT J-U-B'S PRIOR WRITTEN CONSENT BY APPLICABLE LAW, OR WITHOUT LIABILITY OR LEGAL EXPOSURE TO J-U-B. REVISION | |
| J-U-B SHALL RETAIN ALL COMMON LAW, STATUTORY, AND THE SAME SHALL NOT BE REUSED WITHOUT J-U-B'S PRIOR WRITTEN CONSENT ANY TIME, WHETHER WITH OR WITHOUT LIABILITY OR LEGAL EXPOSURE TO J-U-B. | |
| J-U-B ENGINEERS, INC. W. 422 Riverside Ave. Suite 304 Spokane, WA 99201 Phone: 509.456.3727 www.jub.com | |
| TAXILANE CHARLIE EXTENSION YAKIMA AIR TERMINAL - MCALLISTER FIELD AIP 3-53-0089-052-2002 OPERATIONAL SAFETY & PHASING PLAN - PHASE 1 | |

LEGEND

- PHASE 1 WORK AREA LIMITS (FENCE ONLY)
- CONTRACTOR STAGING AREA
- STABILIZED CONSTRUCTION ENTRANCE (CONSTRUCTED DURING PHASE 1)
- CONTRACTOR ACCESS ROUTE
- SECURITY ACCESS GUARD
- CLOSURE BARRICADE (SEE DETAIL SHEET G-102)
- TSA — TAXIWAY/TAXILANE SAFETY AREA
- TOFA — TAXIWAY/TAXILANE OBJECT FREE AREA
- X — EXISTING PERIMETER FENCE

0 100 200
SCALE IN FEET

CULTURAL MONITORING AREA, SEE NOTES ON G-101

CULTURAL MONITORING AREA,
SEE NOTES ON G-101

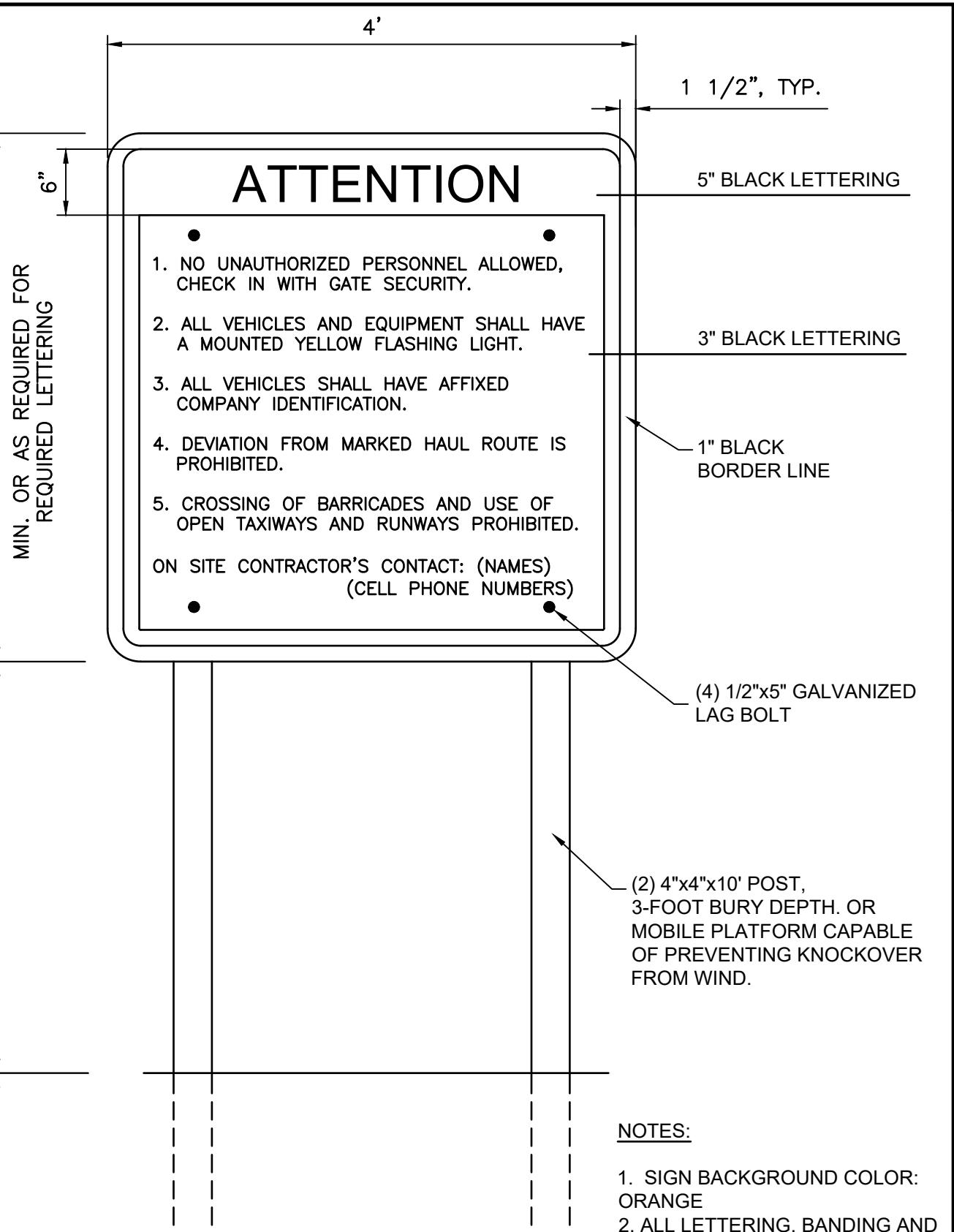
PHASE 2 WORK AREA

STOCKPILE AREA

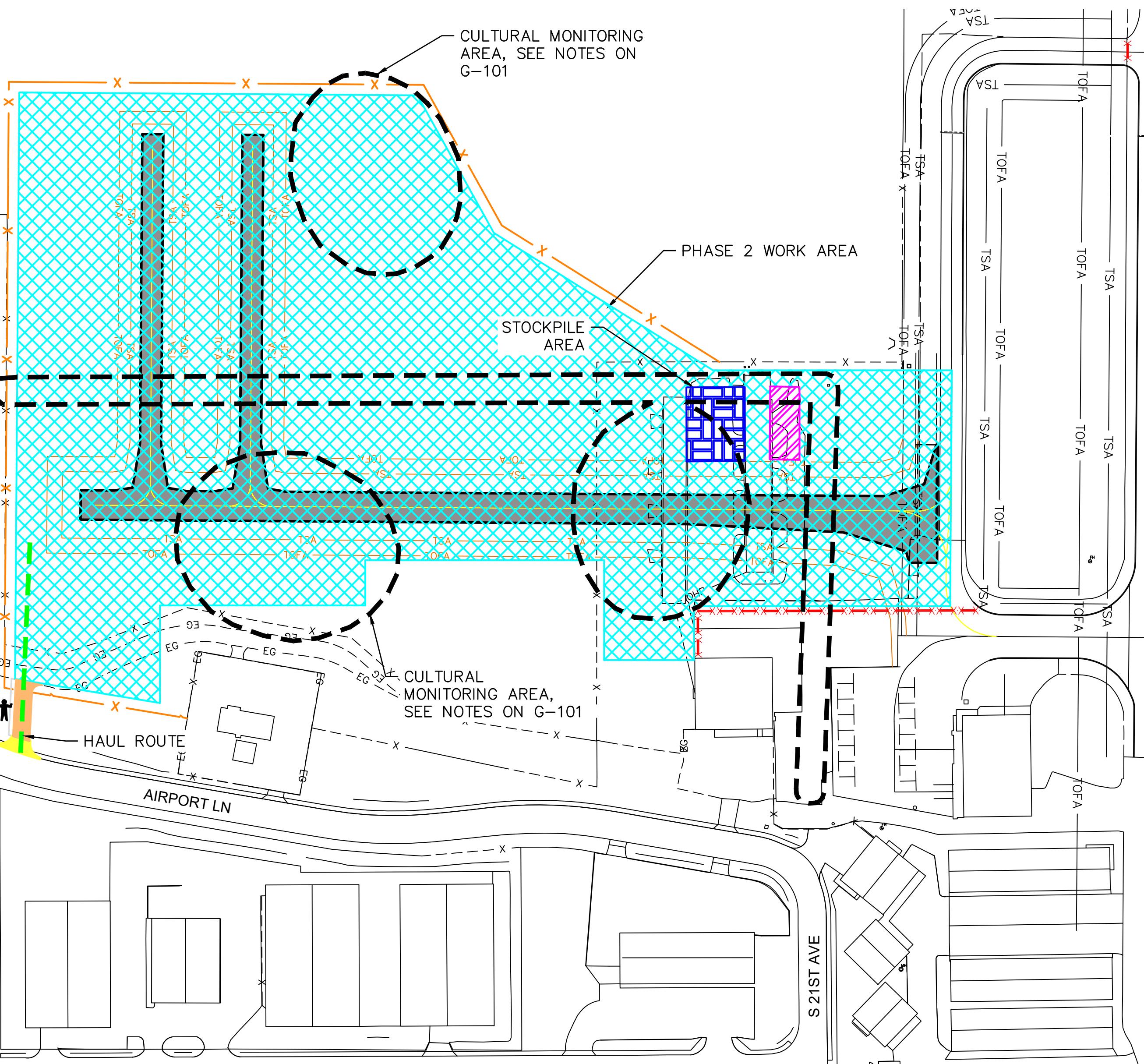
INSTALL SECURITY SIGN
SEE DETAIL A
SHEET G-102



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YOU DIG, GRADE, OR EXCAVATE FOR THE
MARKING OF UNDERGROUND MEMBER UTILITIES**



A SECURITY SIGN
SCALE:NTS



**TAXILANE CHARLIE EXTENSION
YAKIMA AIR TERMINAL - MCALLISTER FIELD**

OPERATIONAL SAFETY & PHASING PLAN - PHASE 2

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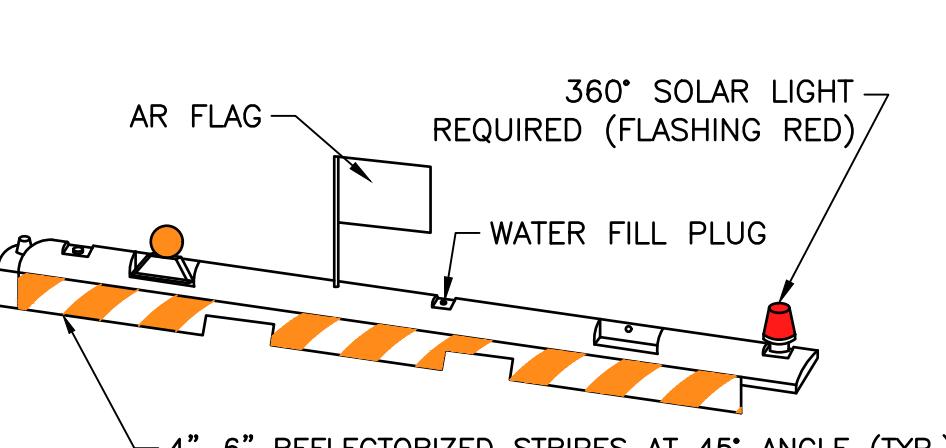
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LAST UPDATED: 2/9/2023

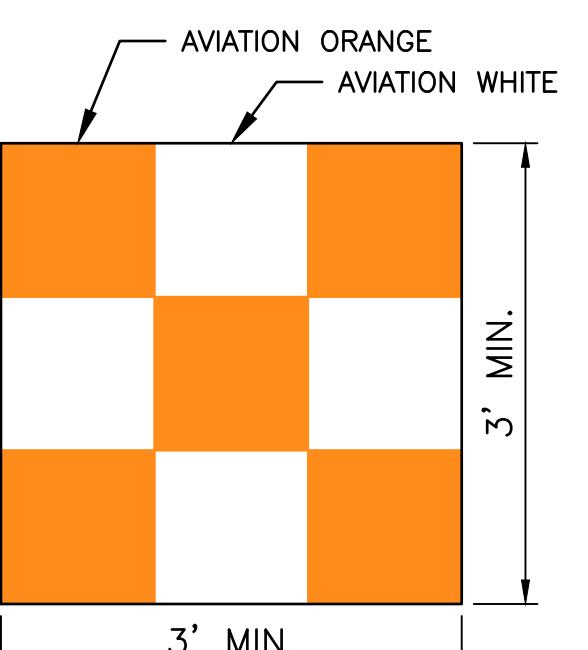
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G-102



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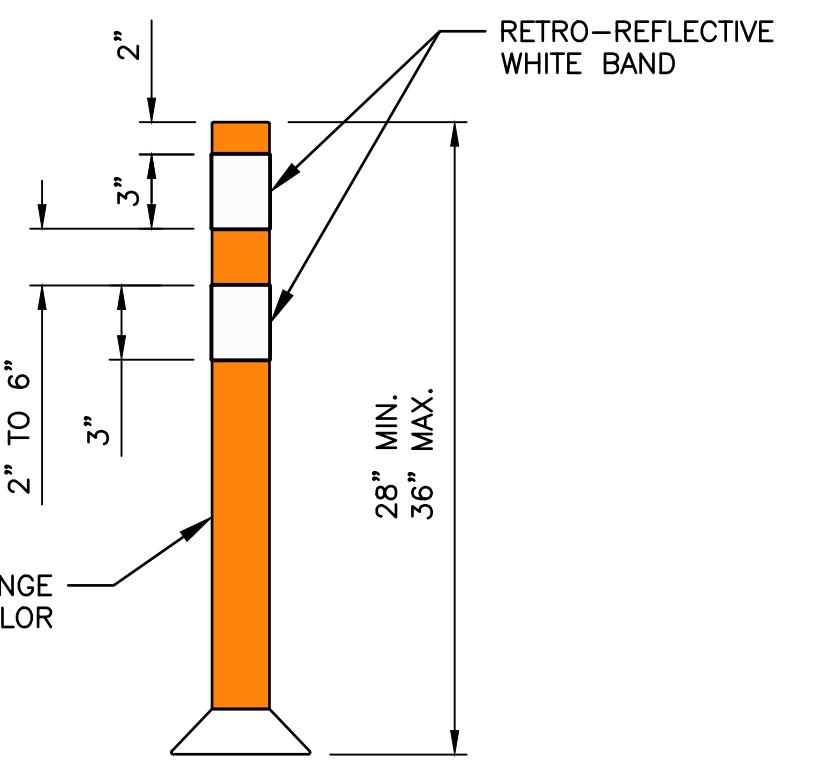
- BARRICADES SHALL BE SPACED A MAXIMUM OF 5' APART.
- ONE SOLAR RED FLASHING LIGHT (MINIMUM) SHALL BE AFFIXED TO THE TOP OF EACH BARRICADE SECTION. THE FLASHING LIGHT SHALL CONFORM TO FAA STANDARDS FOR LUMINESCENCE.
- THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE BARRICADES AND LIGHTS AND SHALL MAKE PROMPT REPAIRS AS NECESSARY.
- BARRICADES SHALL BE PLACED AT EDGES OF WORK AREAS AS SHOWN ON PROJECT PLANS, AND AS DIRECTED BY THE ENGINEER.

B LOW PROFILE AVIATION BARRICADE
SCALE:NTS



NOTE: A SAFETY FLAG DURING DAYLIGHT HOURS AND A FLASHING HIGH VISIBILITY CONSTRUCTION BEACON SHALL BE PROMINENTLY DISPLAYED ON ALL CONSTRUCTION EQUIPMENT WITH NO EXCEPTIONS.

C CONSTRUCTION SAFETY FLAG
SCALE:NTS



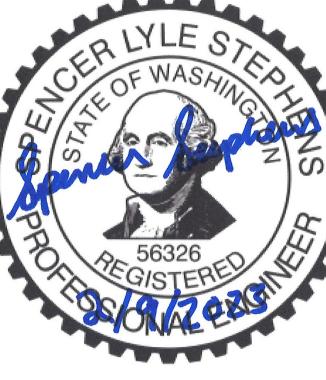
NOTES:

- CHANNELIZATION CONES SHALL BE PLACED AT EDGES OF WORK AREAS AS SHOWN ON PROJECT PLANS, AND AS DIRECTED BY THE ENGINEER.
- CHANNELIZATION CONES SHALL BE ORANGE IN COLOR.

