



**भारतीय राष्ट्रीय राजमार्ग प्राधिकरण  
(सड़क परिवहन और राजमार्ग मंत्रालय)  
National Highways Authority of India  
(Ministry of Road Transport & Highways)**

उप महाप्रबंधक (तकनिकी) सह परियोजना निदेशक के कार्यालय

परियोजना कार्यालय इकाई

Office of the Deputy General Manager (Tech) cum Project Director

Project Implementation Unit

प्लॉट नंबर : 3, सूर्या टवर्स, दूसरी मंज़िल, पूर्व पहले स्ट्रीय, के.के. नगर, मदुरै - 625020, तमில்நாடு.

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[mad@nhai.org](mailto:mad@nhai.org)

Ref: NHAI/PIU/MDU/NH-44/North River Bank road/2024/ 1543

Date: 03.08.2024

To

The Regional Officer,  
National Highways Authority of India,  
Regional Office,  
Madurai.

Sir,

Sub: NHAI-PIU, Madurai-Tolling, Operation and Transfer of Madurai to Kanyakumari Section from Km.0+000 to Km.116+500 of NH-44 in the State of TamilNadu - **Proposal for the Connectivity of North River Bank Road with NH-44 at km.2+800 near Samayanallur - Recommendation submitted for NOC - Reg**

- Ref:
1. IE Lr.No. APL/NHAI/Madurai - Kayathar/NH-44/2024-25/090 dt:25.07.2024.
  2. RSA mail dt:24.07.2024
  3. Concessionaire Lr.No:MKTL/PS/NHAI/O&M/2024/1248 dt:27.07.2024
  4. DE, Highways, C&M, Madurai Lr.78/2024/JDO2 dt:12.07.2024.
  5. This office Lr.No.NHAI/PIU/MDU/NOC/NH 44 & TN-05/2024/1380 dt:06.07.2024
  6. This office Lr.No. NHAI/PIU/MDU/NOC/NH-44/2024/1377 dated.04/07/2024.
  7. DE, Highways, C&M, Madurai Lr.78/2024/JDO2 dt:25.06.2024

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This has the reference to the above subject and letter cited at ref.4, wherein DE, Highways (C&M), Madurai has requested NOC for the proposed Connectivity of North River Bank Road with NH-44 at km.2+800 near Samayanallur.

2. In this regard, the following points are submitted for your kind consideration,

a) Earlier, DE, Highways (C&M), Madurai (Ref.7) has submitted the NOC for the subject road connecting works. The site has been inspected by the Undersigned and observed the following which are in turn communicated to the DE, Highways, C&M, Madurai vide letter under ref.6.

i. Traffic towards Virudhunagar on NH-44 towards State Highway:

- The proposed road width is 7m with 1.5m of Earthen Shoulder. Due to the high embankment location, the proposed free slopes may not fit within the existing NH right-of-way (ROW).
- The ROW has not been delineated in the drawing.
- Merging/diverging points are not indicated in the drawing.
- The location being on MJB Approaches, quadrant pitching/protection works may be at risk of damage.

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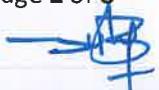
ii. Traffic from State Highway Joining NH-44 (towards Dindigul):

- The Finished Road Level (FRL) is proposed beneath the High Flood Level (HFL).
- Existing levels of the MJB structure, HFL, and proposed road top level must be clearly depicted in the drawing.
- Given the road is proposed beneath the HFL, any necessary NOC from Irrigation/WRD should be provided.
- The proposed vertical clearance of 3.55 meters from the Girder Soffit level may not be adequate for traffic movement of large freight vehicles on NH-44.
- Similar issues were observed with the proposed road width of 7m with 1.5m of Earthen Shoulder due to its high embankment location.
- The proposed free slopes may not fit within the existing NH ROW.
- ROW details and merging/diverging points not indicated in the drawing.
- As the location is on MJB Approaches, there is a risk to quadrant pitching/protection works.

iii. The overall layout plan of the proposed road starting from Kamaraj Bridge connecting NH-44 needs to be submitted.

b) In compliance, DE, Highways (C&M), Madurai (Ref.4) has re submitted the instant Proposal with point wise reply as follows:-

<b>Observation</b>	<b>Reply by DE, Highways (C&amp;M), Madurai</b>
<u>Traffic towards Virudhunagar on NH 44 towards State Highways</u>	
The proposed road width is 7m with 1.5m of Earthen Shoulder. Due to the high embankment location, the proposed free slopes may not fit within the existing NH right-of-way (ROW).	The proposed road will be laid near to the NHAI road. There is enough ROW available to provide free slope as shown in drawing attached herewith
The ROW has not been delineated in the drawing	The ROW has been fixed by NHAI Concessionaire surveyor at field on 05.07.2024. After taking coordinates new drawing has been prepared in which ROW has been delineated.
Merging/diverging points are not indicated in the drawing	The Chainages of NH-44 has been indicated to show where merging and diverging with NH takes place
The location being on MJB Approaches, quadrant pitching/protection works may be at risk of damage	The embankment for road will be formed on the slope pitching, hence any damage to remaining pitching will be rectified
<u>Traffic from State Highway Joining NH-44 (towards Dindigul)</u>	
The Finished Road Level (FRL) is proposed beneath the High Flood Level (HFL)	The FRL of road beneath Deck slab of Bridge is below HFL as well as Bed level to provide clear 4.50m height from soffit of beam
Existing levels of the MJB structure, HFL, and proposed road top level must be clearly depicted in the drawing	A cross-section drawing has been prepared showing all necessary details and attached herewith
Given the road is proposed beneath the HFL, any	NOC requested from WRD



necessary NOC from Irrigation/WRD should be provided	
The proposed vertical clearance of 3.55 meters from the Girder Soffit level may not be adequate for traffic movement of large freight vehicles on NH-44	The proposed vertical clearance is 4.5m which is enough for all heavy vehicles except container trucks. The height gauge will be erected before entering beneath Bridge
Similar issues were observed with the proposed road width of 7m with 1.5m of Earthen Shoulder due to its high embankment location	ROW delineated in drawing. The retaining wall is proposed where RoW is not sufficient to provide free slope (i.e., in chainage 7/800 - 7/900)
The proposed free slopes may not fit within the existing NH ROW	ROW delineated in drawing. The retaining wall is proposed where RoW is not sufficient to provide free slope (i.e., in chainage 7/800 - 7/900)
ROW details and merging/diverging points not indicated in the drawing	ROW details are provided in the drawing. The Chainages of NH-44 has been indicated to show where merging and diverging with NH takes place
As the location is on MJB Approaches, there is a risk to quadrant pitching/protection works	Any damages to Quadrant/protection works will be rectified.

