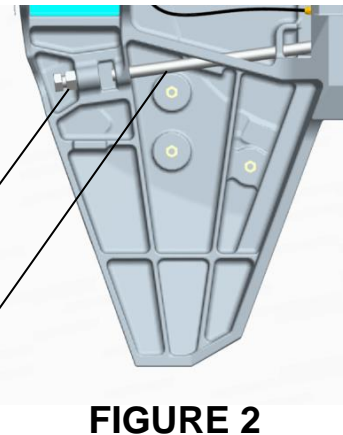
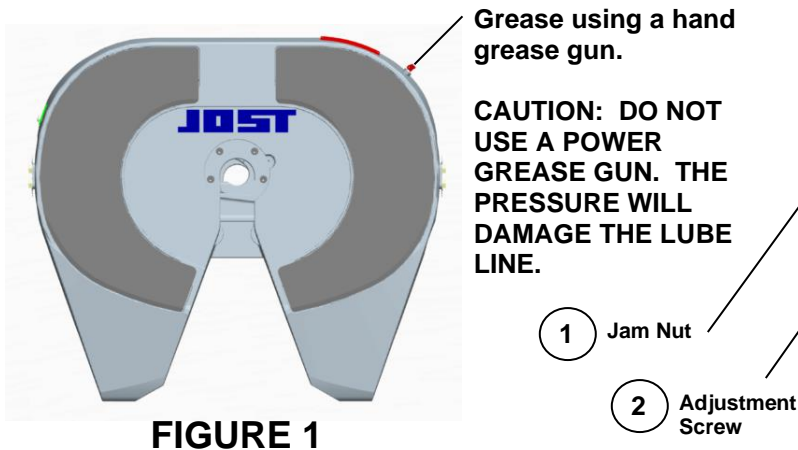




1. **FAILURE TO FOLLOW THESE INSTRUCTIONS COULD CAUSE A HAZARDOUS CONDITION!**
2. **PERFORM ALL MAINTENANCE WITH TRACTOR UNCOUPLED FROM TRAILER.**
3. **ALL PROCEDURES MUST BE PERFORMED BY QUALIFIED PERSONNEL.**

A. LUBRICATION (as required):

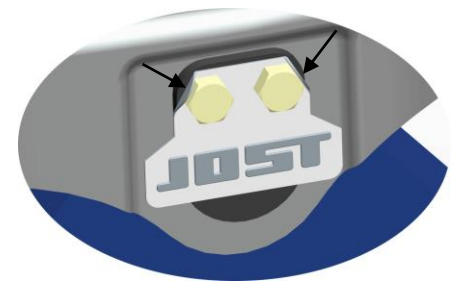
1. Lubricate the kingpin lock using a hand pump grease gun through the grease zerk provided on the skirt of the fifth wheel (see figure 1).
2. Apply a lithium-based grease with EP additive (extreme pressure) to the trailer contact surface of the fifth wheel.
3. Apply a light oil to all moving parts.



B. INSPECTIONS AND ADJUSTMENTS:

(Perform the following every 3 months or 30,000 miles)

1. Inspect the mounting – repair or replace any missing or damaged fasteners or broken components.
2. Inspect bracket pin bolts, make sure locking tabs are properly securing the bolts in place (Figure 3).
3. Inspect the fifth wheel for bent, worn or broken parts. Replace with genuine Jost parts (see page 4 for parts list).
4. Inspect the lube plates and replace if damaged or when the plates wear down to the topplate ridge surrounding the lube pockets.
5. Inspect the throat guides and replace once the head of the kingpin comes in contact with the topplate casting throat wall (see figure 8).



6. Check Adjustment - Lock and unlock several times using a lock tester. Check that locking mechanism functions properly (see LT-SK37Y-05).

- A. **The lock is properly adjusted if:** The wheel locks and unlocks easily each time and the lock tester rotates freely.
- B. **The lock is too loose if:** You feel fore-aft play when pulling and pushing on the lock tester handle (see figure 6)
- C. **The lock is too tight if:** The lock tester does not rotate freely when attempting to rotate it in the lock. (See figure 6)