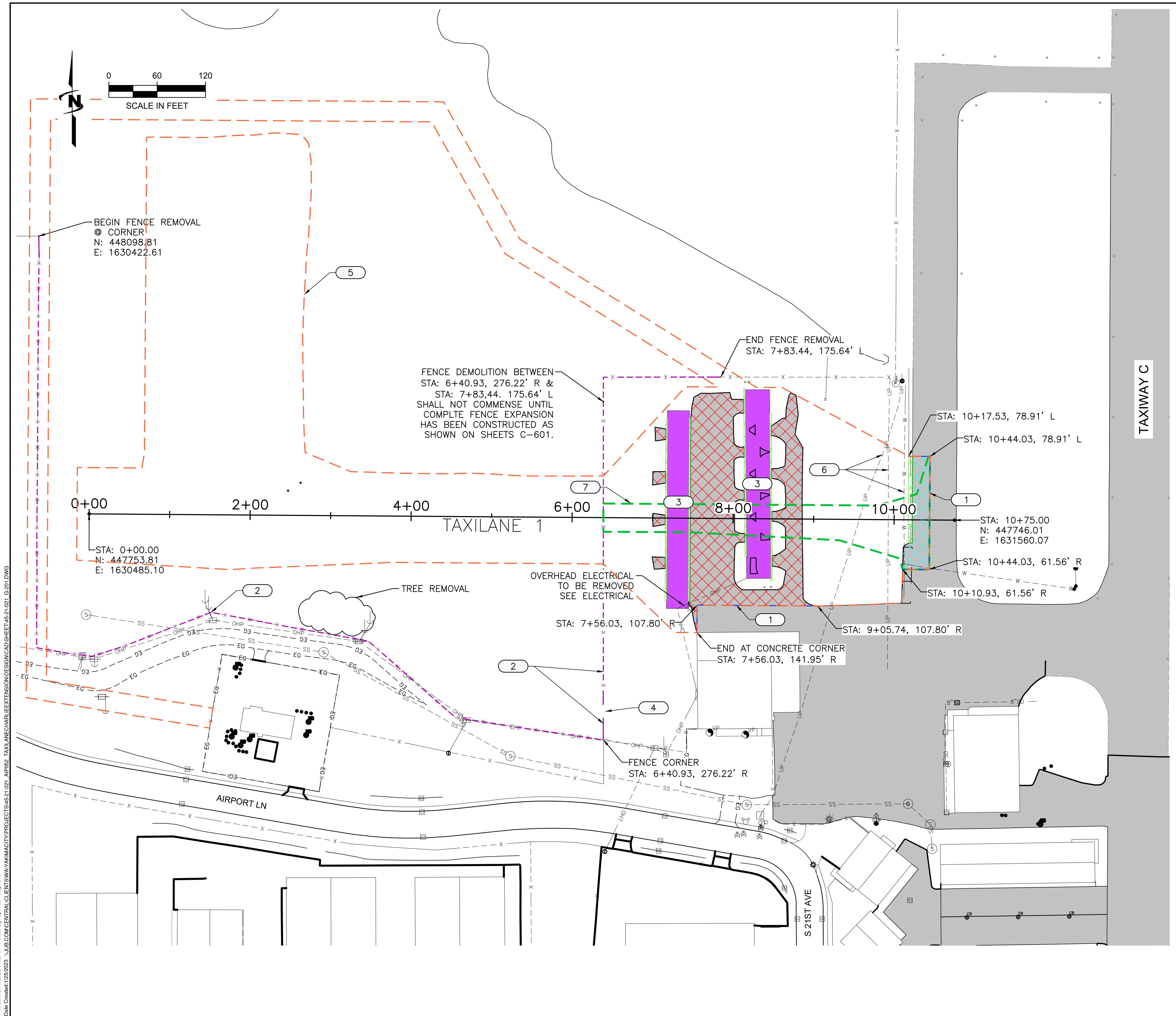
D CHANNELIZATION CONES
SCALE:NTS

J-U-B
J-U-B ENGINEERS, INC.

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REVISION
BY API DATE
NO. DESCRIPTION
NO. DATE



NOTES:

1. THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES IS SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
 2. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT MARKINGS OUTSIDE WORK AREAS.
 3. RETAIN AND PROTECT EXISTING UTILITIES UNLESS OTHERWISE SHOWN.
 4. EXCAVATED MATERIAL FOR PROPOSED PAVEMENT SECTION (EXCLUDING ASPHALT) SHALL BE UTILIZED FOR TAXIWAY SHOULDER FILL ELSEWHERE ON THE PROJECT. CONTRACTOR SHALL STOCKPILE THIS MATERIAL ONSITE, OR TRANSPORT DIRECTLY TO SHOULDER FILL LOCATION. PAYMENT SHALL BE MADE UNDER P-152 PER CUBIC YARD OF MATERIAL EXCAVATED, STOCKPILED (IF ELECTED), AND PLACED. EXCAVATED MATERIAL FROM OUTSIDE OF THESE LIMITS SHALL BE DISPOSED OFFSITE.
 5. HANGAR DEMOLITION SHALL INCLUDE FULL REMOVAL OF ASSOCIATED FOUNDATION AND SUPPORT STRUCTURE AND DISPOSAL OF DEMOLISHED MATERIALS. DEMOLISHED MATERIALS SHALL BE DISPOSED OF OFFSITE IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REQUIREMENTS. PRIOR TO DEMOLITION OF HANGARS, HANGAR ELECTRICAL SHALL BE DECOMMISSIONED AS DETAILED BY THE ELECTRICAL PLANS.
 6. PAYMENT FOR SAWCUTTING SHALL BE INCIDENTAL TO PAVEMENT REMOVAL BID ITEMS.



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KEYED NOTES:

- 1 SAWCUT PAVEMENT FULL DEPTH PER ITEM P-101
 - 2 REMOVE FENCE PER ITEM P-101
 - 3 DEMOLITION HANGAR PER ITEM P-101
 - 4 REMOVE GATE
 - 5 CLEARING/GRUBBING
 - 6 RETAIN & PROTECT UNDERGROUND UTILITIES
 - 7 EXCAVATION AREA FOR SHOULDER FILL MATERIAL. SEE NOTE 4.

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LEGEND

- | | |
|---|--------------------------------------|
|  | EXISTING PAVEMENT |
|  | REMOVE PAVEMENT |
|  | REMOVE ASPHALT OVERLAYED CONCRETE |
|  | REMOVE CONCRETE |
|  | DEMOLITION HANGAR, SEE NOTE 5 |
|  | SAWCUT PAVEMENT/CONCRETE, SEE NOTE 6 |
|  | FENCE REMOVAL |
|  | CLEARING/GRUBBING LIMITS |

| | |
|---|------------------|
| TAXILANE CH | |
| YAKIMA AIR TERMINAL | |
| | AIP 3-53 DEMO |
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TEMPORARY EROSION AND SEDIMENT CONTROL NOTES

- THE TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ALL OTHER CONSTRUCTION.
- PROPERTIES ADJACENT TO THE PROJECT SITE THAT ARE SUBJECT TO POTENTIAL EROSION CAUSED BY CONSTRUCTION ACTIVITIES SHALL BE PROTECTED FROM SEDIMENT DEPOSITION THROUGH THE USE OF SILT FENCE, HAY BALES, OR OTHER BMP SELECTED BY THE CONTRACTOR.
- ALL STORM DRAIN INLETS MADE OPERABLE DURING CONSTRUCTION SHALL BE FITTED WITH INLET PROTECTION AS SHOWN IN DETAIL B1, THIS SHEET TO PREVENT SEDIMENT FROM ENTERING THE SYSTEM. THE FILTER FABRIC SHALL BE INSPECTED REGULARLY AND CLEANED WHEN NECESSARY.
- WHEREVER CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED AREAS, A STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE CONSTRUCTED (SEE DETAIL THIS SHEET) TO MINIMIZE THE TRANSPORT OF SEDIMENT (MUD) ONTO THE PAVED AREAS. IF SEDIMENT IS TRANSPORTED ONTO A PAVED SURFACE, THE PAVED SURFACE SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM PAVED SURFACES BY SHOVELING OR SWEEPING AND BE TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA. STREET WASHING SHALL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED. TRAPPED SEDIMENT SHALL BE REMOVED OR STABILIZED ON SITE. DISTURBED SOIL AREAS RESULTING FROM REMOVAL SHALL BE PERMANENTLY STABILIZED.
- ALL POLLUTANTS OTHER THAN SEDIMENT THAT OCCUR ON-SITE DURING CONSTRUCTION SHALL BE HANDLED AND DISPOSED OF IN A MANNER THAT DOES NOT CAUSE CONTAMINATION OF STORM WATER OR THE SITE.
- ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL FACILITIES SHALL BE INSPECTED, MAINTAINED, AND REPAIRED BY THE CONTRACTOR AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED USE.
- THE CONTRACTOR IS RESPONSIBLE TO PROVIDE ADDITIONAL EROSION CONTROL MEASURES, INCLUDING BUT NOT LIMITED TO SILT FENCING, SEDIMENT PONDS/TRAPS, DIVERSION SWALES, CHECK DAMS, SEDIMENT BARRIERS, FILTER FABRIC, MULCH, AND SEEDING, AS CONDITIONS REQUIRE. THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR PREVENTING SILT-LADEN RUNOFF FROM DISCHARGING FROM THE PROJECT SITE. FAILURE BY THE CONTRACTOR CAN RESULT IN A FINE.
- AT NO TIME SHALL CONCRETE, CONCRETE BY-PRODUCTS, VEHICLE FLUIDS, PAINT, CHEMICALS, OR OTHER POLLUTANTS BE PERMITTED TO DISCHARGE TO THE TEMPORARY OR PERMANENT DRAINAGE SYSTEM, OR TO DISCHARGE FROM THE PROJECT SITE.
- AT ALL TIMES OF THE YEAR, THE CONTRACTOR SHALL HAVE SUFFICIENT MATERIALS, EQUIPMENT AND LABOR ON-SITE TO STABILIZE AND PREVENT EROSION FROM ALL DENUDED AREAS WITHIN 12-HOURS AS SITE AND WEATHER CONDITIONS DICTATE. (SEE BMP C150E MATERIALS ON HAND IN THE EASTERN WASHINGTON STORMWATER MANAGEMENT MANUAL FOR DETAILS)
- CONTRACTOR SHALL PROVIDE DUST CONTROL, AS NECESSARY, TO BE COMPLIANT WITH ALL LOCAL AND STATE CLEAN AIR/DUST CONTROL POLICIES. THE SPRAYING OF WATER ON DRY AREAS SHALL BE USED TO CONTROL DUST. CONTRACTOR SHALL SUPPLY ALL THE NECESSARY WATER FOR DUST CONTROL.
- ALL EXISTING STORM DRAIN INLETS SURROUNDING THE CONSTRUCTION SITE SHALL BE FITTED WITH INLET PROTECTION AS SHOWN IN DETAIL B1, THIS SHEET TO PREVENT SEDIMENT FROM ENTERING THE SYSTEM. THE FILTER FABRIC SHALL BE INSPECTED REGULARLY AND CLEANED WHEN NECESSARY.
- THE CONTRACTOR SHALL RESTORE ALL STAGING AREAS TO CONDITION PRIOR TO CONSTRUCTION. THIS MAY REQUIRE SEEDING IN ACCORDANCE WITH SPECIFICATION T-901. NO PAYMENT SHALL BE MADE FOR SEEDING REQUIRED FOR STAGING AREA RESTORATION.

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DESCRIPTION _____
BY APR. DATE _____

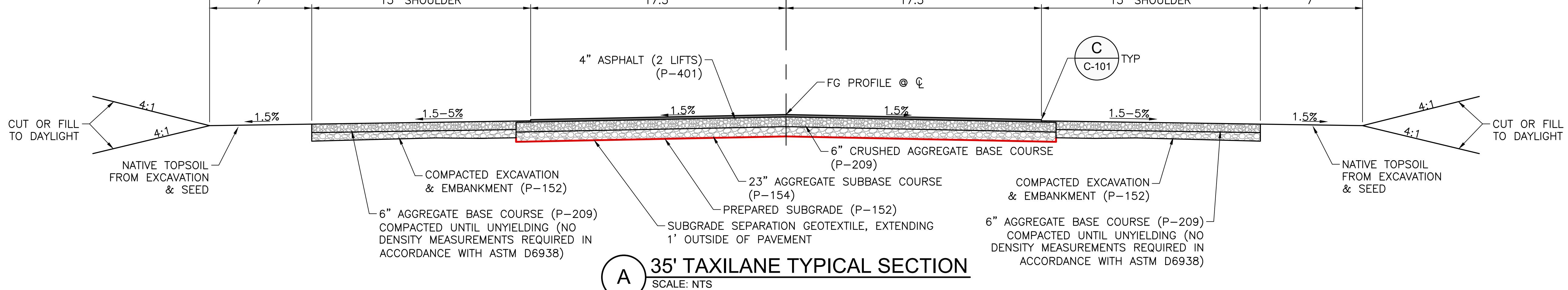
TAXILANE CHARLIE EXTENSION
YAKIMA AIR TERMINAL - MCALLISTER FIELD
AIP 3-53-0089-052-2002
TYPICAL SECTIONS

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C-101

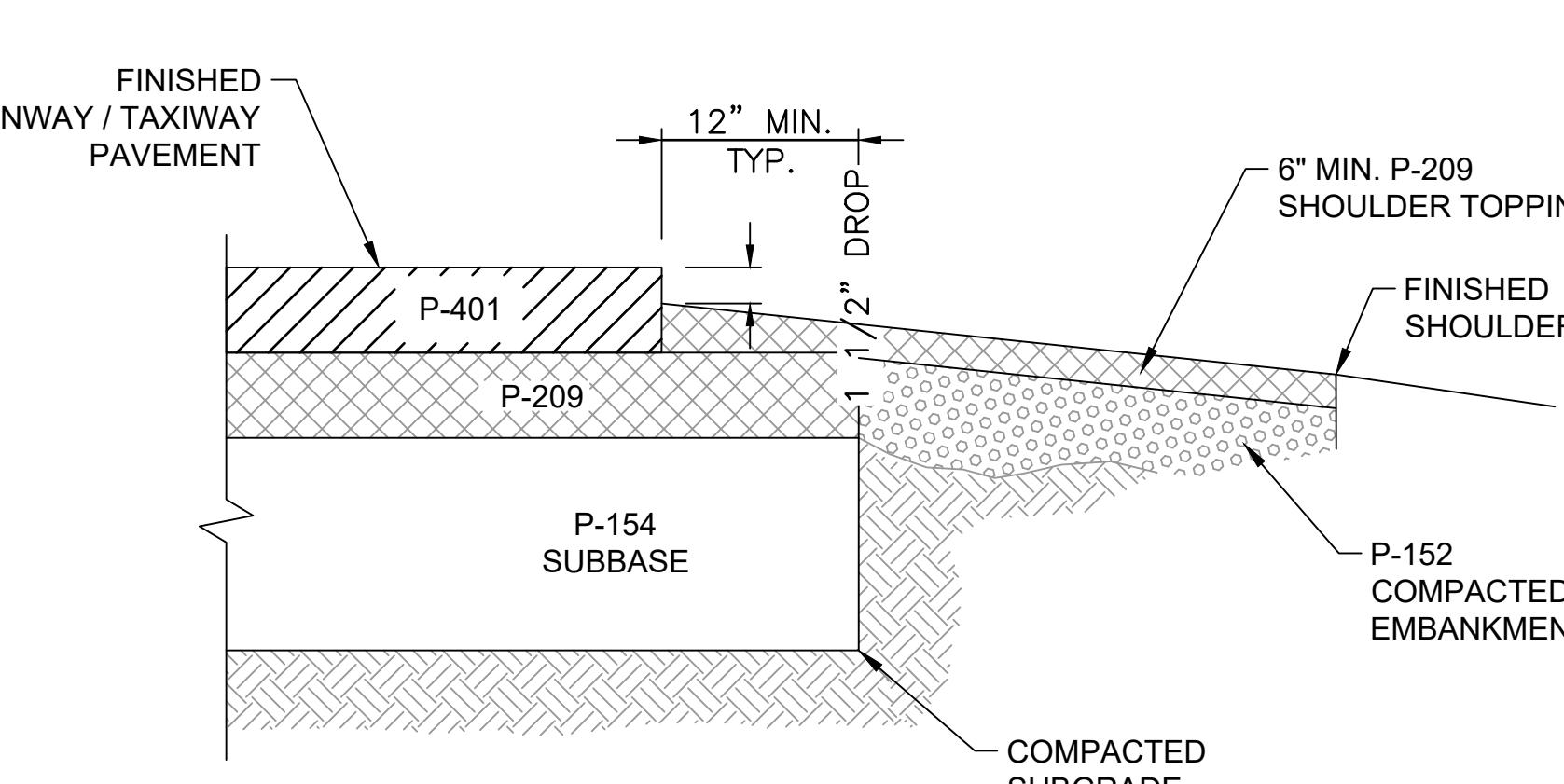
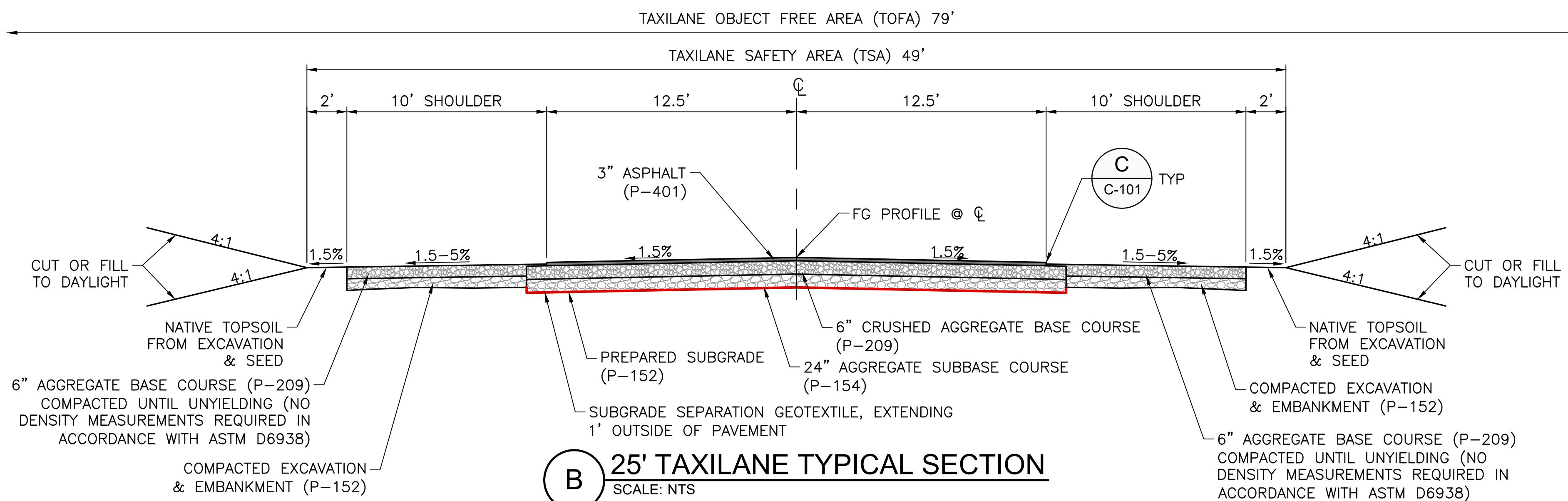
TAXILANE OBJECT FREE AREA (TOFA) 115'