c) The above proposal has been forwarded to IE as well as Concessionaire for furnishing comments vide ref.5

d) On verification, IE (Ref.1) has recommended the Instant NOC proposal with the following conditions as follows,

i. Retaining Wall beneath bridge portion:

The retaining wall measurements/details are not included in the typical cross-section at CH: 7550 - 7650 (LHS). Additionally, the retaining wall foundation on the RHS may disturb the existing bridge abutment foundation during construction. Therefore, it is requested to omit the retaining wall construction in the abutment wall portions, as the abutment wall serves as the retaining wall in this section.

ii. Retaining Wall Dimensions:

The retaining wall measurements/details including levels are not mentioned in the submitted typical cross-sections. These details need to include in the submitted drawing according to site conditions.

iii. Cross-Section Overlap:

In view of the typical cross sections, there is a 75m overlap between Type C2 (RHS) at CH: 7375-7650 and Type E (RHS) at CH: 7575-8000. This needs to be corrected to reflect the actual site conditions.

iv. Tapering Portions:

The typical cross-sections of tapering portions, such as diverging and converging on BHS, are not included in the submitted drawings. The ramp terminals shall be provided as per clause



6.5 and Fig. 6.3 Typical Designs for Entry and Exit Terminals as per IRC: 92-2017. These need to be included as per requirements.

v. Weep Holes:

Weep holes are not shown in the retaining wall cross-sections. These need to be included as per requirements.

vi. Provision of MBCB:

The difference in level between the existing road level (or) existing ground level to the proposed finished road level (FRL) and the average 3m is available from CH: 7925 to CH: 8000 on LHS and from CH: 7575 to CH: 7725 on RHS. These chainages comes under the NHAI ROW and along with the NHAI Road. Hence, MBCB has to be provided as per the NHAI standards and specifications. These need to be included as per site requirements.

e) Further, IE has conveyed certain additional conditions duly in consultation with RSA M/s.GMD Consultants, Mumbai (vide mail dt:24.07.2024) as follows,

i. Parapet/ Retaining Wall Height:

For road sections running along the river bed (Cross Section Type C1, D1), the parapet/retaining wall/railing should be at least 1.1m above the adjacent finished road level. For riverbed sections, this should be a solid parapet/retaining wall to ensure safety during floods. In other cases, a railing can be placed on top of the parapet/kerb, shown as 500mm in the section.

ii. Diverging Point Separator:

At the diverging point on the RHS where the road diverges from NH, a separator should be created in the gore area with a traffic impact attenuator at its face. The tapering should be a minimum of 1 in 20.

iii. Merging Point Traffic Calming:

At the merging point on the LHS where the side road merges with the MCW, proper traffic calming measures, such as rumble strips or speed breakers, should be provided to ensure traffic merges at a controlled speed.

iv. Drainage Arrangements:

Drainage arrangements are not shown in the cross-sections. Particularly where there is a footpath or retaining wall on the outer side of the road, it is unclear how water will be drained from the road surface.

v. Flooding Precautions:

Since the road runs under a bridge at this location, standing water may occur during high floods. Boom barriers with road closure signboards should be placed sufficiently in advance to be lowered in case of an emergency.

vi. Road Markings and Reflective Delineators:

Proper road markings and reflective delineators should be provided on road edges, parapets, or crash barriers for visibility during dark conditions.

vii. Lighting:

Lighting should be provided along the slip road, including under the abutment part.

viii. Height Barrier:

Height barriers should be placed on both sides of the road where it goes under the abutment.

ix. Signage:

Proper signage at merging/diverging points and abutment sections must be placed to warn and inform traffic.

x. Riverbank Side Signage:

Quay side or riverbank side signs should be provided on the LHS to caution drivers of the impending danger due to the presence of the water body as per IRC: 67-2012.

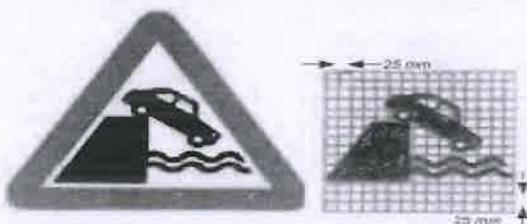


Fig. 15.59 Quay Side or River Bank

xi. Information and Warning Signs:

Information and warning signs should be placed on NH-44 before diverging and merging points to warn and inform traffic on the MCW about diverging and merging traffic at the location.

f) In addition to that, Concessionaire (Ref.3) has informed that there may be chances of Traffic conflicts due to the vehicles coming from proposed connecting road of Highways department merging at km.2+200 on RHS of NH-44 and may take 'U' Turn at existing median opening at km.2+100 and due to emerging Madurai City Traffic, there may be chances of Hot spot/Black spot development at km.2+100 (Median opening). Hence, Concessionaire suggested that the proposed Connecting road may be extended up to adjacent service road at km.1+600 on RHS to redirect vehicles to a nearby U-turn point at km 0+900.

g) Now, recommendation of IE/RSA/Concessionaire has been duly verified with the site conditions and recommended as follows,

#### PD Recommendation:

##### 1. Brief Description of subject Project:

<u>S.No</u>	<u>Description</u>	<u>Details</u>
1	Project stretch	Madurai - Kayathar section of NH-44 from km.0+000 to km.116+500
2	Toll Plaza/Tollable length	1. Kappalur / 52.3Kms (From km.0+000 to km. 52+300)
3	Concessionaire	Madurai - Kanyakumari tollways Ltd (M/s.CUBE Mobility & investment Pte.Ltd)
4	Appointment date	20.10.2020
5	Concession period	30 years (Up to 19.10.2050)
6	Independent Engineer	M/s. Artefact Projects Limited in association with Renaissance IOT LLP, Madurai
7	Appointed date	18.10.2023
8	RSA	M/s.GMD Consultants, Mumbai
8	ROW (m)	30m + 30m

##### 2. Salient Features of Instant Proposal by DE, Highways (C&M), Madurai:

###### LHS Road Portion under NHAI ROW (Km.7+550 to km.8+100):

###### a) From Km.7+550 to km.7+770 (TCS D1):

- This part of road crossing the NH 44 by passing underneath the Existing major Bridge i.e., between A1 Abutment & Pier 1.
- Proposed Carriageway width is 7m and 0.5m of shyness on BHS & 1.5m width of Footpath totally 9.5m.
- The Span length of Existing major Bridge is 14m.
- Proposed vertical clearance is 4.5m.