FIGURE 3

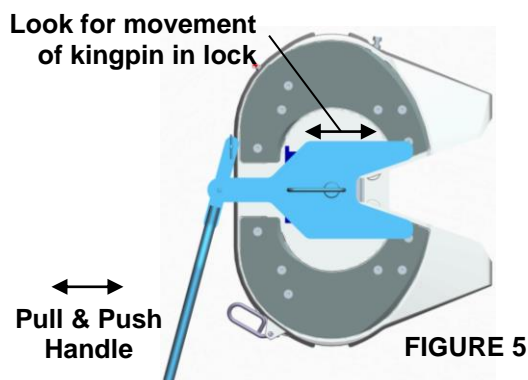


FIGURE 5

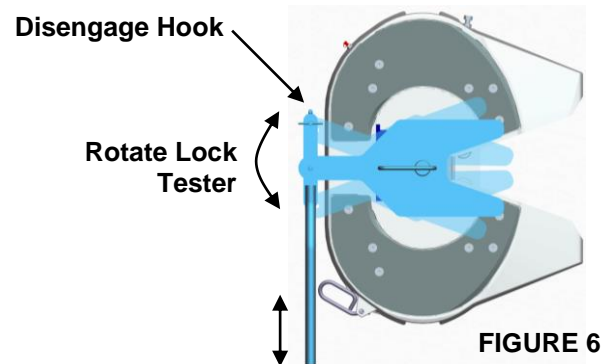
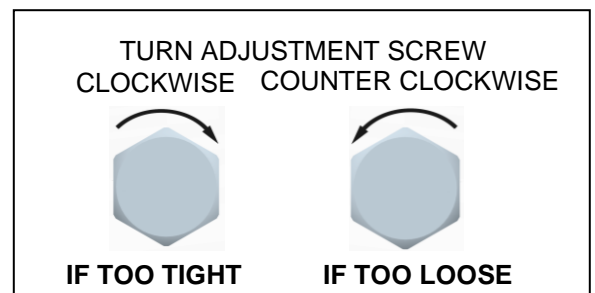


FIGURE 6

7. Correct improper adjustment

- A. **If it is too tight:** loosen the jam nut (see figure 2 item 1), turn the adjustment screw (see figure 2 item 2) clockwise one full turn and reset the jam nut. Then, lock and unlock several times, repeat if necessary.
- B. **If it is too loose:** loosen the jam nut, turn the adjustment screw counterclockwise one full turn and reset the jam nut. Then, lock and unlock several times, repeat if necessary.

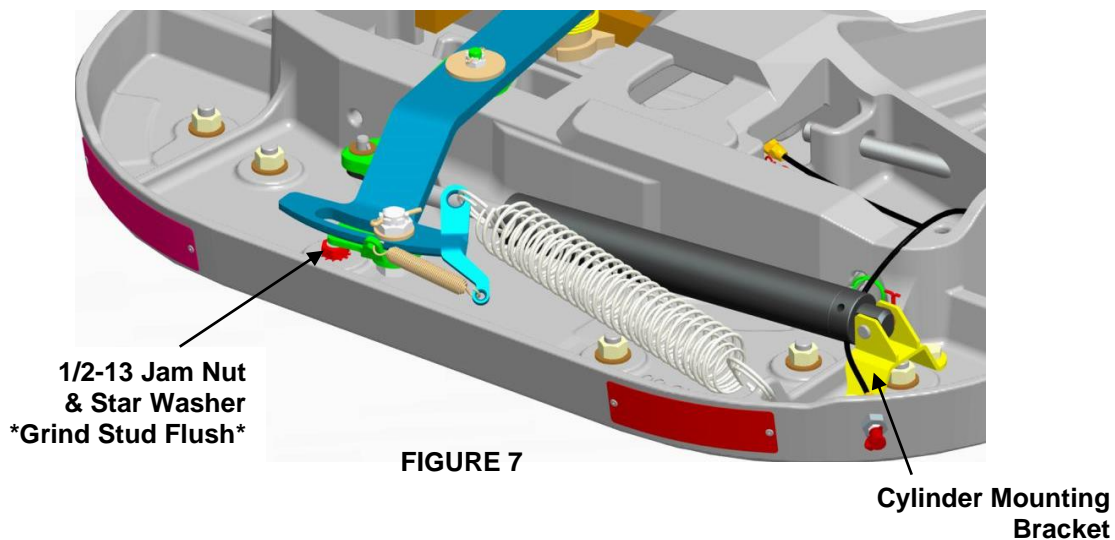


WARNING

DO NOT USE ANY FIFTH WHEEL THAT FAILS TO OPERATE PROPERLY!

C. LUBE PLATE REPLACEMENT PROCEDURES (using kit SK79004-05):

1. Remove the lock nuts and washers on the underside of the topplate with a 3/4" wrench
2. Remove the plates and clean any debris from the steel lube plate pockets of the topplate.
3. Install the new plates.
4. Install the 1/2" washers and 1/2"-13 locknuts provided in the kit. Tighten and torque the nuts to 45 FT-LBS.
5. Take notice of the (1) 1/2-13 jam nut and star washer (see Figure 7). This location will require the stud to be ground flush for release mechanism clearance



D. THROAT GUIDE REPLACEMENT PROCEDURES:

1. Remove the (5) allen head bolts per throat guide using a 5/16" allen wrench.
2. Remove the throat guides and clean any debris from this area of the topplate casting.
3. Install new throat guides.
4. Install (5) new allen head bolts per throat guide. Tighten and torque to 45 FT-LBS.