TAXILANE SAFETY AREA (TSA) 79'



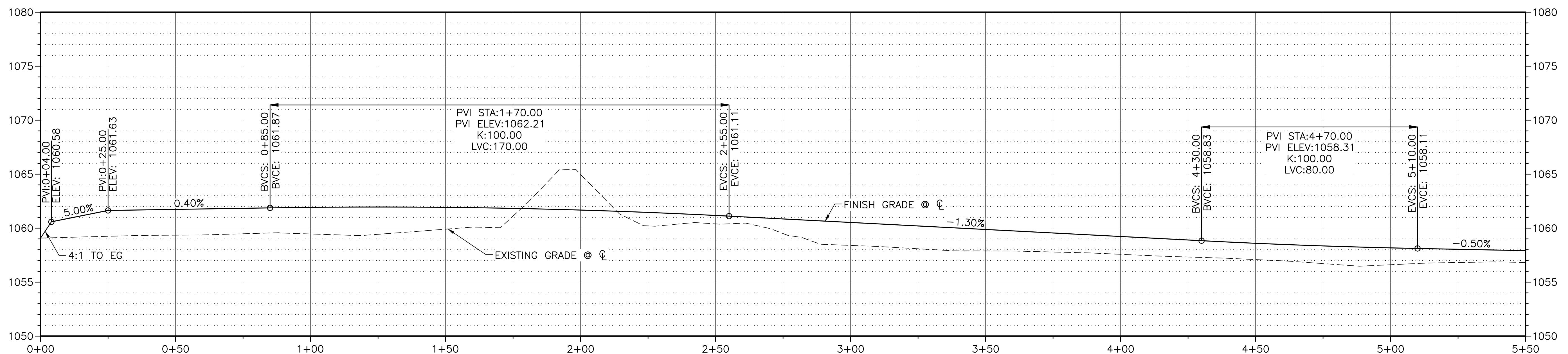
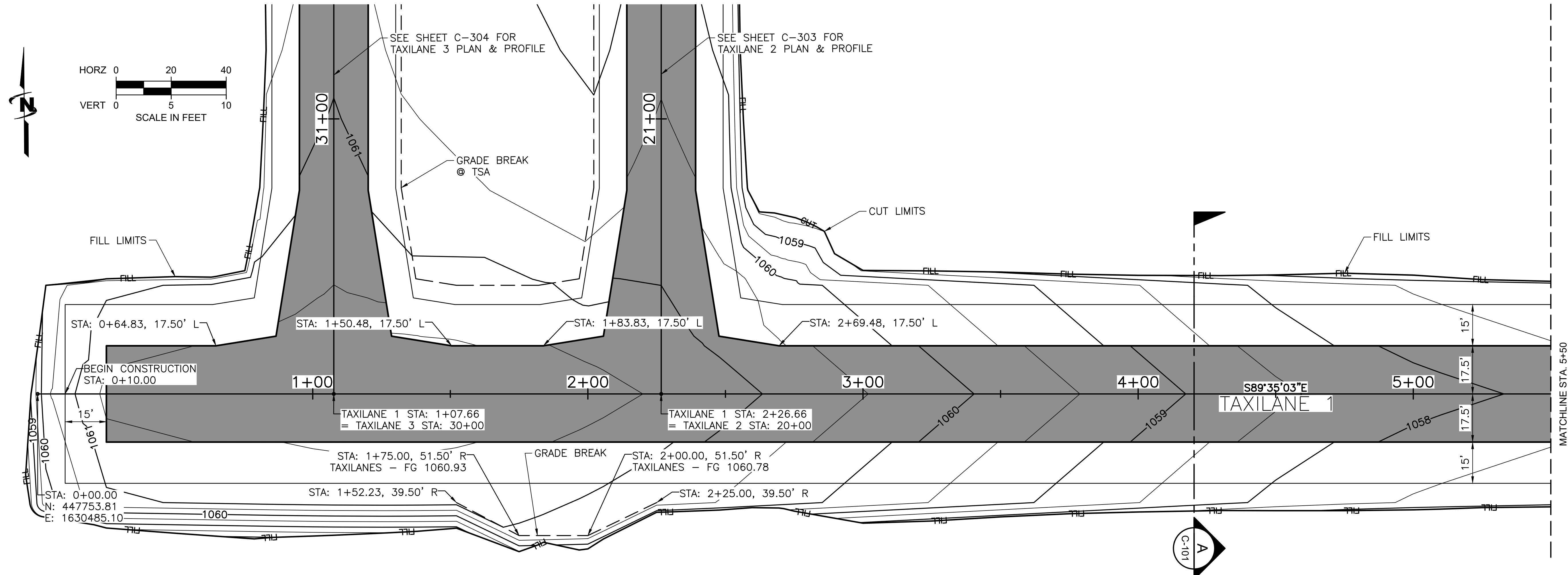
TAXILANE OBJECT FREE AREA (TOFA) 79'

TAXILANE SAFETY AREA (TSA) 49'

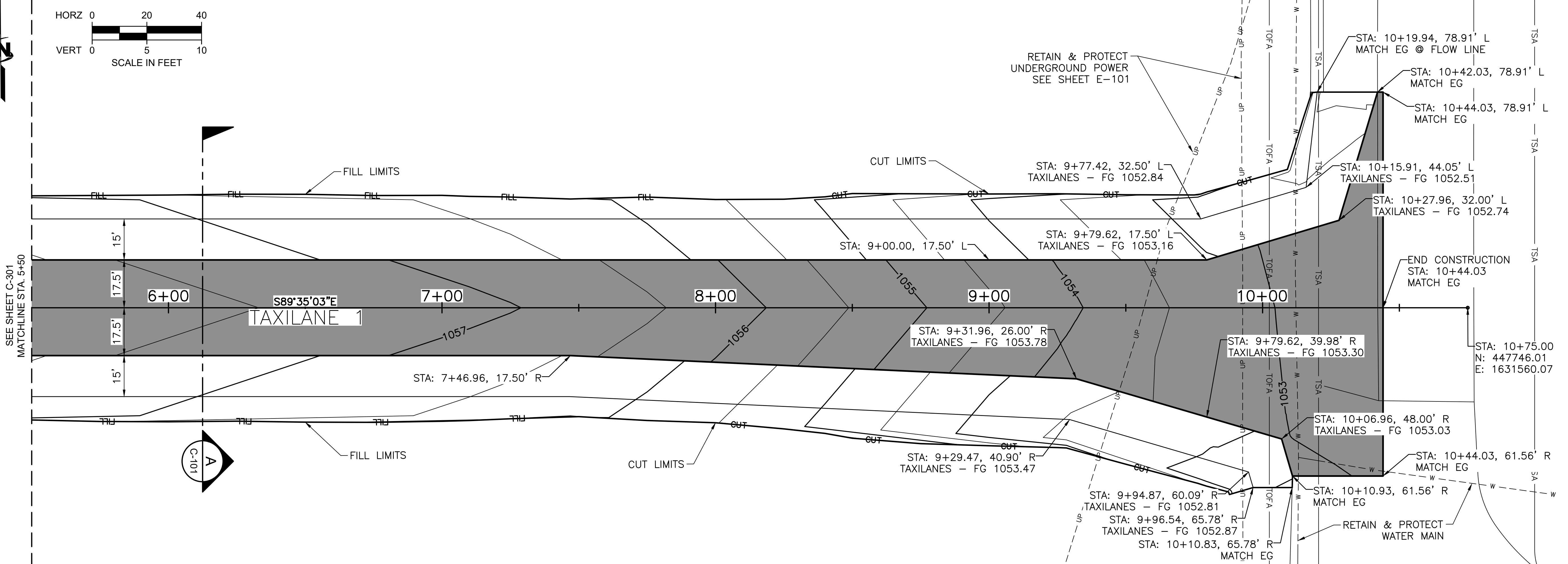


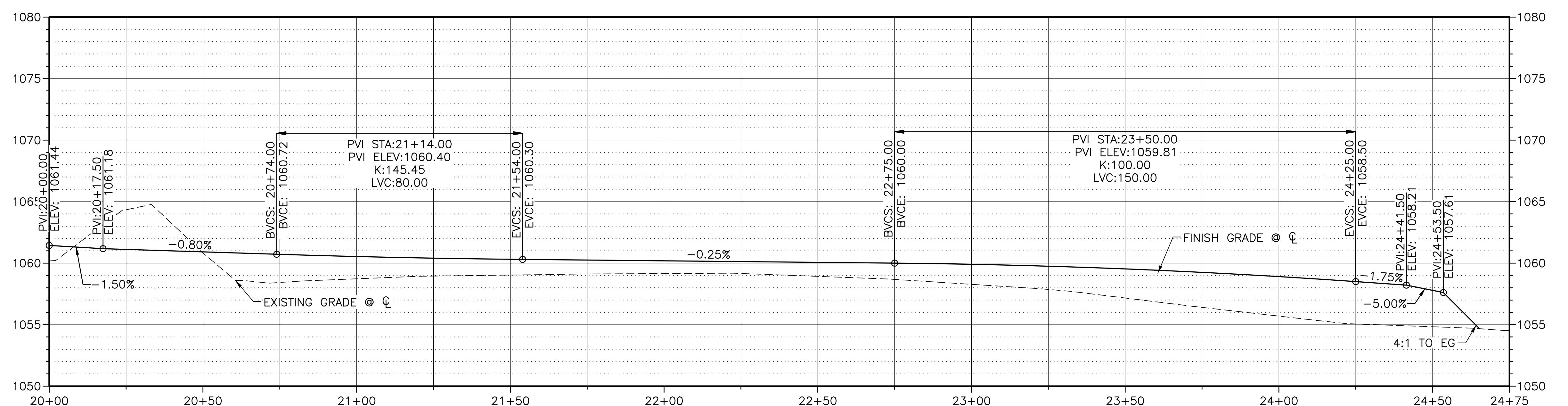
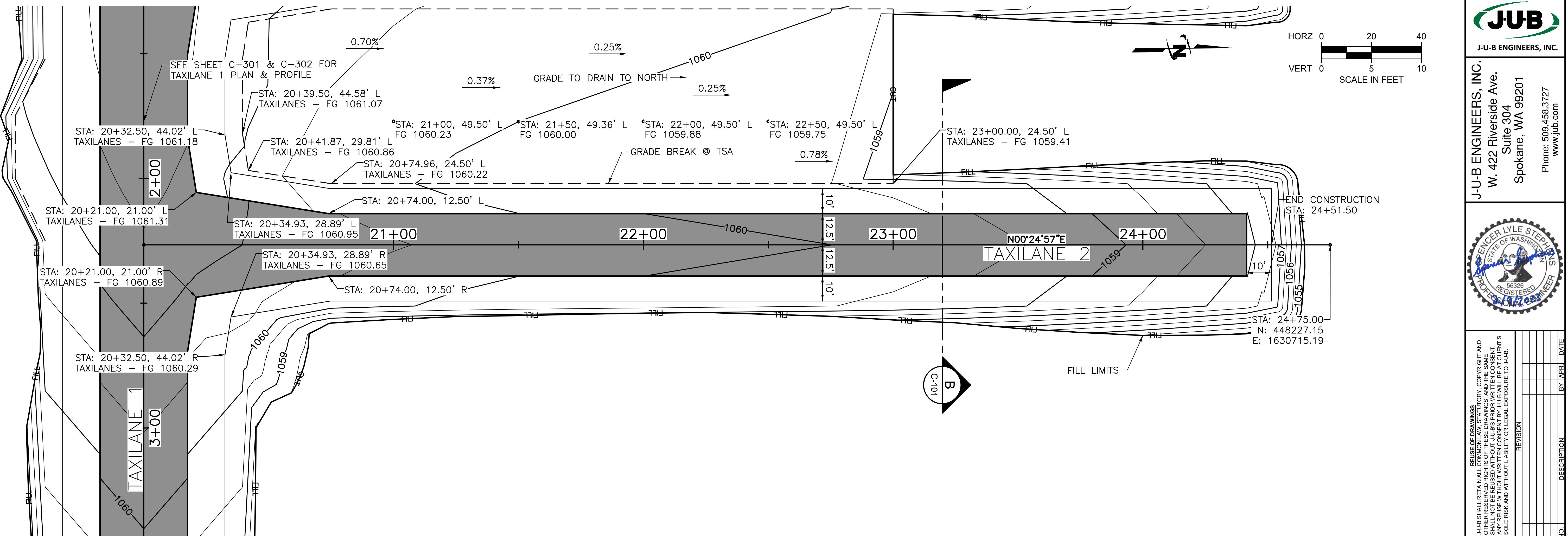
NOTE: THE 1 1/2" PAVEMENT EDGE DROP IS INTENDED TO BE USED
BETWEEN PAVED AND UNPAVED SURFACES.

C TAXILANE EDGE DETAIL
SCALE: NTS

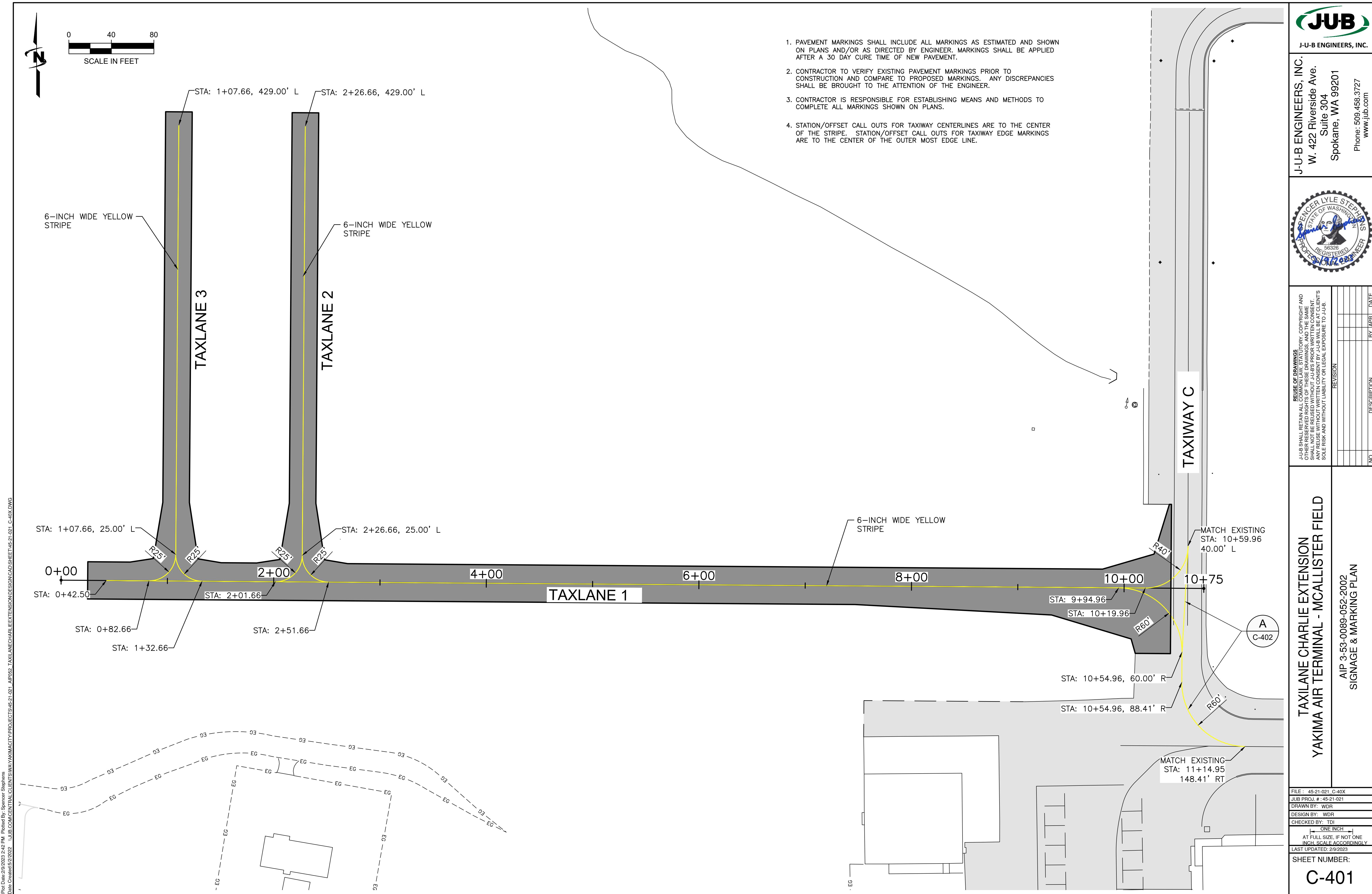


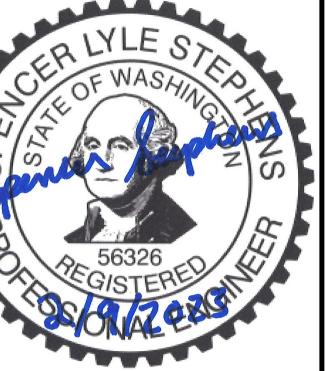
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Date Created:5/2/2022 \JUB.COM\CENTRAL\CLIENTS\\