###### b) From Km.7+550 to km.7+800 (TCS D2):



- This part of road running along the Main carriageway of NH 44.
- Proposed Carriageway width is 7m, 0.5m of shyness & 1.5m Earthen shoulder with Free slope of embankment.

c) From Km.7+800 to km.7+900:

- This part of road running along the Main carriageway of NH 44.
- Proposed Carriageway width is 7m, 0.5m of shyness & 1.5m Earthen shoulder with retaining wall.

d) From Km.7+900 to km.8+100:

- This part of road running along the Main carriageway of NH 44.
- Proposed Carriageway width is 7m, 0.5m of shyness & 1.5m Earthen shoulder with Free slope of embankment.

**RHS Road Portion under NHAI ROW (Km.7+575 to km.8+000):**

- This part of road running along the Main carriageway of NH 44.
- Proposed Carriageway width is 7m, 0.5m of shyness & 1.5m earthen shoulder with free slope of embankment.

In light of above, it is recommended that the Conditional based NOC/In Principal may be solicited as per NHAI Policy circular issued on “Grant of Permission for Construction of Flyover/ Underpass/ Foot over bridge/ Canal crossing proposed by State Govt. Agencies across National Highways dated 03.11.2020”, towards the Connectivity of North River Bank Road with NH-44 at km.2+800 near Samayanallur without any financial implication to NHAI subject to incorporation of above said condition vide Para (d) & (e) & (f) and also to the following terms & conditions or communicated by the Concessionaire & IE,

- I. The entire cost of Construction shall be borne by DE, Highways (C&M), Madurai.
- II. The work shall be executed under the supervision of NHAI or its authorized personnel only. The charges towards the same shall be borne by DE, Highways (C&M), Madurai.
- III. GAD/Design including traffic diversion plan shall be got approved from NHAI or its authorized representative. There shall not be any changes/ modifications in the GAD concurred by NHAI any change/ revision in Drawings shall be got approved by NHAI prior to execution.
- IV. Adequate safety measures for the safety of the Road Users shall be ensured within the Construction Zone.
- V. There shall not be any leakage of traffic or revenue loss to the Concessionaire. In case of any such loss, it shall be fully borne by the DE, Highways (C&M), Madurai.
- VI. Any damages to the Project Highway and appurtenances shall be rectified by DE, Highways (C&M), Madurai as per Standards/ specifications at their own cost.
- VII. The approval shall be without prejudice to the rights of the NHAI.
- VIII. DE, Highways (C&M), Madurai shall bear any contractual issues/ claims raised by the SPV on account of Construction of the connecting road requested.
- IX. No Construction except the permitted expressly be allowed. Any changes/ modification shall be done with approval of NHAI.



- X. DE, Highways (C&M), Madurai shall ensure that construction of connecting road does not in any way impeach safety and smoothest of traffic plying in NH and shall solely responsible for safe construction procedure, safety of road users.
- XI. Proper drainage shall be provided for proper draining of the surface water from slip road from getting in to NHAI road at the cost of DE, Highways (C&M), Madurai and shall be responsible for maintenance of same there on.
- XII. Proper traffic zone management & Safety arrangements shall be in place at the time of construction.
- XIII. Adequate mandatory & cautionary and information boards shall be placed to guide the traffic plying in the main carriageway about the intersection ahead. Any additional requirement and maintenance shall be borne by DE, Highways (C&M), Madurai.
- XIV. The maintenance of the entire Connecting road part adjoining main carriageway shall be done by DE, Highways (C&M), Madurai.
- XV. Rental charges if any fixed by SPV shall be borne by DE, Highways (C&M), Madurai.
- XVI. Utilities such as Electric poles, High mast light poles, Gantry signages, transformers, storm water drains, water pipelines, gas pipeline, sewer lines and any other utilities fouling in above said crossing alignment, will be shifted by the DE, Highways (C&M), Madurai at its own efforts and cost in consultation with utility departments.
- XVII. During construction any damage to carriageway, Footpath, Avenue plants etc., if any, is occurred, it will be rectified by NHAI/ concessionaire at the risk and cost of the DE, Highways (C&M), Madurai.
- XVIII. Construction debris shall not be dumped within NHAI Row and shall be removed at the earliest, failing which the same shall be done by NHAI/ concessionaire at the risk and cost of the DE, Highways (C&M), Madurai.
- XIX. The above work shall be completed within minimum possible time.

Encl: as above

Yours faithfully,



(Keerthi Bharadwaj)  
Project Director  
NHAI-PIU, Madurai

Copy to:

1. DE, Highways (C&M), Madurai - for kind information, please.
2. IE, M/s. Artefact Projects Limited in association with Renaissance IOT LLP, Madurai- for information, please.
3. Concessionaire, Project Head, M/s.MKTL, Madurai - for information, please.
4. RSA, M/s.GMD Consultant, Mumbai - for information, please.

**Registered & Corporate Office:** Block No. 106,  
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In  
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**Website:** www.renaissanceiot.com  
**LLPIN:** AAI-8543

Ref: APL/NHAI/Madurai – Kayathar/NH-44/2024-25/090

Date: 25/07/2024

To

**The Project Director**

**PIU-Madurai**

**National Highways Authority of India**

Plot No.3, Surya Tower,  
 2<sup>nd</sup> Floor, 1<sup>st</sup> East Street, K.K.Nagar,  
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<b>NHAI PIU MADURAI</b>	
<b>IN: No</b>	<b>2706</b>
<b>DATE :</b>	<b>25/7/24</b>
<b>PD</b>	
<b>DM(T)</b>	✓
<b>LO</b>	
<b>SE</b>	✓



**Sub:** Independent Engineer Services for Operation & Maintenance of (I) Madurai – Virudhunagar section from Km.0+000 to Km.52+300 of NH-44(Old No -7) on TOT mode in the state of Tamil Nadu and (II) Virudhunagar – Kayathar Section from Km.52+300 to Km.116+500 of NH-44(Old No -7) on TOT mode in the state of Tamil Nadu – Request for No Objection Certificate (NOC) for Connecting Proposed Vaigai North Bank road with National Highways (NH-44) Near Samayanallur – IE Comments - Reg.