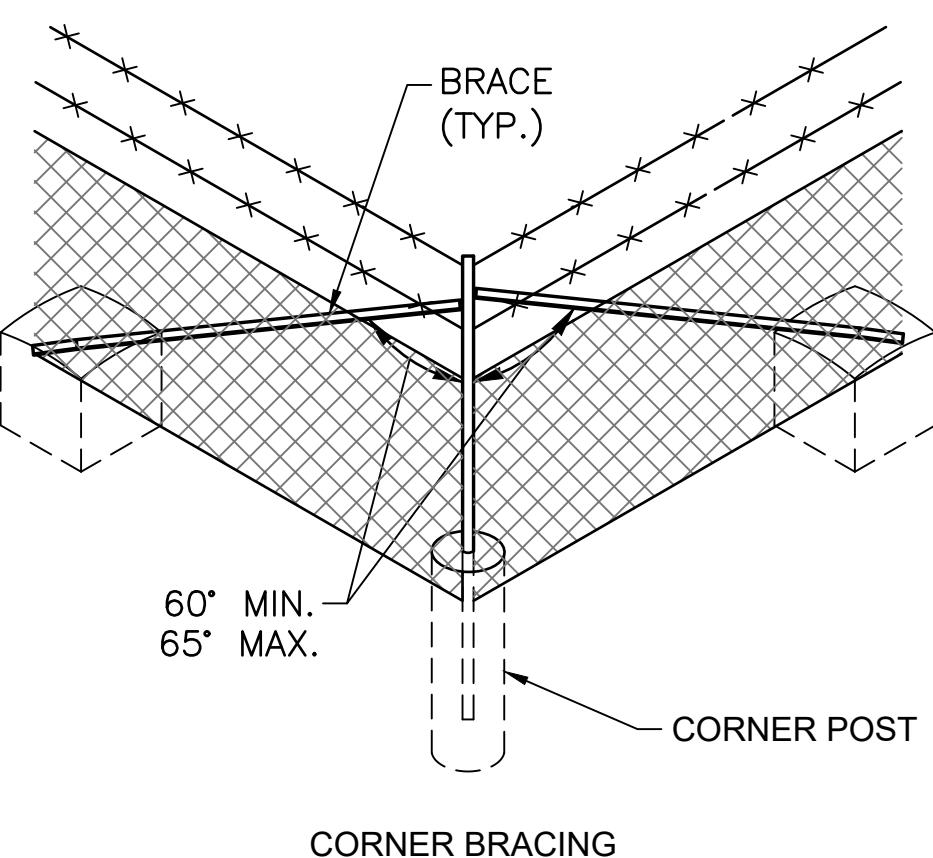
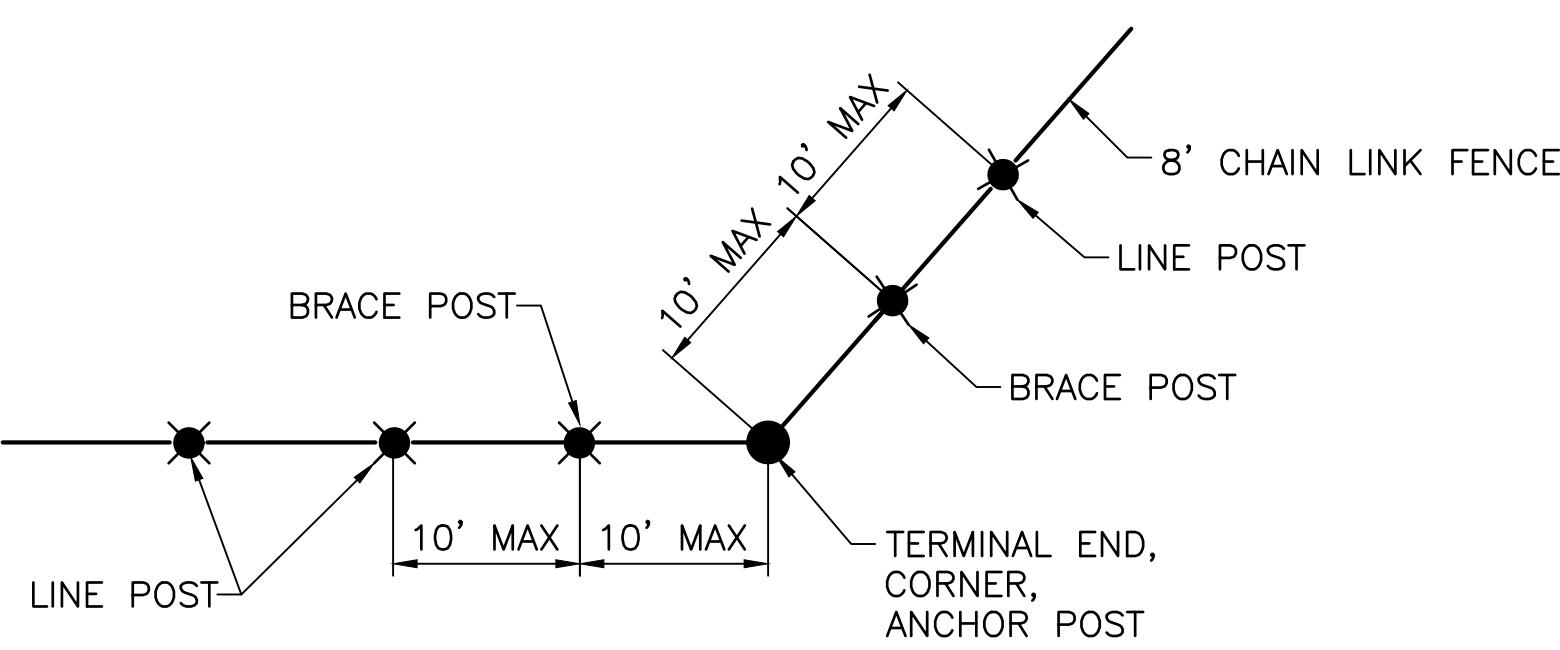


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REGISTERED

2/17/2023

REVISION

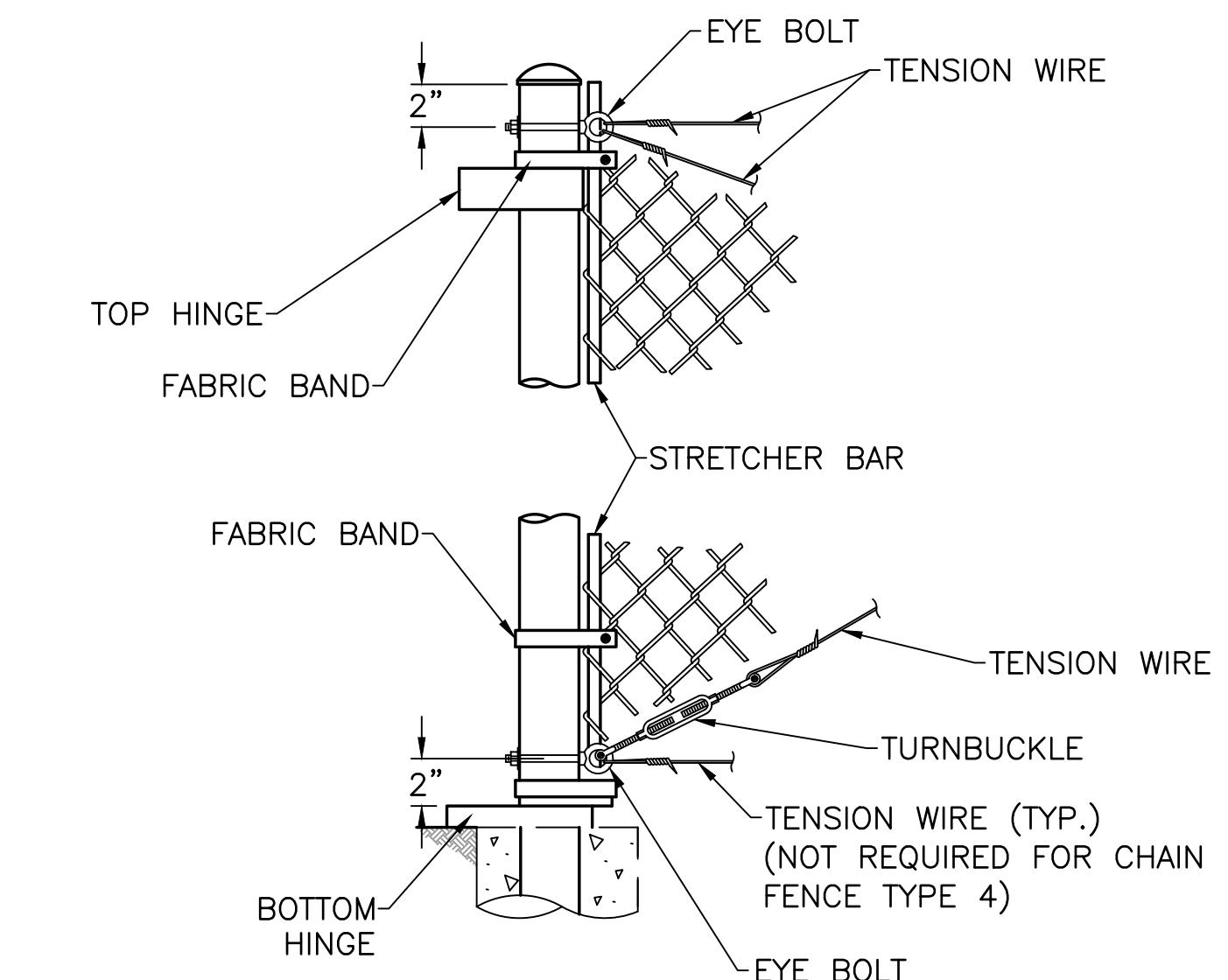


A TYPICAL FENCE CORNER LAYOUT

SCALE: NTS

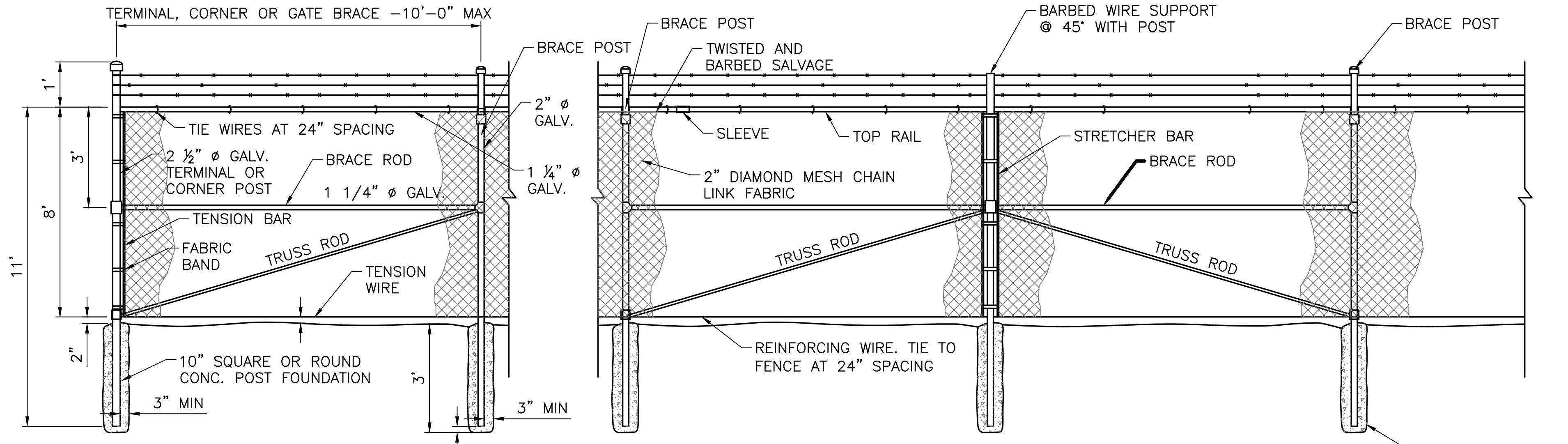
B CORNER BRACING DETAIL (CHAIN LINK FENCE)

SCALE: NTS



C GATE POST DETAIL

SCALE: NTS

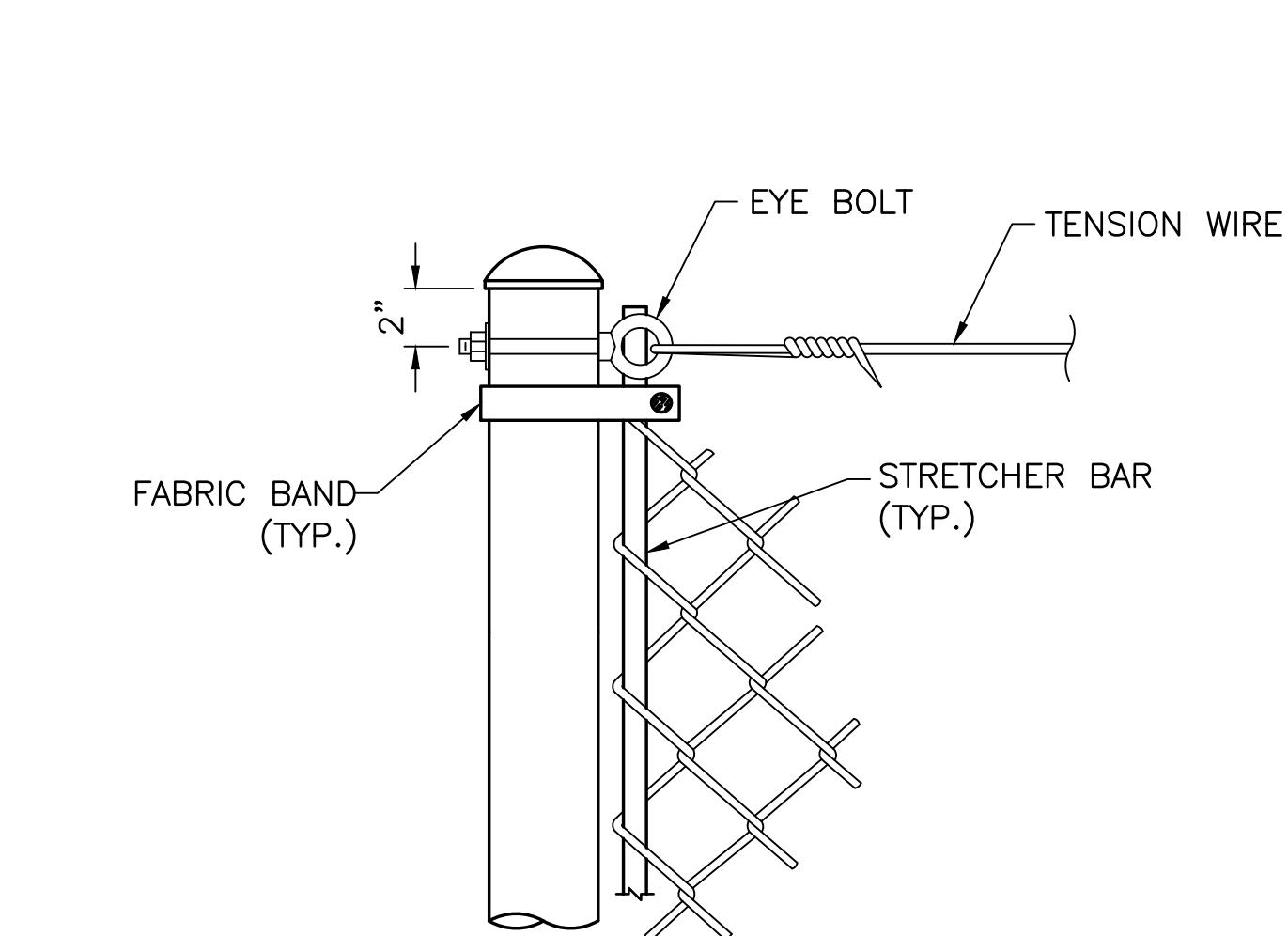


D TYPICAL 8' SECURITY CHAINLINK FENCE

SCALE: NTS

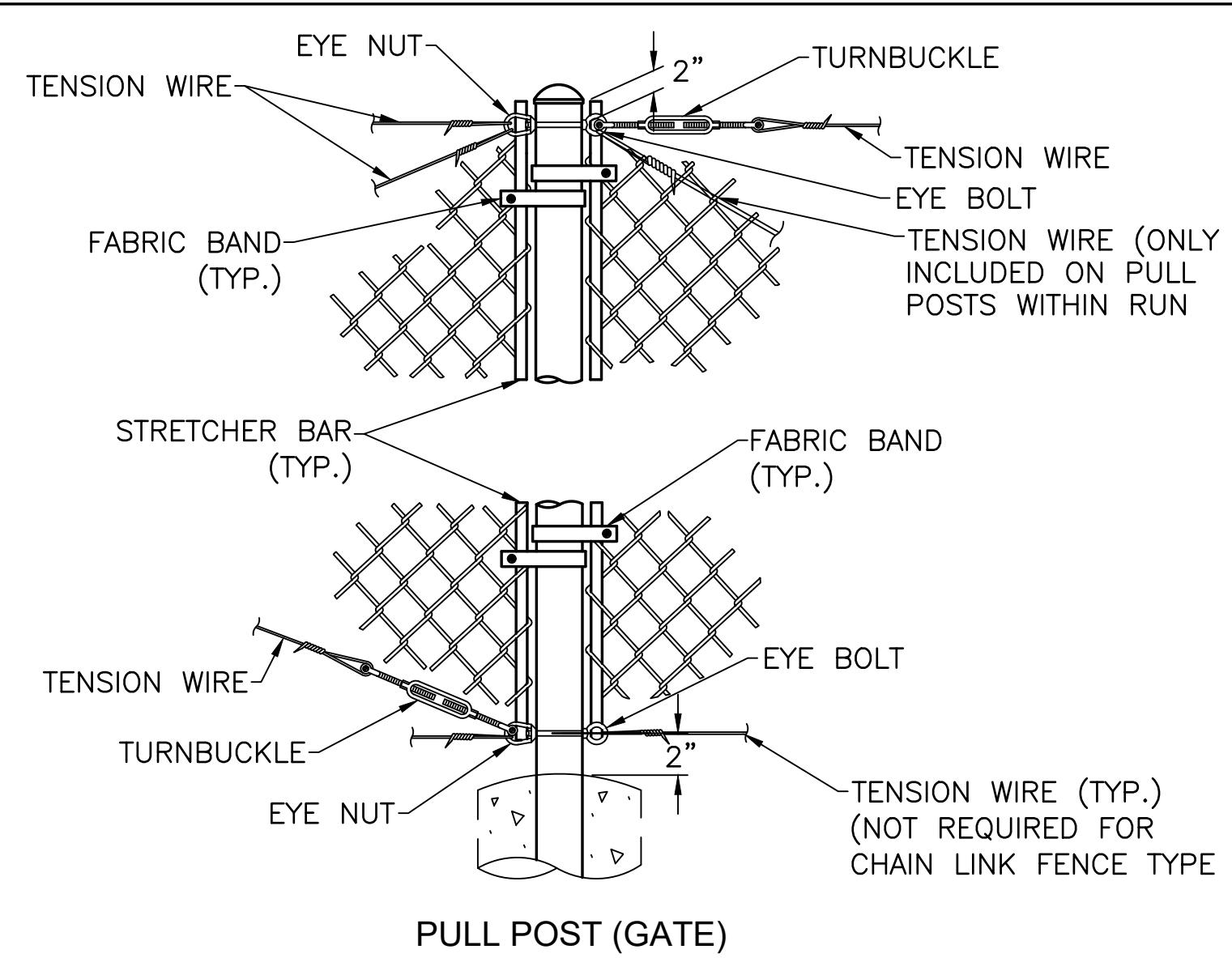
NOTES:

1. FABRIC BANDS SHALL BE PLACED AT 15" MAX SPACING



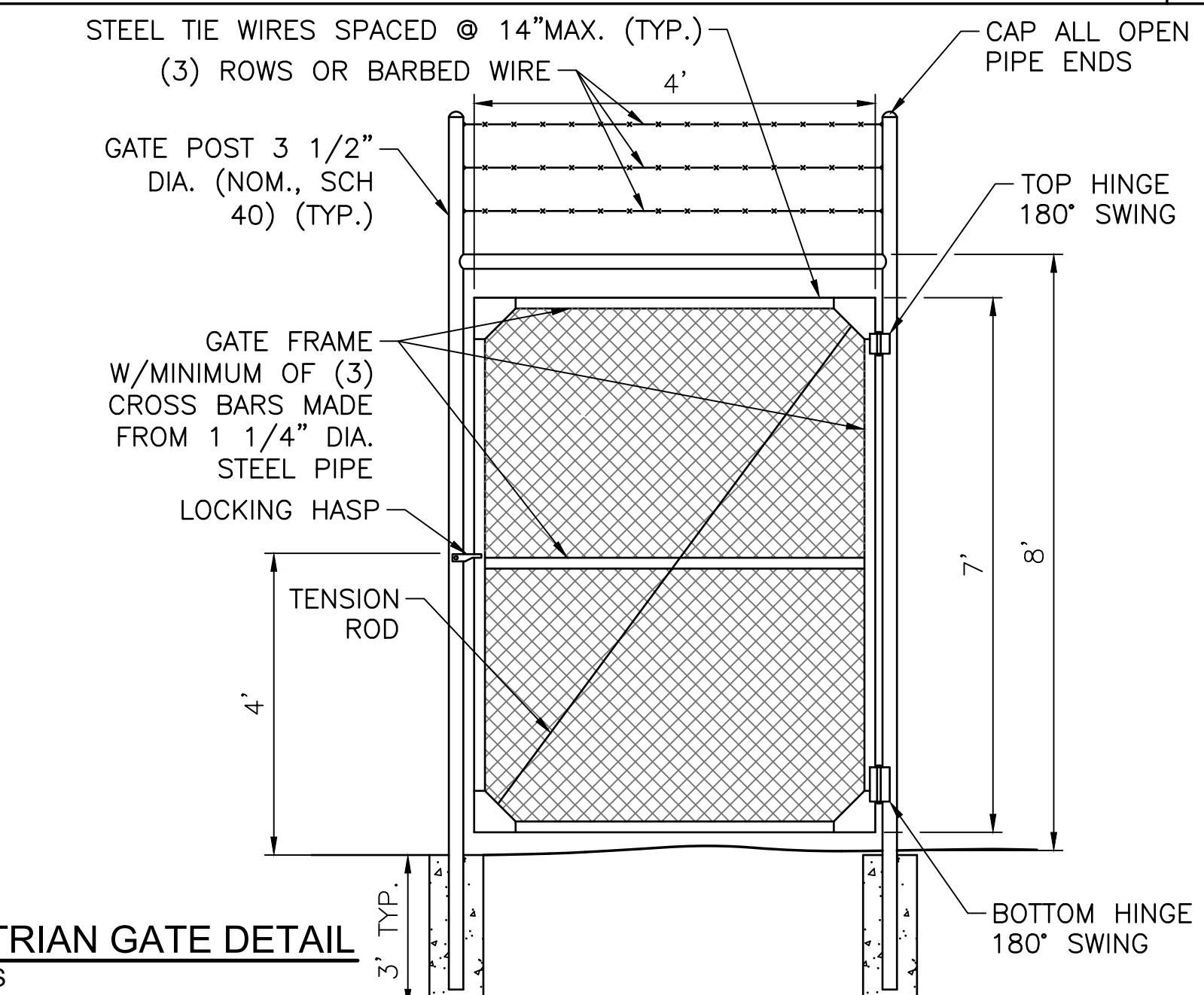
E END OR CORNER (BRACE) DETAIL

SCALE: NTS



F PULL POST (GATE) DETAIL

SCALE: NTS



G PEDESTRIAN GATE DETAIL

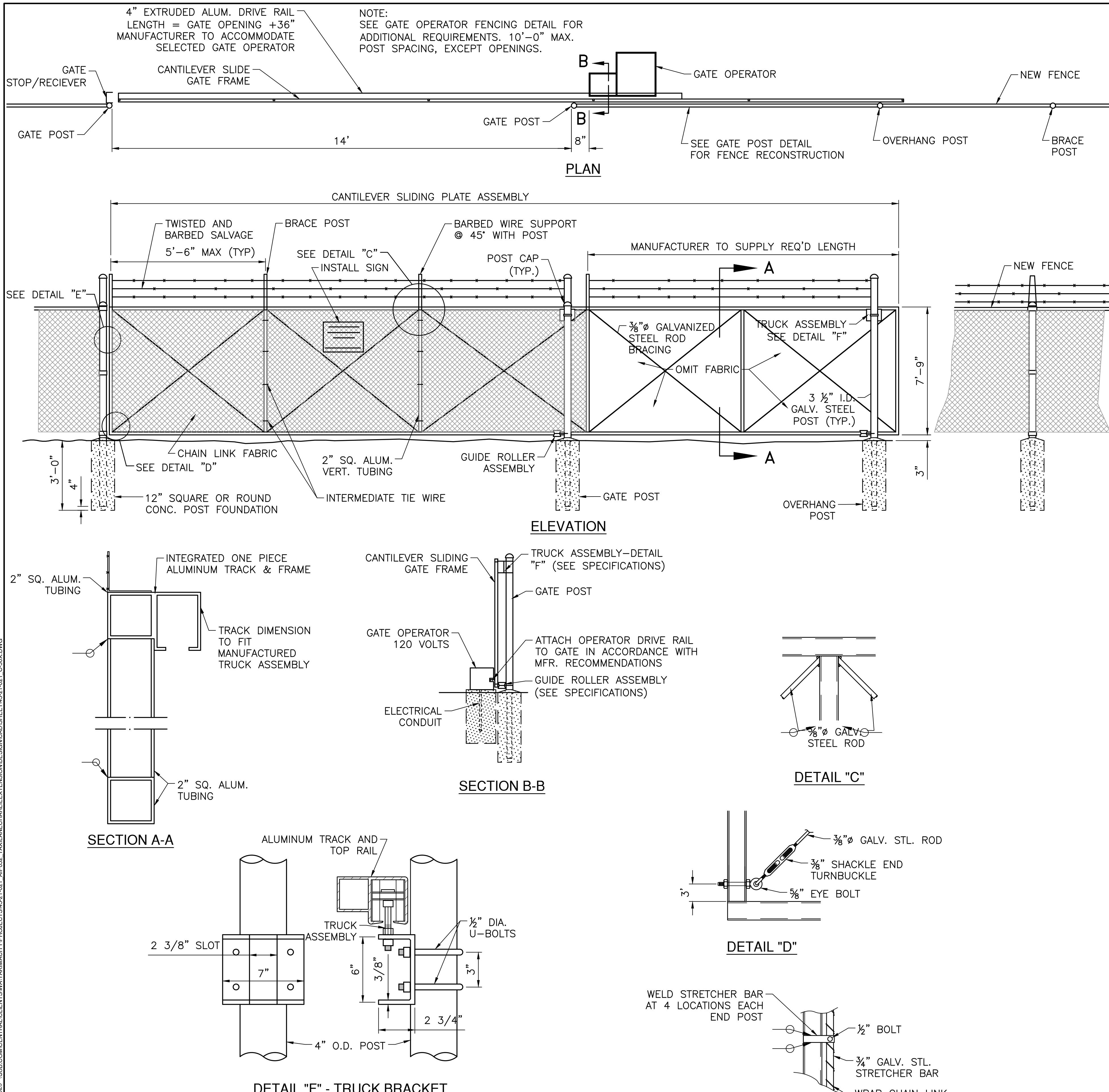
SCALE: NTS

| | |
|---|-------------|
| REUSE OF DRAWINGS | |
| J-U-B SHALL RETAIN ALL COMMON LAW, STATUTORY, COPYRIGHT AND OTHER RESERVED RIGHTS OF THESE DRAWINGS AND THE SAME SHALL NOT BE REUSED WITHOUT J-U-B'S PRIOR WRITTEN CONSENT. ANY REUSE WITHOUT J-U-B'S PRIOR WRITTEN CONSENT IS UNLAWFUL AND SUBJECT TO LIABILITY FOR LEGAL EXPENSES TO J-U-B. | |
| NO. | DESCRIPTION |
| | BY API DATE |
| | REVISION |

TAXILANE CHARLIE EXTENSION
YAKIMA AIR TERMINAL - MCALLISTER FIELD

AIP 3-53-0089-052-2002
FENCING DETAILS

FILE #: 45-21-021 C-50X
 JUB PROJ. #: 45-21-021
 DRAWN BY: WDR
 DESIGN BY: WDR
 CHECKED BY: ???
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 AT FULL SIZE, IF NOT ONE
 INCH, SCALE ACCORDINGLY
 LAST UPDATED: 2/9/2023
 SHEET NUMBER:
C-502

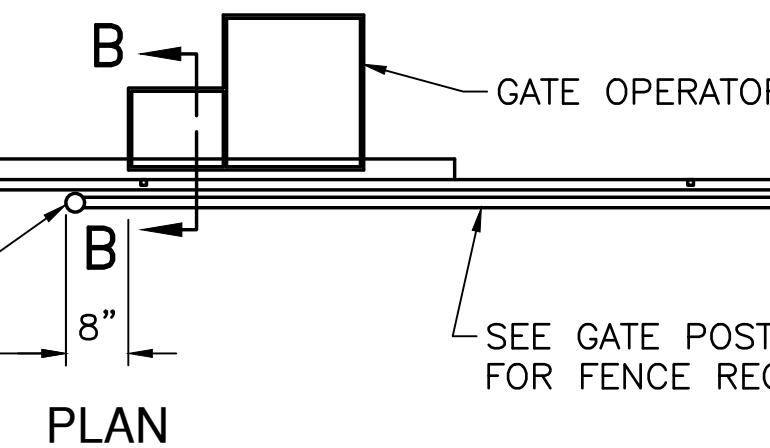


A CANTILEVER SLIDING GATE

SCALE: NTS

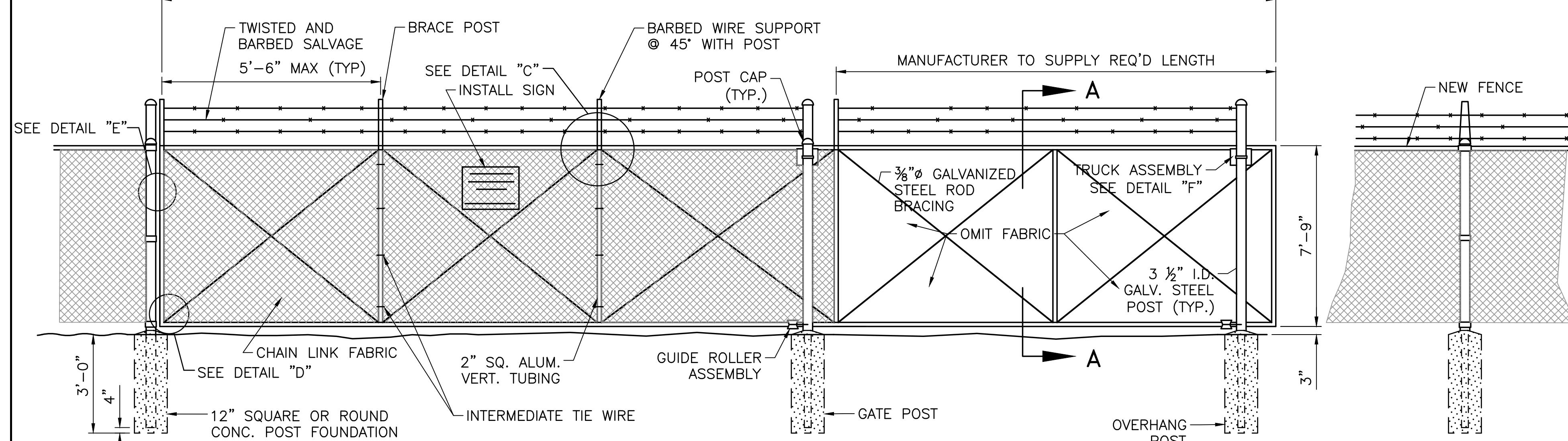
4" EXTRUDED ALUM. DRIVE RAIL LENGTH = GATE OPENING +36" MANUFACTURER TO ACCOMMODATE SELECTED GATE OPERATOR

NOTE: SEE GATE OPERATOR FENCING DETAIL FOR ADDITIONAL REQUIREMENTS. 10'-0" MAX. POST SPACING, EXCEPT OPENINGS.

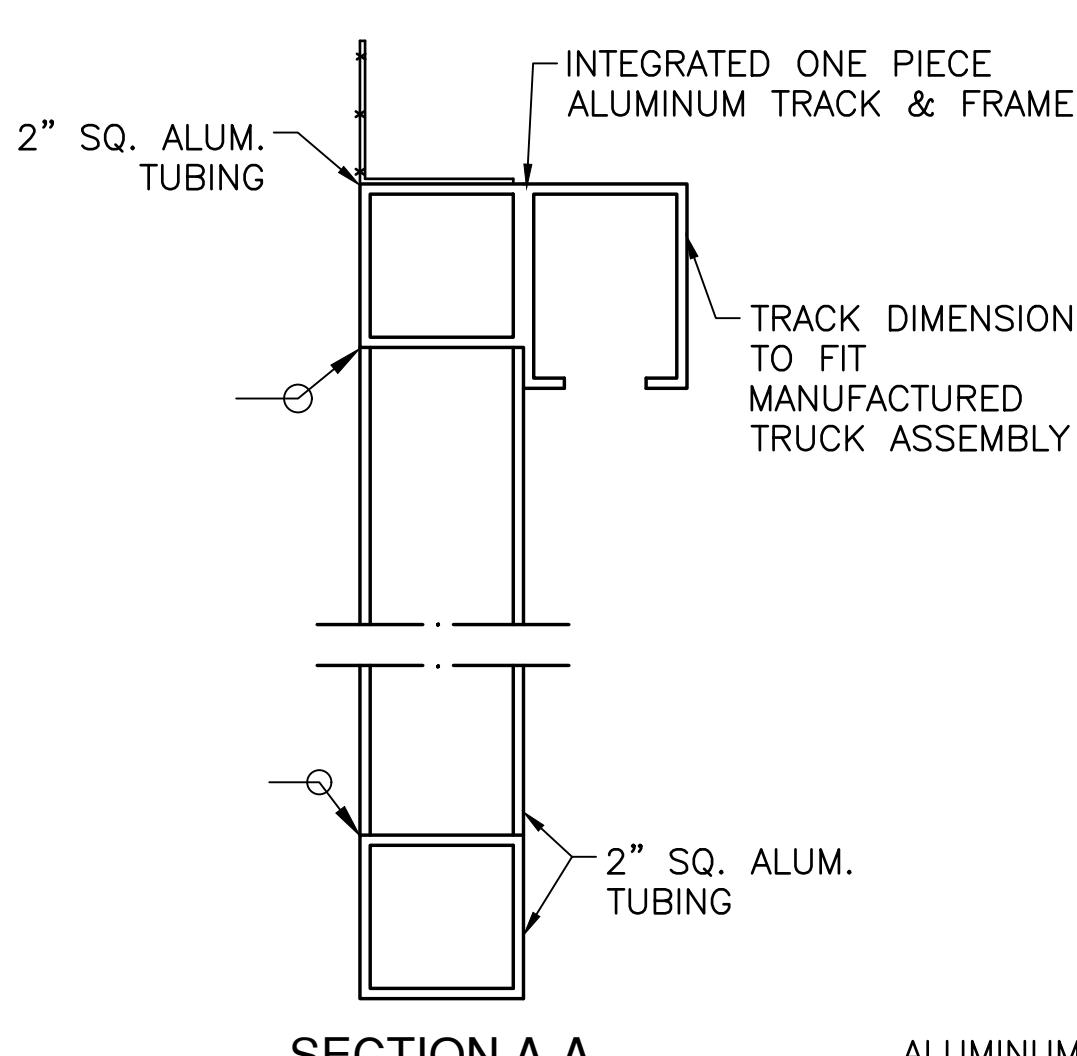


PLAN

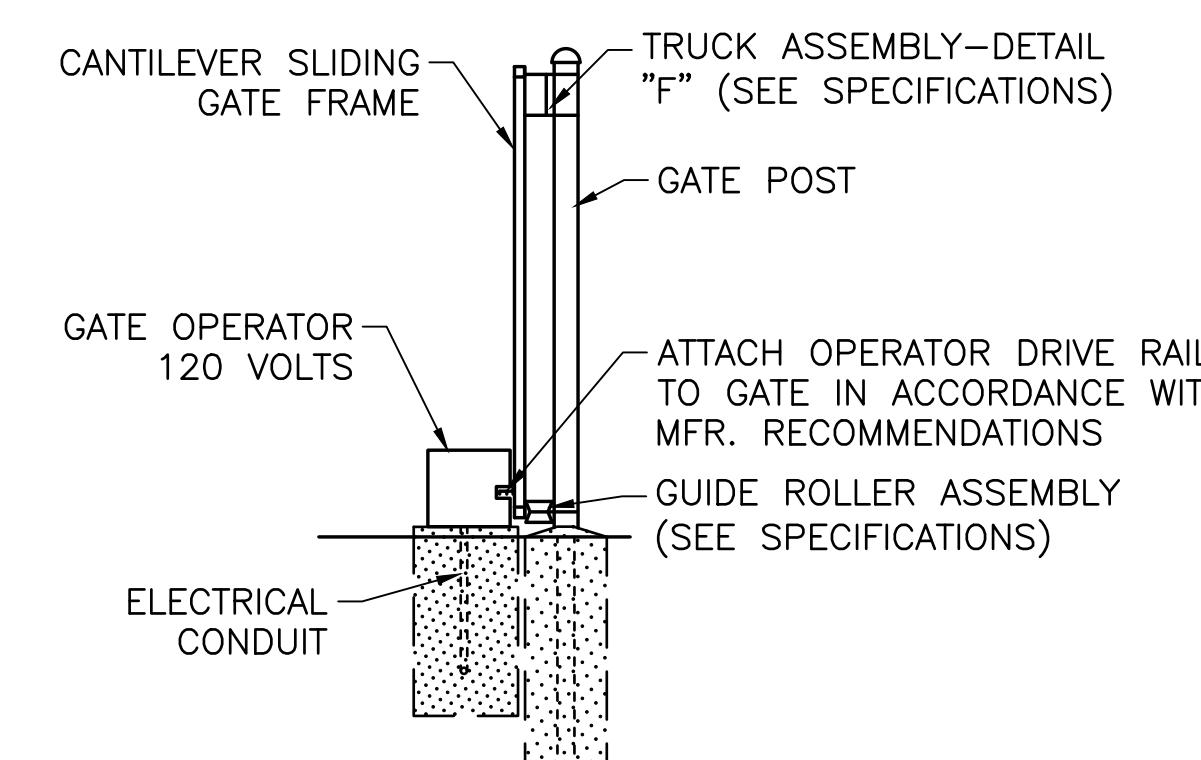
CANTILEVER SLIDING PLATE ASSEMBLY



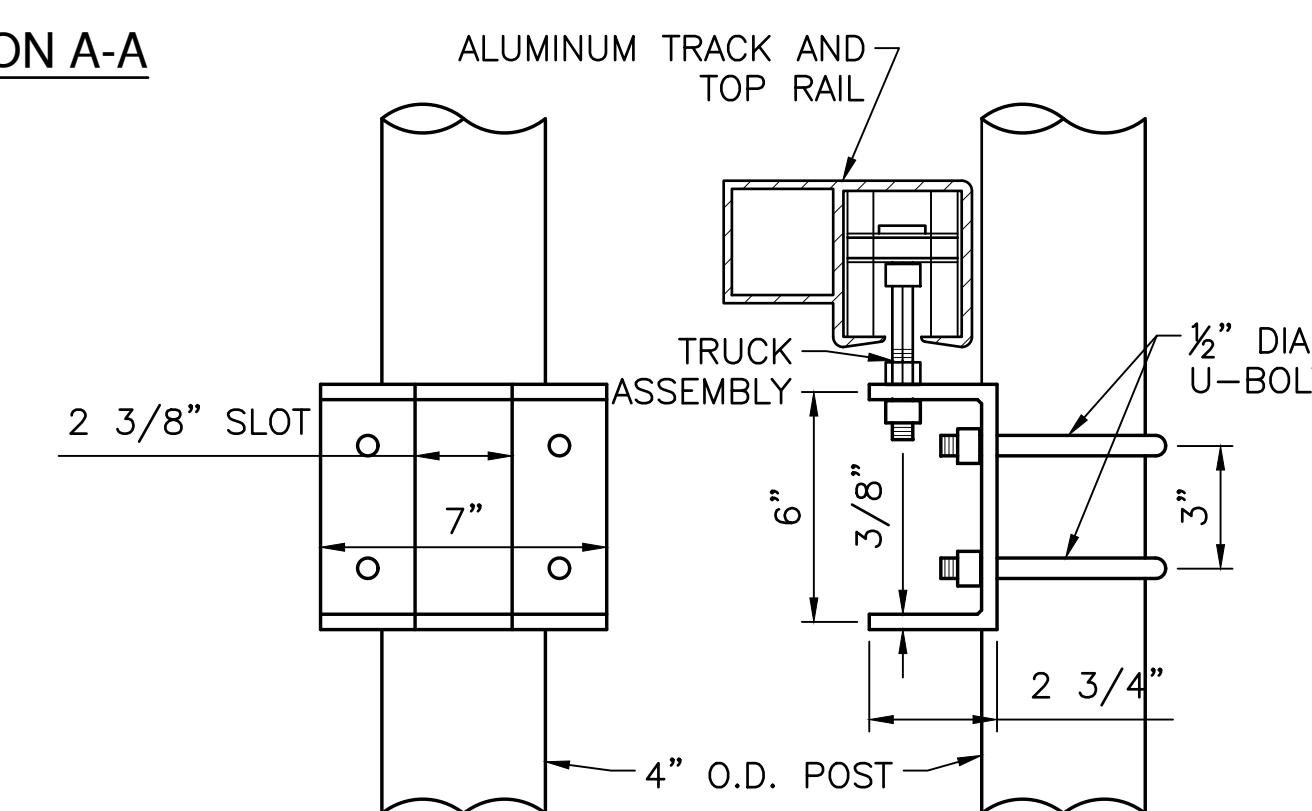
ELEVATION



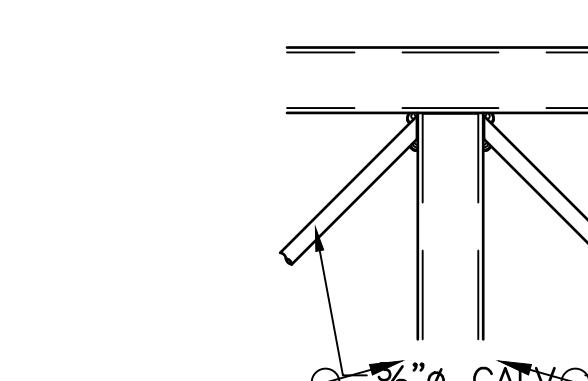
SECTION A-A



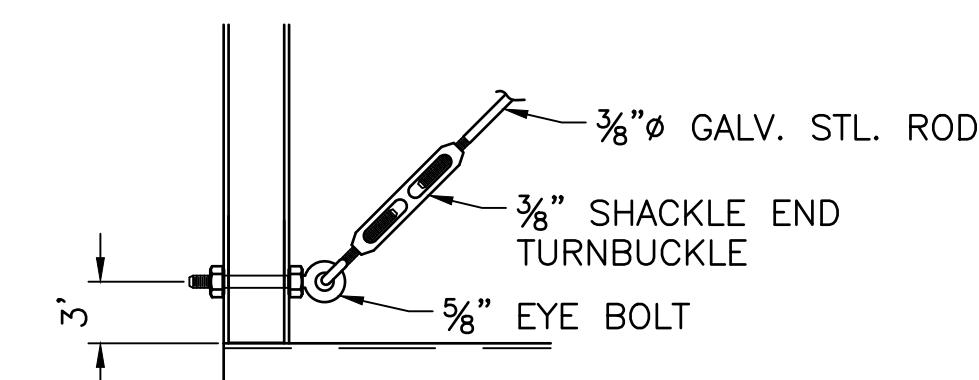
SECTION B-B



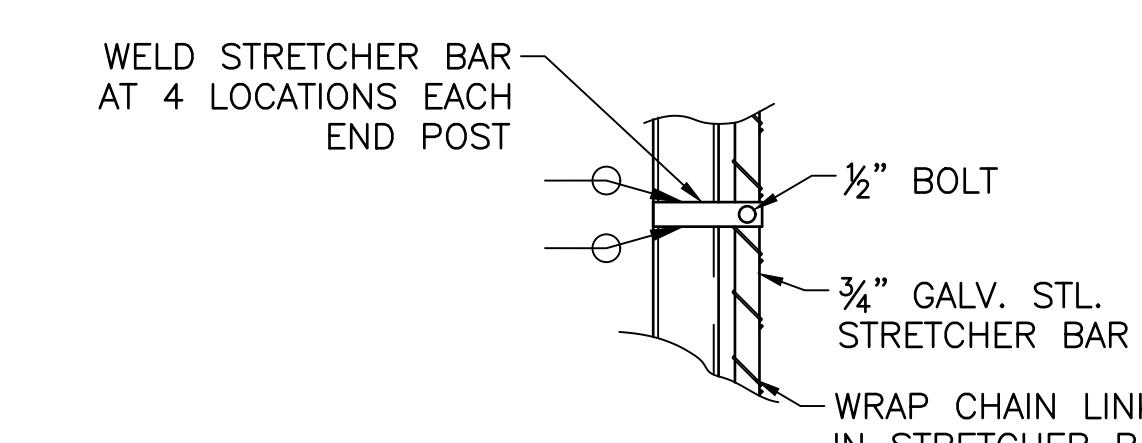
DETAIL "F" - TRUCK BRACKET



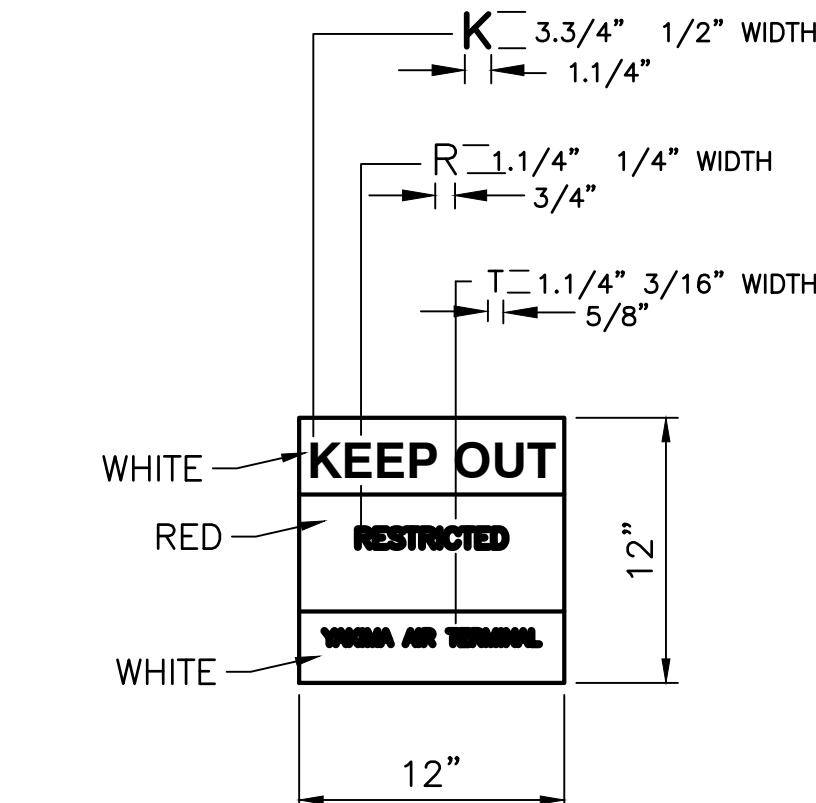
DETAIL "C"



DETAIL "D"



DETAIL "E"

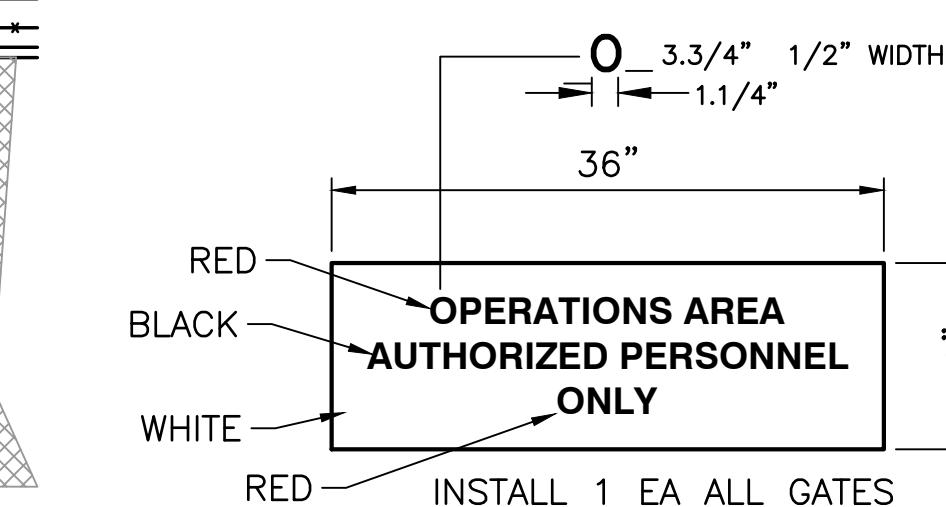


WHITE
RED
WHITE
KEEP OUT
RESTRICTED
YAKIMA AIR TERMINAL

12"

12"

INSTALL 1 EA. PER NEW FENCE SYSTEM AND ALL NEW GATES



RED
BLACK
WHITE
OPERATIONS AREA
AUTHORIZED PERSONNEL
ONLY
RED
INSTALL 1 EA ALL GATES



BLACK LETTERS
YELLOW BACKGROUND
AUTOMATIC GATES
DO NOT LEAN ON
YAKIMA AIR TERMINAL

18"

12"

DETAIL "E"

NOTE:
SOME OF THE EXISTING DETAILS ARE SHOWN FOR CLARITY.
EXISTING FENCE AND GATE CONTROLLER TO REMAIN.



REUSE OF DRAWINGS
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OTHER RESERVED RIGHTS OF THESE DRAWINGS AND THE SAME
SHALL NOT BE REUSED WITHOUT JUB'S PRIOR WRITTEN CONSENT
EXCEPT AS PROVIDED IN THE AGREEMENT. JUB WILL LEAVE NO MARKS
ON DRAWINGS UNLESS OTHERWISE AGREED. EXISTING FENCE AND GATE
CONTROLLER TO REMAIN

TAXILANE CHARLIE EXTENSION
YAKIMA AIR TERMINAL - MCALLISTER FIELD

AIP 3-53-0089-052-2002
FENCING DETAILS

C-503

- SIGN NOTES:
1. SIGNS SHALL BE CONSTRUCTED OF ALUMINUM SHEETING CONFORMING TO ASTM B 209 ALLOY6061-T6 OF ALLOY 5052-H36 AND SHALL BE TREATED WITH AN ALLODINE 1200 APPLICATION.
 2. PAINT SHALL CONFORM WITH WSDOT SPECIFICATIONS FOR ALUMINUM SHEET SIGNS.
 3. THE SIGNS SHALL BE ANCHORED TO THE AVIATION SIDE OF THE CHAIN LINK FABRIC FACING THE PUBLIC SIDE USING 9 GAGE GALVANIZED WIRE MEETING AASHTO M 279, GALVANIZING CLASS 1. THE SIGNS SHALL BE ANCHORED IN ALL FOUR CORNERS.
 4. (1) SIGN SHALL BE PLACED IN THE CENTER OF SECURITY GATE.

NOTE:
SOME OF THE EXISTING DETAILS ARE SHOWN FOR CLARITY.
EXISTING FENCE AND GATE CONTROLLER TO REMAIN.

0 40 80
SCALE IN FEET



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ANY REUSE WITHOUT CONSENT BY J-U-B WILL LEAD TO LIABILITY OR LEGAL EXPOSURE TO J-U-B.
REGISTERED
BY SPENCER LYLE STEPHENS
STATE OF WASHINGTON
CHARTERED 1917
ENGINEER SINCE 1917

REVISION
NO. _____
DESCRIPTION BY APRIL DATE

FILE: 45-21-021 C-60X
JUB PROJ. #: 45-21-021
DRAWN BY: WDR
DESIGN BY: WDR
CHECKED BY: TDI
ONE INCH
AT FULL SIZE, IF NOT ONE
INCH, SCALE ACCORDINGLY
LAST UPDATED: 1/26/2023
SHEET NUMBER:
C-601

TAXILANE CHARLIE EXTENSION YAKIMA AIR TERMINAL - MCALLISTER FIELD

AIP 3-53-0089-052-2002
FENCING PLAN

NOTE:
CONTRACTOR SHALL ADJUST ALIGNMENT
OF THE LAST 100 FEET OF FENCE LINE
TO MEET AN EXISTING POST.

TAXILANE 1



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 SOLELY TO J-U-B AND WITHOUT LIMITATION OR LIABILITY FOR J-U-B'S
 EXPOSURE TO J-U-B.