- Ref:**
1. PIU, Madurai, Mail dated.23.07.2024.
  2. Divisional Engineer, Construction & Maintenance (H), Madurai Lr.NO. 78/2024/JD02  
-dated. 12.07.2024
  3. NHAI/PIU/MDU/NOC/NH-44/2024/1380 dated.06/07/2024.
  4. NHAI/PIU/MDU/NOC/NH-44/2024/1377 dated.04/07/2024.
  5. Divisional Engineer, Construction & Maintenance (H), Madurai Lr.NO. 78/2024/JD02  
-dated. 25.06.2024

With reference to the above subject, the Divisional Engineer, Construction & Maintenance (H), Madurai, has requested a No Objection Certificate (NOC) for connecting the proposed Vaigai North Bank Road with National Highway (NH-44) near Samayanallur, vide reference no. 05.

A joint inspection was conducted by the Project Director, PIU-Madurai, and the Divisional Engineer, Construction & Maintenance (H), on 04.07.2024. Based on the joint inspection, the Project Director, PIU-Madurai, conveyed observations regarding the proposed NOC to the Divisional Engineer, Construction & Maintenance (H), Madurai, vide reference no. 04.

The Divisional Engineer, Construction & Maintenance (H), Madurai, has incorporated these observations and resubmitted the proposal for the NOC for connecting the proposed Vaigai North Bank Road with NH-44 near Samayanallur.

Subsequently, the Project Director, PIU-Madurai, has requested our office to review the drawings and provide comments on the subject.



Based on our review and scrutiny of the proposal, considering the site requirements, our observations are as follows:

**1. Retaining Wall beneath bridge portion:**

The retaining wall measurements/details are not included in the typical cross-section at CH: 7550 – 7650 (LHS). Additionally, the retaining wall foundation on the RHS may disturb the existing bridge abutment foundation during construction. Therefore, it is requested to omit the retaining wall construction in the abutment wall portions, as the abutment wall serves as the retaining wall in this section.

**2. Retaining Wall Dimensions:**

The retaining wall measurements/details including levels are not mentioned in the submitted typical cross-sections. These details need to include in the submitted drawing according to site conditions.

**3. Cross-Section Overlap:**

In view of the typical cross sections, there is a 75m overlap between Type C2 (RHS) at CH: 7375–7650 and Type E (RHS) at CH: 7575–8000. This needs to be corrected to reflect the actual site conditions.

**4. Tapering Portions:**

The typical cross-sections of tapering portions, such as diverging and converging on BHS, are not included in the submitted drawings. The ramp terminals shall be provided as per clause 6.5 and Fig. 6.3 Typical Designs for Entry and Exit Terminals as per IRC: 92-2017. These need to be included as per requirements.

**5. Weep Holes:**

Weep holes are not shown in the retaining wall cross-sections. These need to be included as per requirements.

**6. Provision of MBCB:**

In view of the plan and profile, the difference in level between the existing road level (or) existing ground level to the proposed finished road level (FRL) and the average 3m is available from CH: 7925 to CH: 8000 on LHS and from CH: 7575 to CH: 7725 on RHS. These chainages comes under the NHAI ROW and along with the NHAI Road. Hence, MBCB has to be provided as per the NHAI standards and specifications. These need to be included as per site requirements.

Additionally, we have consulted with RSA GMD consultants, Navi Mumbai, regarding the required safety measures for the NOC for connecting the proposed Vaigai North Bank Road with NH-44 near Samayanallur. The points discussed between the Independent Engineer and RSA GMD consultants are as follows:

**1. Parapet/Retaining Wall Height:**

For road sections running along the river bed (Cross Section Type C1, D1), the parapet/retaining wall/railing should be at least 1.1m above the adjacent finished road level. For riverbed sections, this should be a solid parapet/retaining wall to ensure safety during floods. In other cases, a railing can be placed on top of the parapet/kerb, shown as 500mm in the section.

**2. Diverging Point Separator:**

At the diverging point on the RHS where the road diverges from NH, a separator should be created in the gore area with a traffic impact attenuator at its face. The tapering should be a minimum of 1 in 20.

**3. Merging Point Traffic Calming:**

At the merging point on the LHS where the side road merges with the MCW, proper traffic calming measures, such as rumble strips or speed breakers, should be provided to ensure traffic merges at a controlled speed.

**4. Drainage Arrangements:**

Drainage arrangements are not shown in the cross-sections. Particularly where there is a footpath or retaining wall on the outer side of the road, it is unclear how water will be drained from the road surface.

**5. Flooding Precautions:**

Since the road runs under a bridge at this location, standing water may occur during high floods. Boom barriers with road closure signboards should be placed sufficiently in advance to be lowered in case of an emergency.

**6. Road Markings and Reflective Delineators:**

Proper road markings and reflective delineators should be provided on road edges, parapets, or crash barriers for visibility during dark conditions.

**7. Lighting:**

Lighting should be provided along the slip road, including under the abutment part.

**8. Height Barrier:**

Height barriers should be placed on both sides of the road where it goes under the abutment.

**9. Signage:**

Proper signage at merging/diverging points and abutment sections must be placed to warn and inform traffic.

**10. Riverbank Side Signage:**

Quay side or riverbank side signs should be provided on the LHS to caution drivers of the impending danger due to the presence of the water body as per IRC: 67-2012.



Fig 15.59 Quay Side or River Bank

**11. Information and Warning Signs:**

Information and warning signs should be placed on NH-44 before diverging and merging points to warn and inform traffic on the MCW about diverging and merging traffic at the location.

Further, the independent engineer is requesting the Project Director, PIU-Madurai, to request the Divisional Engineer, Construction & Maintenance (H), Madurai to ensure the following points during the construction are as follows,

- The entire cost of construction shall be borne by Divisional Engineer, Construction & Maintenance (H), Madurai.
- The work shall be executed under the supervision of NHAi representatives.
- Detailed design & drawings including the traffic diversion plan shall be approved by NHAi. There shall not be any changes/ modifications in the drawings concurred by NHAi any change/ revision to drawings shall be approved by NHAi prior to execution.

- Adequate safety measures for the safety of the road users shall be ensured within the Construction Zone.
- There shall not be any leakage of traffic or revenue loss to the Concessionaire. In case of any such loss, it shall be fully borne by the Divisional Engineer, Construction & Maintenance (H), Madurai.
- Any damages to the Project Highway and appurtenances shall be rectified by Divisional Engineer, Construction & Maintenance (H), Madurai as per Standards/ specifications at their own cost.
- The approval shall be without prejudice to the rights of the NHAI.
- The Divisional Engineer, Construction & Maintenance (H), Madurai, shall bear any contractual issues / claims raised by the SPV/Concessionaire on account of construction of the slip road requested.
- No construction except the permitted expressly is allowed. Any changes / modifications shall be made with the approval of NHAI.
- The Divisional Engineer, Construction & Maintenance (H), Madurai, shall ensure that construction of Slip roads does not in any way impeach safety and the smoothest traffic plying in NH and shall be solely responsible for safe construction procedures and the safety of road users.
- Proper drainage shall be provided for proper draining of the surface water from the slip road from getting in to NHAI road at the cost of the divisional Engineer, Construction & Maintenance (H), Madurai and shall be responsible for maintenance of the same there on.
- Proper traffic zone management & safety arrangements shall be in place at the time of construction.
- Adequate mandatory & cautionary and information boards shall be placed to guide the traffic plying on the main carriageway about the junction / slip road ahead. Any additional requirements and maintenance shall be borne by Divisional Engineer, Construction & Maintenance (H), Madurai.
- The maintenance of the Slip road & transition length (including boards & road markings) adjoining the main carriageway shall be done by the divisional Engineer, Construction & Maintenance (H), Madurai.
- Rental charges, if any fixed by SPV/Concessionaire, shall be borne by the divisional Engineer, Construction & Maintenance (H), Madurai.
- Utilities such as electrical poles, high mast light poles, Gantry signages, transformers, storm water drains, water pipelines, gas pipeline, sewer lines and any other utilities fouling in above said interchanges/slip road alignment, will be shifted by the divisional Engineer, Construction & Maintenance (H), Madurai, at its own efforts and cost in consultation with utility departments.

During construction any damage to carriageway, service road, drain, footpath, and flyover structure etc., if any occurs, it will be rectified by NHAI/ concessionaire at the risk and cost of the divisional Engineer, Construction & Maintenance (H), Madurai.

In light of the above, it is requested that the Project Director, PIU-Madurai, issue a conditional concurrence based on the incorporation of the aforementioned observations for the NOC for connecting the proposed Vaigai North Bank Road with NH-44 near Samayanallur.

This is for kind information and further action please.

Yours faithfully,

**For Artefact Projects Limited in association with Renaissance IOT LLP**

P. Vijay Ananth

*P. Vijay Ananth*  
25/07/2024

**Resident Engineer**

Copy to - The Authorized Signatory, M/s. Artefact Projects Ltd in association with Renaissance IOT LLP, Nagpur.



MKTPL/PS/NHAI/O&M/2024/1248

Dated: 27-07-2024

To,

The Project Director,  
National Highways Authority of India,  
Office of the Project Director,  
Plot No.3, Surya Towers, 2<sup>nd</sup> Floor,  
1<sup>st</sup> East Street, K.K.Nagar,  
Madurai – 625 020.

NHAI PIU MADURAI	
IN: No	2748
DATE	29/7/24
PD	
DM(T)	✓
LO	
SE	✓

St. sub (p/put up)  
~~Sub~~  
30/7/24

**Subject:** Tolling, Operation, Maintenance & Transfer of Madurai-Kanyakumari Package 01 From Km 0+000 to Km 52+300 (Old NH-7) in the State of Tamil Nadu on TOT Basis – Requesting NOC for connecting the proposed Vaigai North Bank road with National Highways (NH-44) near Samayanallur – Comments sought - Reg.

**Ref:** 1. NHAI/PIU/MDU/NOC/NH-44 & (TN-05)/2024/1380. Dated: 06.07.2024.

Dear sir,

With reference to the above subject and references cited, we herewith kindly submit our observations / Comments based on the review of the subject proposal and prevailing site conditions as below,

1. Since the road runs under a bridge at this location, standing water may occur during high floods. Boom barriers with road closure signboards should be placed sufficiently in advance to be lowered in case of an emergency.
2. Height barriers shall be placed on both sides of the road where it goes under the abutment.
3. Riverbank side signs should be provided on the LHS to caution drivers of the impending danger due to the presence of the water body as per IRC: 67-2012.
4. Lighting should be provided along the slip road, including under the abutment part.  
Information and warning signs should be placed on NH-44 before diverging and merging points To warn and inform traffic on the MCW about diverging and merging traffic at the location.
5. The entire cost of construction shall be borne by Divisional Engineer, Construction & Maintenance (H), Madurai.
6. Adequate safety measures for the safety of the road users shall be ensured within the Construction Zone.



*Lai*  
Madurai-Kanyakumari Tollway Private Limited

(formerly known as Madurai-Kanyakumari Tollway Limited)

Regd. Office: B-376, Upper Ground Floor, Nirman Vihar, New Delhi – 110092

SPV Office : No.11, Amaithi Solai Road, Sundar nagar, Opp. Vettrivinayagar Kovil, Tirunagar, Madurai Dist, Tamilnadu - 625 006.

Site Office Address: Kappaur Toll Plaza, Post - Kappalur, Taluk - Tirumangalam, District- Madurai, Tamilnadu-625008

Corporate Office: Unit No. 1901, 19<sup>th</sup> Floor, Tower B, World Trade Tower, Plot No. C-1, Sector-16 Noida, U.P. - 201301

Phone: +91-0120-486 8300 CIN: U45100DL2019FTC358752 Email: corporate.secretarial@cubehighways.com

Dated: 27-07-2024

7. There shall not be any leakage of traffic or revenue loss to the Concessionaire. In case of any such loss, it shall be fully borne by the Divisional Engineer, Construction & Maintenance (H), Madurai.
8. Any damages to the Project Highway and appurtenances shall be rectified by Divisional Engineer, Construction & Maintenance (H), Madurai as per Standards/ specifications at their own cost.
9. The Divisional Engineer, Construction & Maintenance (H), Madurai, shall bear any contractual issues / claims raised by the SPV/Concessionaire on account of construction of the slip road requested.
10. The Divisional Engineer, Construction & Maintenance (H), Madurai, shall ensure that construction of Slip roads does not in any way impeach safety and the smoothest traffic plying in NH and shall be solely responsible for safe construction procedures and the safety of road users.
11. Proper drainage shall be provided for proper draining of the surface water from the slip road from getting in to NHAI road at the cost of the divisional Engineer, Construction & Maintenance (H), Madurai and shall be responsible for maintenance of the same there on.
12. The maintenance of the Slip road & transition length (including boards & road markings) adjoining the main carriageway shall be done by the divisional Engineer, Construction & Maintenance (H), Madurai.
13. Adequate mandatory & cautionary and information boards shall be placed to guide the traffic plying on the main carriageway about the junction / slip road ahead and the maintenance of the same shall be carried out by the Divisional Engineer, Construction & Maintenance (H), Madurai.
14. The maintenance of the Slip road & transition length (including boards & road markings) adjoining the main carriageway shall be done by the divisional Engineer, Construction & Maintenance (H), Madurai.
15. Utilities such as electrical poles, Gantry signages, transformers, storm water drains, water pipelines, gas pipeline, sewer lines and any other utilities fouling in above said interchanges/slip road alignment, will be shifted by the divisional Engineer, Construction & Maintenance (H), Madurai, at its own efforts and cost in consultation with utility departments.
16. During construction any damage to carriageway, service road, drain, footpath, and flyover structure etc., if any occurs, it will be rectified by NHAI/ concessionaire at the risk and cost of the divisional Engineer, Construction & Maintenance (H), Madurai.
17. There are several well grown avenue plants / trees are exists both sides of the Highway. If there is a need to remove those plants Tree transplantation method may be adopted by the Divisional Engineer.