REVISION

BY API#, DATE

NO. DESCRIPTION

NO. DRAWING

FILE#

JUB PROJ. #

DRAWN BY

DESIGN BY

CHECKED BY

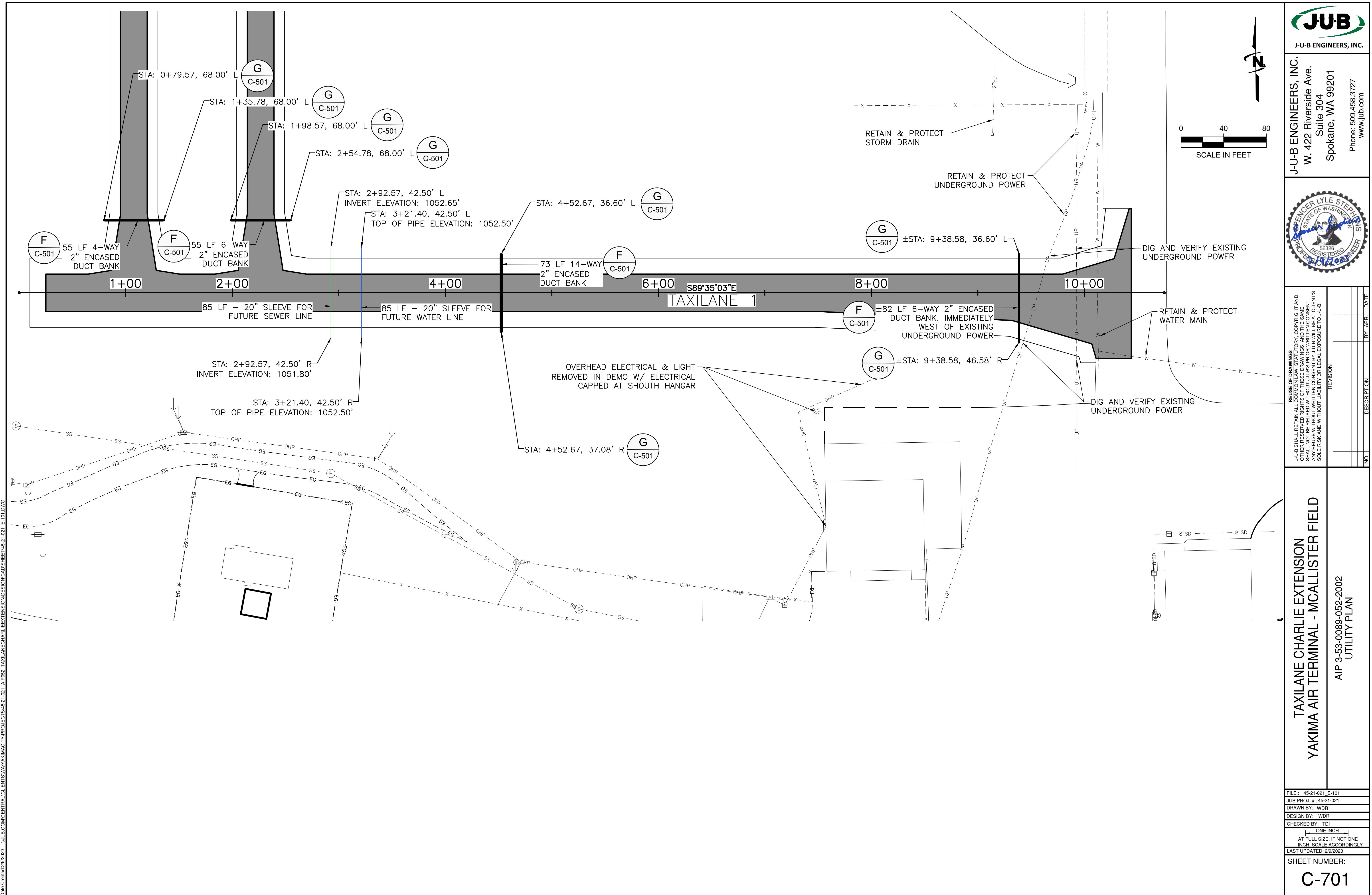
ONE INCH

AT FULL SIZE, IF NOT ONE
INCH, SCALE ACCORDINGLY

LAST UPDATED: 2/9/2023

SHEET NUMBER:

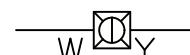
C-701



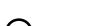


02/09/2

SITE PLAN SYMBOLS

| <u>NEW</u> | <u>EXISTING</u> | |
|---|---|---|
| — P // | — P // | FOR AIRFIELD, NEW/EXISTING BURIED SERIES CIRCUIT CONDUIT AND CONDUCTORS. SLASHES INDICATE NUMBER OF CONDUCTORS. |
| — P // | — P // | FOR AIRFIELD, NEW SERIES CIRCUIT CONDUCTORS IN EXISTING CONDUIT. SLASHES INDICATE NUMBER OF CONDUCTORS. |
| — — P — — | — — P — — | EXPOSED TEMPORARY AIRFIELD CONDUCTOR |
| — E — — | — E — — | FOR AIRFIELD, NEW/EXISTING BURIED CONSTANT VOLTAGE CONDUIT AND/OR CONDUCTORS |
| — A — — | — A — — | FOR AIRFIELD, TAXIWAY A EXISTING BURIED CONSTANT VOLTAGE CONDUIT AND/OR CONDUCTORS |
| — B — — | — B — — | FOR AIRFIELD, TAXIWAY B EXISTING BURIED CONSTANT VOLTAGE CONDUIT AND/OR CONDUCTORS |
| — 4 — — | — 4 — — | FOR AIRFIELD, RUNWAY 4/22 EXISTING BURIED CONSTANT VOLTAGE CONDUIT AND/OR CONDUCTORS |
| W  Y | W  Y | NEW/EXISTING RUNWAY LIGHT ELEVATED |
| W  Y | W  Y | NEW/EXISTING RUNWAY LIGHT FLUSH MOUNT |
| COLOR LEGEND: | | |
| W | — WHITE/CLEAR | |
| Y | — YELLOW | |
| R | — RED | |
| G | — GREEN | |
| O | — OBSCURED | |
| —  — | —  — | NEW/EXISTING TAXIWAY EDGE LIGHT |
| —  — | —  — | NEW/EXISTING TAXIWAY LIGHT FLUSH MOUNT |
|  |  | NEW/EXISTING ILLUMINATED AIRFIELD SIGN |
|  |  | NEW/EXISTING ILLUMINATED AIRFIELD SIGN WITH DUAL BASECANS |
| /// | /// | DEMOLITION |
|  |  | NEW/EXISTING ELECTRICAL HANDHOLE |
|  |  | NEW/EXISTING BASECAN HANDHOLE |
|  |  | NEW/EXISTING LIGHTED WIND CONE |
|  |  | NEW/EXISTING REIL |
|  | | LIGHTED CLOSURE X |
|  |  | NEW/EXISTING DUCT MARKER |
|  |  | CONDUIT STUB UP |
|  |  | CONDUIT TURNING UP |
|  | | EXISTING, TO REMAIN AS-IS |
|  | | EQUIPMENT IDENTIFIER |

BUILDING PLAN SYMBOLS

| | | | |
|--|--|---|------------------------------------|
|  | HOMERUN |  | CONDUIT TURNING DOWN |
|  | CONDUIT TRANSITION, i.e. PVC TO GRS |  | CONDUIT TURNING UP |
|  | EQUIPMENT, PURPOSE AS NOTED |  | CONDUIT STUBBED UP AND CAPPED |
|  WP | DUPLEX RECEPTACLE, GFCI WP SUBSCRIPT INDICATES WATERPROOF WHILE IN USE |  | CONNECTION TO EQUIPMENT |
|  | GROUND |  | EXISTING EQUIPMENT TO REMAIN AS-IS |
|  | GROUND ROD | | |

ONE-LINE SYMBOLS

| | | | |
|--|-------------------|--|----------------------------|
| | DEMOLITION | | CONDUIT STUB |
| | FUSE | | METER |
| | GENERATOR | | CURRENT TRANSFORMER |
| | GROUND | | CONSTANT CURRENT REGULATOR |
| | MOTOR | | FUSED SWITCH |
| | POWER TRANSFORMER | | ANTENNA |
| | SWITCH | | GROUND ROD |
| | SURGE ARRESTER | | AVAILABLE FAULT CURRENT |

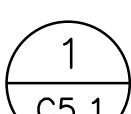
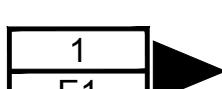
GENERAL NOTES

1. VERIFY LOCATIONS OF OTHER UTILITIES PRIOR TO COMMENCING WORK, PROVIDE REQUIRED CLEARANCES FROM OTHER UTILITIES, BUILDINGS AND FREESTANDING STRUCTURES DURING INSTALLATION OF CONDUITS, CABLES, ETC.
 2. ALL WORKMANSHIP AND MATERIALS SHALL CONFORM TO FAA DESIGN AND CONSTRUCTION STANDARDS, THE "WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION" (2023/LATEST EDITION), COPIES OF THE ABOVE DOCUMENTS SHALL BE AVAILABLE AT THE JOB SITE DURING CONSTRUCTION.
 3. ALL WIRING SHALL BE COPPER UNLESS NOTED OTHERWISE.

LEGEND (EXISTING)

| | |
|---------------------|----------------------------|
| ▲ | BRASS CAP SURFACE MONUMENT |
| ● | REBAR & CAP |
| ○ CO | SANITARY SEWER CLEANOUT |
| □ | STORM DRAIN MANHOLE |
| □ | CATCH BASIN |
| ☒ EJB | ELECTRICAL JUNCTION BOX |
| □ | SIGN |
| — SD — SD — | STORM LINE |
| — SS — SS — | SANITARY SEWER LINE |
| — W — W — | WATER LINE |
| — G — G — | GAS LINE |
| * — * — * | FENCE LINE |
| — P — — P — — P — — | AERIAL POWER LINE |
| — P — — P — — P — — | BURIED POWER LINE |
| — — — — — | PROPERTY LINE |
| -○- UP | UTILITY POLE |

LEGEND (PROPOSED)

| | | | |
|---|----------|---|---|
| — | — RSA — | — | RUNWAY SAFETY AREA |
| — | — ROFA — | — | RUNWAY OBJECT FREE AREA |
| — | — TSA — | — | TAXIWAY SAFETY AREA |
| — | — TOFA — | — | TAXIWAY OBJECT FREE AREA |
| — | — APCH — | — | PART 77 APPROACH SURFACE |
| — | — RPZ — | — | RUNWAY PROTECTION ZONE |
| —○— | —○— | — | CONSTRUCTION LIMIT BARRIER FENCE (ORANGE) |
| Ⓐ | | | BRASS CAP SURFACE MONUMENT |
| • | | | REBAR/CAP |
|  | | | DETAIL REFERENCE |
|  | | | IMAGE REFERENCE |

ABBREVIATIONS

| | | | |
|---------------|---|----------|---|
| A | AMPERE | MAX | MAXIMUM |
| AFF | ABOVE FINISHED FLOOR | MH | MANHOLE |
| AFG | ABOVE FINISHED GRADE | MIN | MINIMUM |
| AIC | AMPERE INTERRUPTING CAPACITY | MIRL | MEDIUM INTENSITY RUNWAY LIGHTING |
| AL | ALUMINUM | MLO | MAIN LUGS ONLY |
| AMP | AMPERE | MT | EMPTY |
| AP | ANGLE POINT | MV | MEDIUM VOLTAGE |
| ASOS | AUTOMATED SURFACE OBSERVATION SYSTEM | N | NEUTRAL |
| AWOS | AUTOMATED WEATHER OBSERVATION SYSTEM | NEMA | NATIONAL ELECTRICAL MANUFACTURERS ASSOCIATION |
| AUTO | AUTOMATIC | NFPA | NATIONAL FIRE PROTECTION ASSOCIATION |
| AWG | AMERICAN WIRE GAUGE | NIC | NOT IN CONTRACT - TO BE INSTALLED BY OTHERS |
| BKR | BREAKER | NTS | NOT TO SCALE |
| BLDG | BUILDING | OOS | OUT OF SERVICE |
| C | CONDUIT, CONTROL, CONDUCTOR | P | POLE, PUMP, POWER |
| CCR | CONSTANT CURRENT REGULATOR | PAPI | PRECISION APPROACH PATH INDICATOR |
| CKT | CIRCUIT | PB | PULL BOX, PUSHBUTTON |
| CL | CENTERLINE | PC | PHOTO-CELL |
| CMU | CONCRETE MASONARY UNIT | PH, Ø | PHASE |
| CT | CURRENT TRANSFORMER | PNL | PANEL |
| CU | COPPER | PR | PAIR |
| DIA | DIAMETER | PUD | PUBLIC UTILITY DISTRICT |
| DRM | DISTANCE REMAINING MARKER | PVC | POLYVINYL CHLORIDE |
| DWG | DRAWING | R | RADIUS |
| E, ELEC, ELEC | ELECTRICAL | RCPT | RECEPTACLE |
| EMT | ELECTRICAL METALLIC TUBING | REIL | RUNWAY END IDENTIFIER LIGHT |
| EP | EDGE OF PAVEMENT | RM | ROOM |
| EQUIP | EQUIPMENT | ROFA | RUNWAY OBJECT FREE AREA |
| EXIST | EXISTING | RPZ | RUNWAY PROTECTION ZONE |
| FAA | FEDERAL AVIATION ADMINISTRATION | RSA | RUNWAY SAFETY AREA |
| FLA | FULL LOAD AMPERES | RWY, R/W | RUNWAY |
| FLEX | FLEXIBLE | SCA | SHORT CIRCUIT AMPS |
| FT | FEET, FOOT | SCH | SCHEDULE |
| FU | FUSE | SCO | SERIES CUT OUT |
| GFCI | GROUND FAULT CIRCUIT INTERRUPTER | SE | SERVICE ENTRANCE |
| G, GND | GROUND | S.O. | HARD SERVICE OIL-RESISTANT |
| GRS | GALVANIZED RIGID STEEL | SPD | SURGE PROTECTIVE DEVICE |
| HIRL | HIGH INTENSITY RUNWAY LIGHTING | SPEC | SPECIFICATION |
| HH | HANDHOLE | SQ | SQUARE |
| HP | HORSEPOWER | SS | STAINLESS STEEL |
| HPS | HIGH PRESSURE SODIUM | STA | STATION |
| HTR | HEATER | SW | SWITCH |
| ILS | INSTRUMENT LANDING SYSTEM | TSA | TAXIWAY SAFETY AREA |
| JB | JUNCTION BOX | TWY, T/W | TAXIWAY |
| KCMIL | THOUSAND CIRCULAR MILS | TYP | TYPICAL |
| KIP | KILO INCH POUND | XFMR | TRANSFORMER |
| KV | KILOVOLT | UG | UNDERGROUND |
| KVA | KILOVOLT-AMPERE | UNO | UNLESS NOTED OTHERWISE |
| KW | KILOWATT | U.S.E. | UNDERGROUND SERVICE ENTRANCE CABLE |
| KWH | KILOWATT-HOUR | UV | UTILITY VAULT, CO. |
| LO/TO | LOCK-OUT/TAG-OUT | V | VOLT |
| MALSR | MEDIUM INTENSITY APPROACH LIGHT SYSTEM WITH RUNWAY ALIGNMENT INDICATOR LIGHTS | VA | VOLT-AMPERE |
| | | VASI | VISUAL APPROACH SLOPE INDICATOR |
| | | WP | WATERPROOF |

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TAXILANE CHARLIE EXTENSION CITY OF VANCOUVER

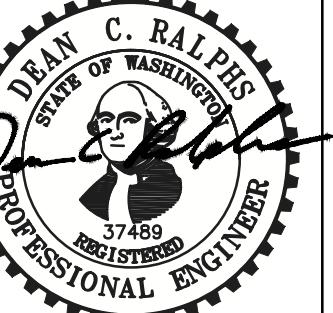
AIP 3-53-0089-052-2002

FILE : E001
JUB PROJ. # : ---
DRAWN BY: AAV
DESIGN BY: DCR
CHECKED BY: DCR

ONE INCH

AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

MEET NUMBER:
E001



02/09/2023

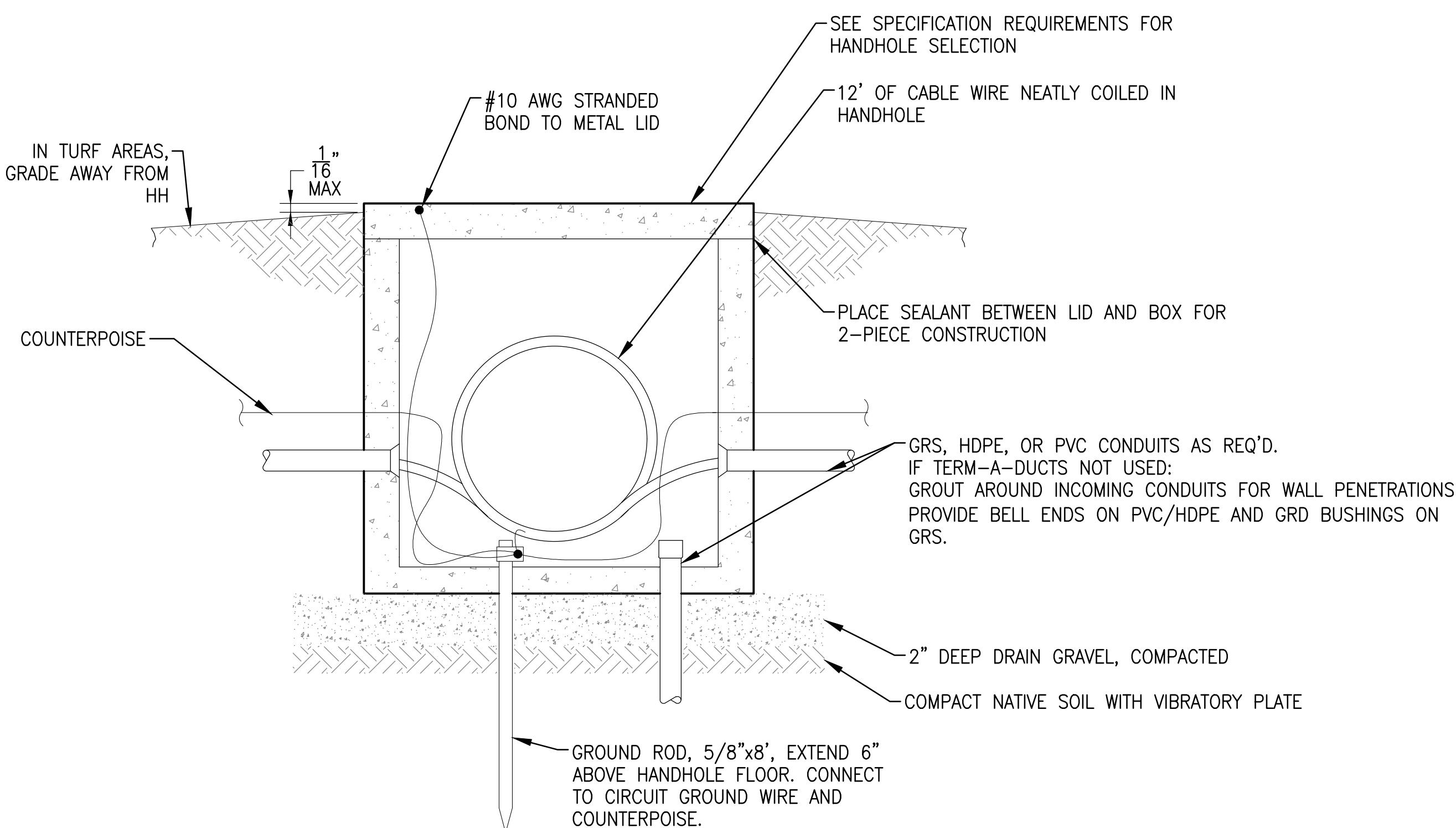
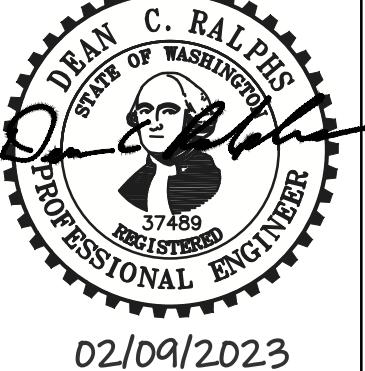
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REVISION