*Sar.*



MKTPL/PS/NHAI/O&M/2024/1248

Dated: 27-07-2024

18. All the norms mentioned in the latest guidelines for obtaining access permission / for the utilization of NHAI land shall be duly followed.

Notwithstanding the above, the Authority may consider the following important points in terms of road safety and future traffic growth as well,

There may be a chance of traffic conflicts due to existing median opening @ 2+100 where the vehicles coming from the proposed slip road, merging at Km. 2+200 and making u turns in to the main carriageway. Due to emerging Madurai city traffic there may be a chance of black spot / hot spot development. In this regard a detailed traffic study may be carried out.

Further the proposal may be modified to lengthen the proposed slip road up to 1+600 RHS so that slip road commuters can take U turn at the VUP Km. 0+900.

Also a provision shall be adopted to prevent the slip road commuter's entry in to the Km. 2+100 median opening in terms of potential traffic conflicts at this location.

Therefore, it is requested the Authority to peruse the above said observations over the Vaigai North Bank road with National Highways (NH-44) near Samayanallur proposal.

This is for your kind information.

For MADURAI KANYAKUMARI TOLLWAY PRIVATE LIMITED,

AMBATI SRINIVASA KIRAN KUMAR



Project Head

CC: The Team Leader, M/s. Artefact Projects Limited in association with Renaissance IOT LLP, Madurai



*Imp*  
From  
M. Mohanagandhi AMIE  
Divisional Engineer  
Highways, C&M  
Madurai

To  
The Project Director  
The National Highways Authority of India  
Plot No. 3, Surya Towers, 2<sup>nd</sup> Floor  
East Street, KK Nagar  
Madurai – 625 020

NHAI-PIU MADURAI	
IN: No	2583
DATE :	19/7/24
PD	
DM(T)	
LO	
SE	

Lr. No. 78/2024/JDO2

Date : 12.07.2024

Sir,

19/7/24  
(Pl putup)

**Subject: Request for No Objection Certificate (NOC) for Connecting proposed Vaigai North Bank road with National Highways [NH-44] near Samayanallur – reg**

- Ref:
1. Government of Tamilnadu GO.(Ms) No.32 Highways Minor Ports (HS1) department dated 04.03.2024.
  2. Divisional Engineer (H) C&M letter no. 78/2024/JDO2, dt.25.06.2024
  3. Project Director, NHAI-PIU, Madurai letter no. NHAI/PIU/MDU/NOC/NH-44/2024/1377

With reference (2) Joint Inspection conducted by Project Director with ADE, Highways on 04.07.2024, regarding the proposed connection of Vaigai North Bank road with National Highways near Samayanallur. The PD, NHAI, Madurai has noted some observations vide reference (3). The reply to the observations is stated as below

Observations	Reply
<b>Traffic towards Virudhunagar on NH-44 towards State Highways</b>	
The proposed road width is 7m with 1.50m of Earthen Shoulder. Due to the High embankment location, the proposed free slopes may not fit within the existing right-of-way (ROW)	The proposed road will be laid near to the NHAI road. There is enough ROW available to provide free slope as shown in drawing attached herewith
The ROW has not been delineated in the drawing	The ROW has been fixed by NHAI Concessionaire surveyor at field on 05.07.2024. After taking coordinates new

	drawing has been prepared in which ROW has been delineated.
Merging / Diverging points are not indicated in the drawing	The Chainages of NH-44 has been indicated to show where merging and diverging with NH takes place
The location being on MJB Approaches, quadrant pitching / protection works may be at risk of damage	The embankment for road will be formed on the slope pitching, hence any damage to remaining pitching will be rectified
<b>Traffic from State Highway Joining NH-44 (towards Dindigul)</b>	
The Finished Road Level (FRL) is proposed beneath the High Flood Level)	The FRL of road beneath Deck slab of Bridge is below HFL as well as Bed level to provide clear 4.50m height from soffit of beam
Existing levels of the MJB structure, HFL, and proposed road top level must be clearly depicted in the drawing	A cross-section drawing has been prepared showing all necessary details and attached herewith
Given the road is proposed beneath the HFL, any necessary NOC from irrigation /WRD should be provided	NOC requested from WRD
The Proposed vertical clearance of 3.55 m from the girder soffit level may not be adequate for traffic movement of large freight vehicles on MH-44	The proposed vertical clearance is 4.5m which is enough for all heavy vehicles except container trucks. The height gauge will be erected before entering beneath Bridge
Similar issues were observed with the proposed road width of 7m with 1.5 m of earthen shoulder due to its high embankment location	ROW delineated in drawing. The retaining wall is proposed where RoW is not sufficient to provide free slope (ie in chainage 7/800 - 7/900)
The proposed free slopes may not fit within the existing ROW	ROW delineated in drawing. The retaining wall is proposed where RoW is not sufficient to provide free slope (ie in chainage 7/800 - 7/900)
ROW details and merging/diverging points not indicated in the drawing	ROW details are provided in the drawing. The Chainages of NH-44 has been indicated to show where merging and diverging with NH takes place
As the location is on MJB Approaches, there is a risk to quadrant pitching/protection works	Any damages to Quadrant/protection works will be rectified

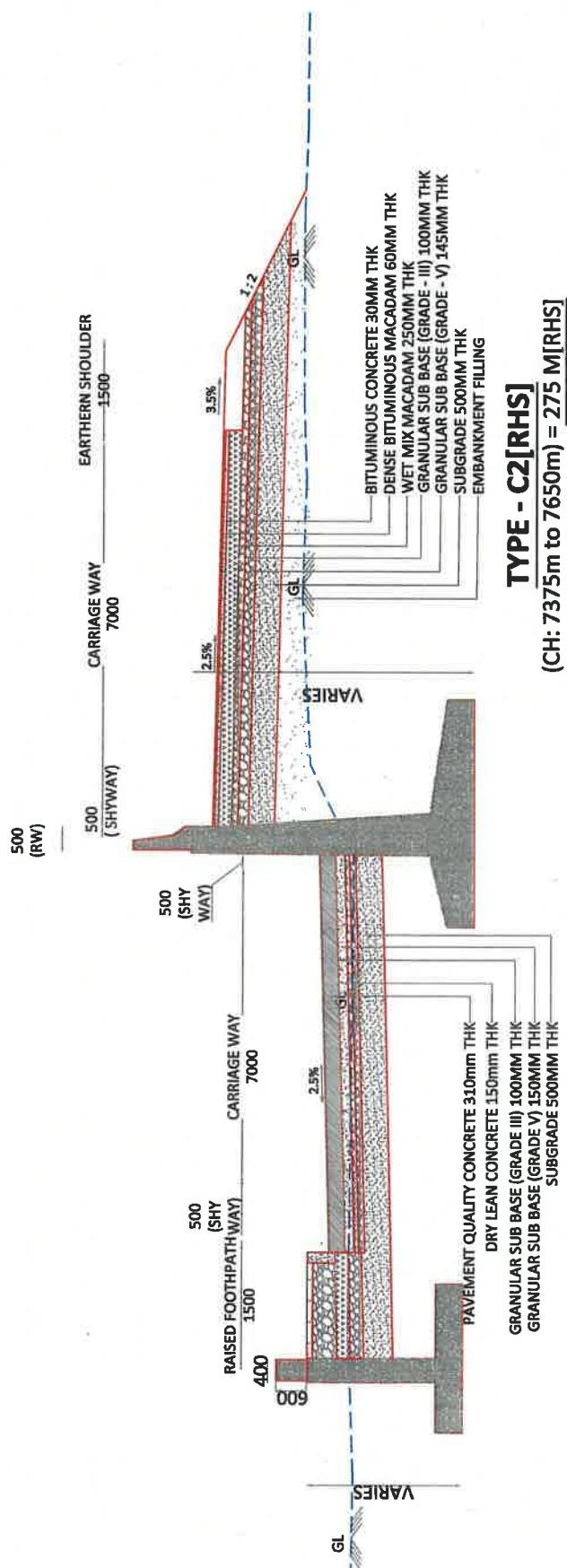
The overall layout plan of proposed road incorporating all details to comply the observation is submitted herewith. We kindly request the National Highways Authority of

India to grant the No Objection Certificate (NOC) for the proposed connection of the Vaigai North Bank Road with the Srinagar – Kanyakumari National Highway (NH-44) at the earliest.

Encl: Layout plan and cross sections



Divisional Engineer  
Highways C&M  
Madurai



**GS ASSOCIATES**  
HIGHWAY DESIGNERS, VALUERS, EXC-TECH CONSULTANTS  
No:12, Pasupathi Garden, Shenbaghoo Street,  
Edhi Nagar, Yanamangalore,  
Madurai - 625024  
Ph No: 9840073732  
Email : [gssassociatesmadurai@gmail.com](mailto:gssassociatesmadurai@gmail.com)

**DESIGN CONSULTANT :-**  
**GS ASSOCIATES**  
HIGHWAY DESIGNERS, VALUERS, EXC-TECH CONSULTANTS  
No:12, Pasupathi Garden, Shenbaghoo Street,  
Edhi Nagar, Yanamangalore,  
Madurai - 625024  
Ph No: 9840073732  
Email : [gssassociatesmadurai@gmail.com](mailto:gssassociatesmadurai@gmail.com)

**REVIEWED BY** B.P **CHECKED BY** M.S **APPROVED BY** P.R

**TITLE:-** TYPICAL CROSS SECTION (RAMP PORTION)

DWG NO : GSA/VAIGAI/TCS/004

DATE : **REV :**

SHEET : A4 **SCALE :** 1:100

**Assistant Divisional Engineer (H)**  
Construction And Maintenance  
Uttampatti

**Divisional Engineer (H)**  
C&M  
Mutturai

**NAME OF WORK:-**

CONSTRUCTION OF VAIGAI RIVER NORTH BANK ROAD KM 0/0-8/0 FROM KAMARAJ BRIDGE TO VARANASI KANNIYAKUMARI ROAD (NH44)(NEAR SAMAYANALLUR).

**CLIENT:-**

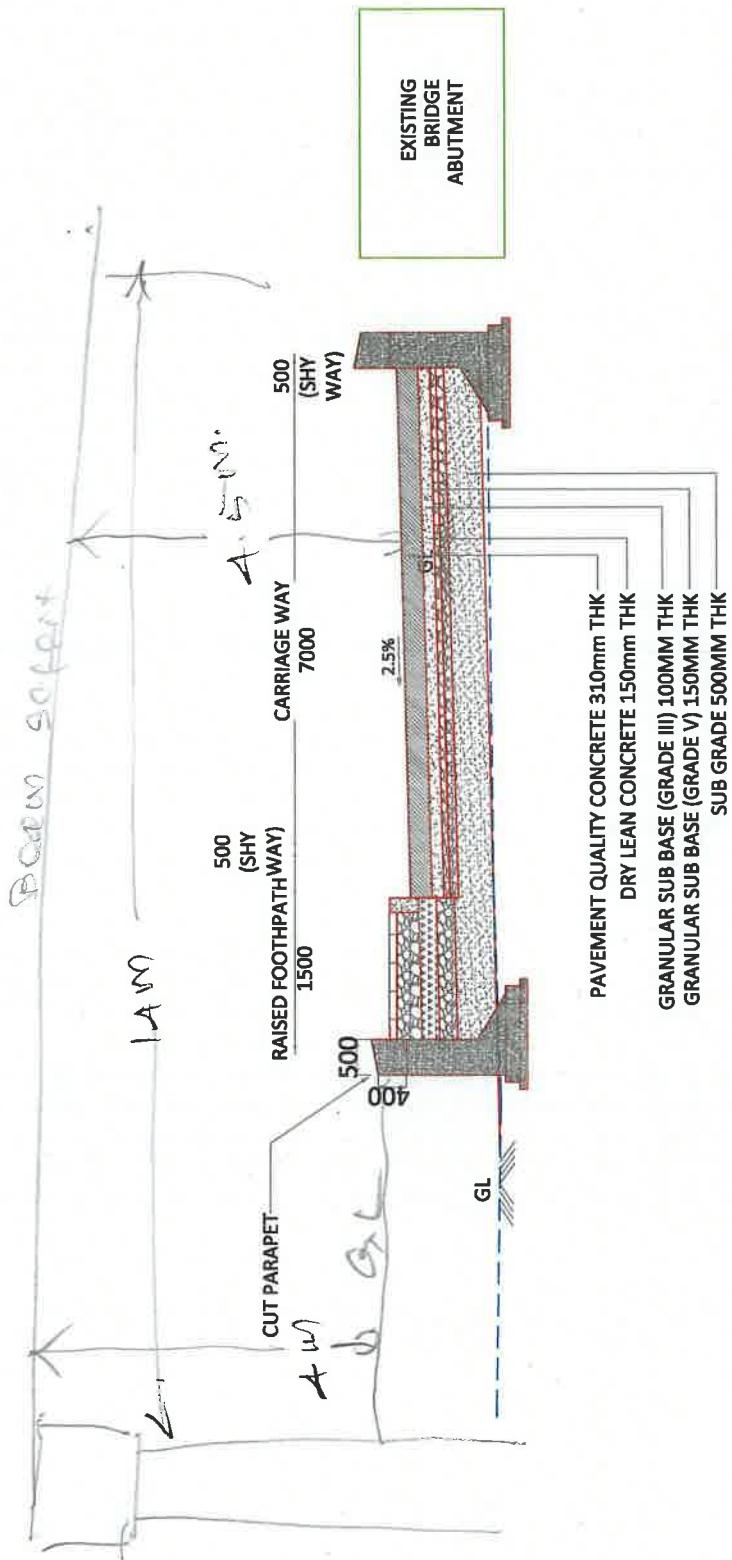
THE DIVISIONAL ENGINEER  
CONSTRUCTION AND MAINTENANCE  
HIGHWAYS DEPARTMENT  
MADURAI DIVISION