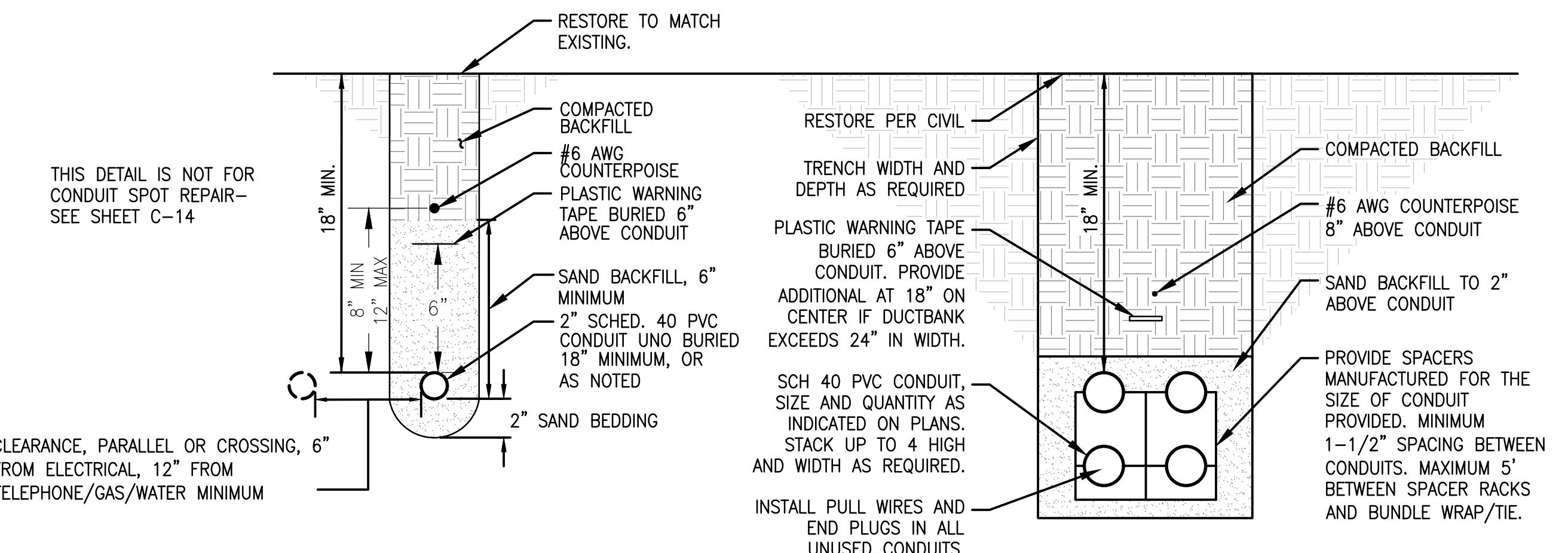
BY APR. DATE

NO. DESCRIPTION

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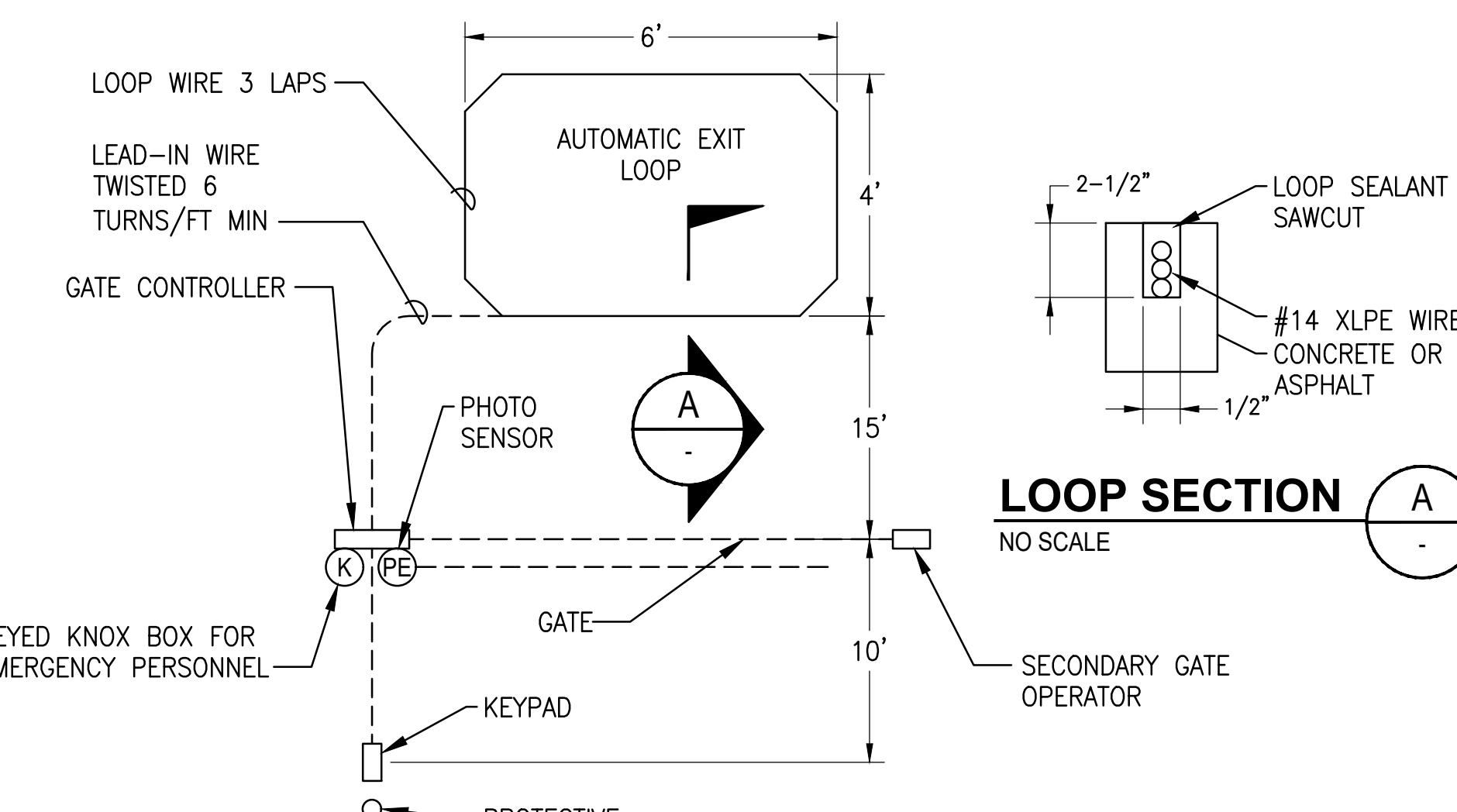


1 **TYPICAL HANHOLE DETAIL**
- SCALE:NTS

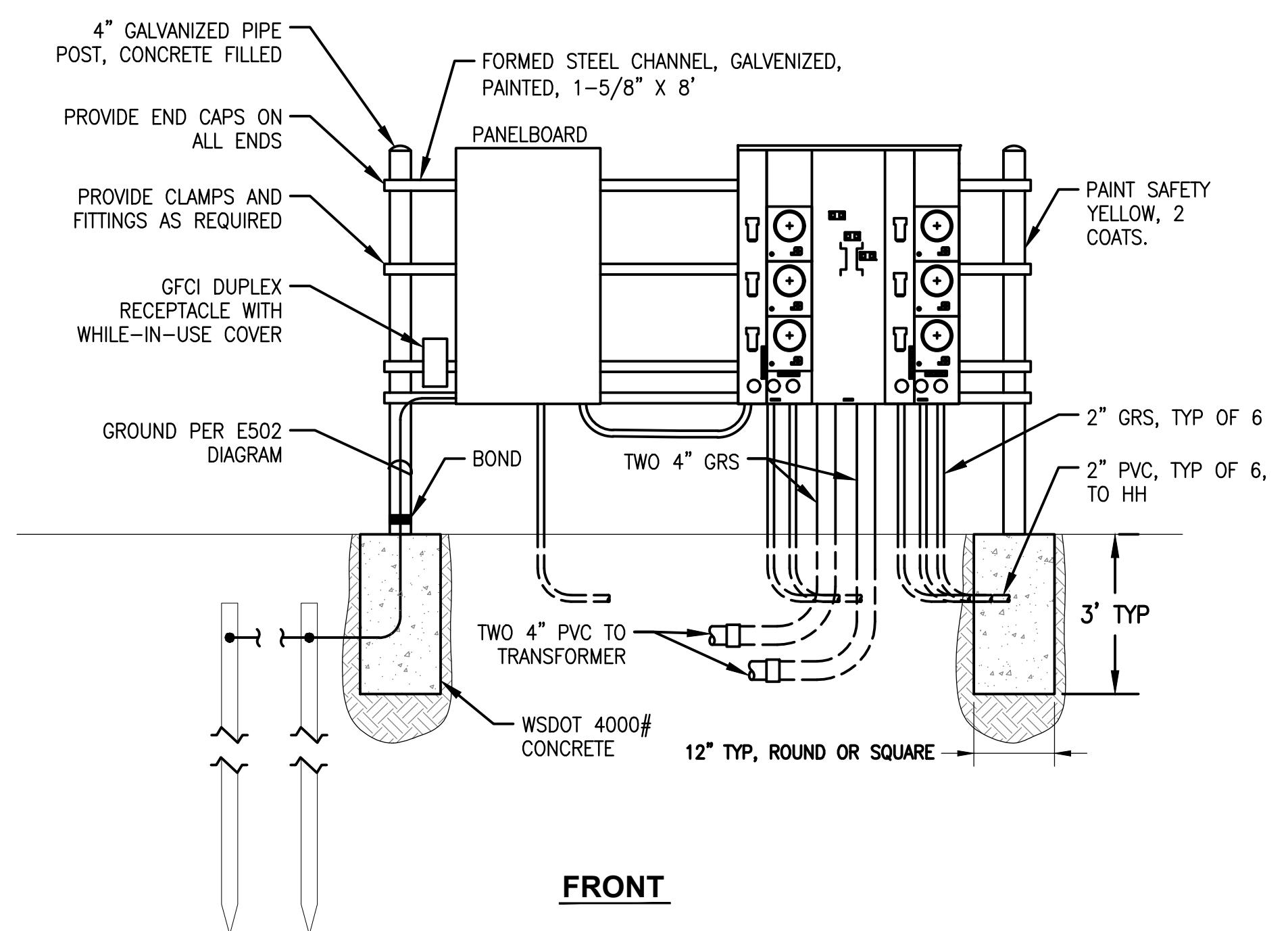


2 **TYPICAL SINGLE
BURIED CONDUIT DETAIL**
- SCALE:NTS

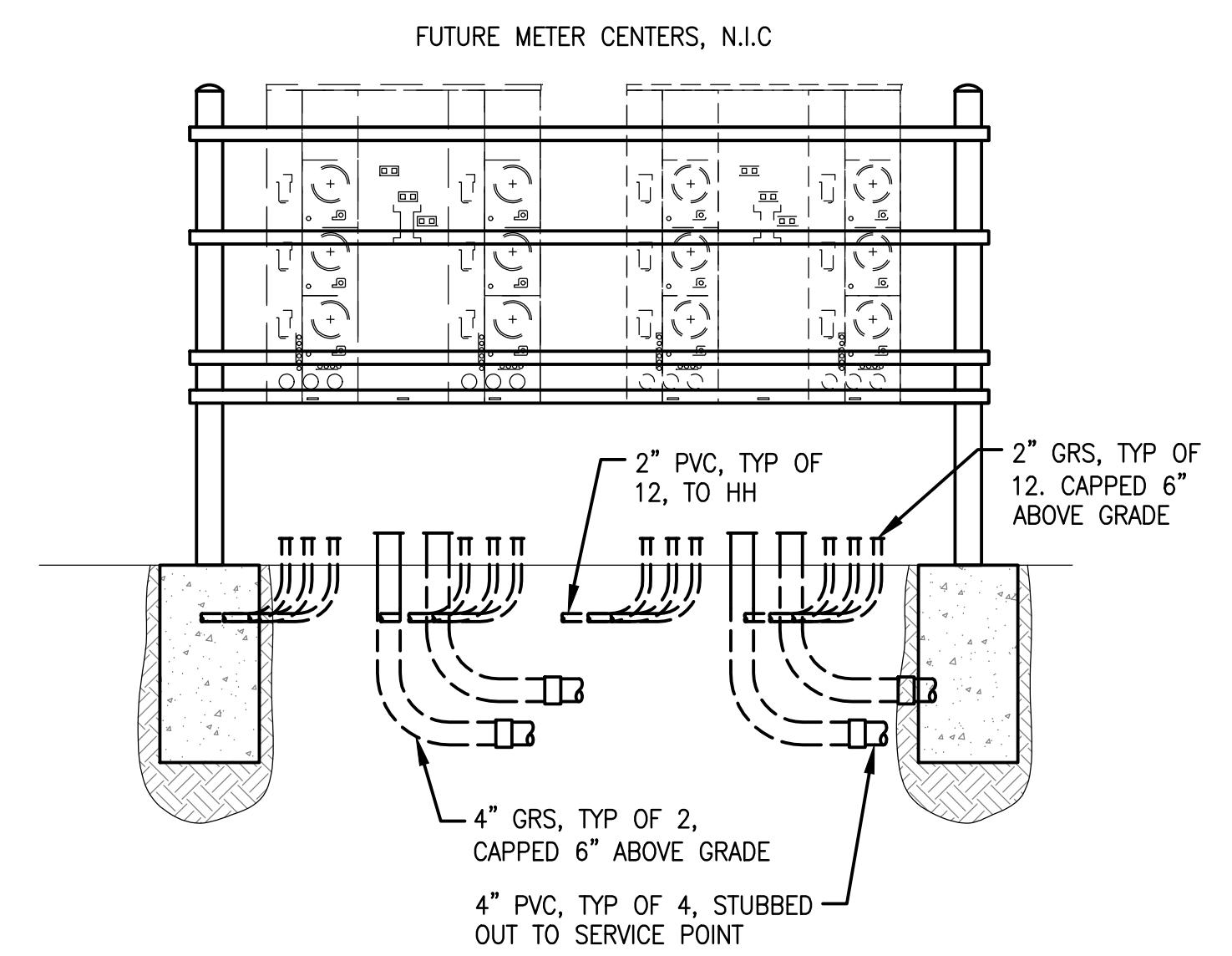
3 **TYPICAL DIRECT
BURY DUCT BANK DETAIL**
- SCALE:NTS



4 **TYPICAL DETECTION LOOP DETAIL**
- SCALE:NTS



5 **ELECTRICAL H-FRAME DETAIL**
- SCALE:NTS

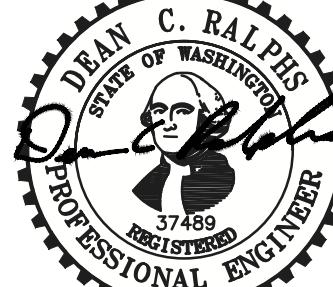


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SOLELY AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO J-U-B
REVISION

TAXILANE CHARLIE EXTENSION
CITY OF YAKIMA
AIP 3-53-0089-052-2002
GENERAL ELECTRICAL DETAILS

FILE : E501
JUB PROJ. #: ---
DRAWN BY : AAV
DESIGN BY : DCR
CHECKED BY : DCR
CONE INCH
AT FULL SIZE, IF NOT ONE
INCH, SCALE ACCORDINGLY
LAST UPDATED: 1/25/2023

SHEET NUMBER:
E5.01



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REVISION

NO. _____

DESCRIPTION _____

BY APPL. DATE _____

NO. _____

BY APPL. DATE _____

NO. _____

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