**TYPICAL CROSS SECTION (RAMP PORTION)**

DWG NO : GSA/VAIGAI/TCS/004

DATE : **REV :**

SHEET : A4 **SCALE :** 1:100

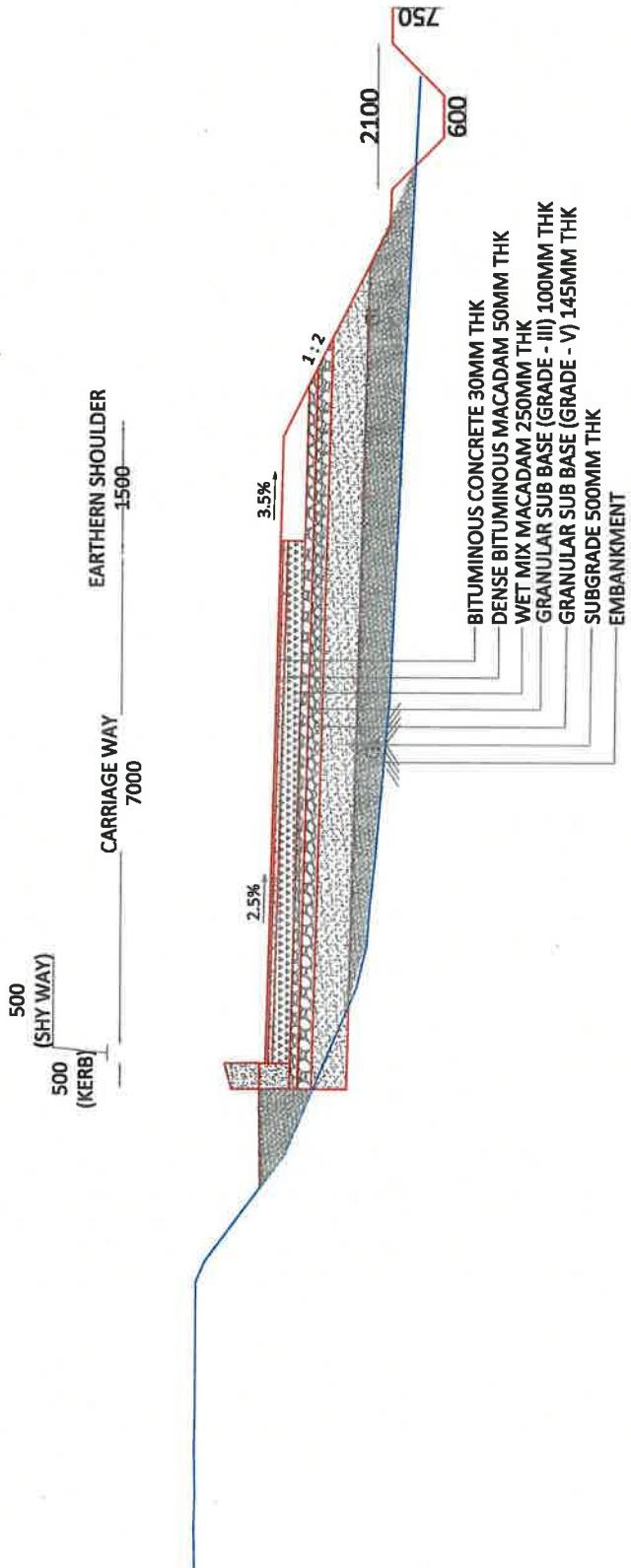


**TYPE - D1**  
(CH: 7550m to 7700m) = 150 M

TITLE:-		TYPICAL CROSS SECTION (RAMP PORTION)	
DWG NO : GSA/VAIGAI/TCS/005		DATE :	
ASSISTANT DIVISIONAL ENGINEER (H)	REV :	SHEET : A4	SCALE : 1:100
CONSTRUCTION AND MAINTENANCE			

Assistant Divisional Engineer (H)  
Construction And Maintenance  
Uttampatti

NAME OF WORK:-		CLIENT:-	
CONSTRUCTION OF VAIGAI RIVER NORTH BANK ROAD KM 0/0-8/0 FROM KAMARAJ BRIDGE TO VARANASI KANNIYAKUMARI ROAD (NH44)(NEAR SAMAYANALLUR).		THE DIVISIONAL ENGINEER CONSTRUCTION AND MAINTENANCE HIGHWAYS DEPARTMENT MADURAI DIVISION	
DESIGN CONSULTANT :-	GS ASSOCIATES ENGINEERS, HIGHWAY DESIGNERS, VALUERS, ETC-TECH CONSULTANTS No:12, Puthiyangal Garden, Shembagaboo Street, Ettai Nagar, Maduravoyal, Madurai - 625014 Ph No: 9640073732 Email: gsassociatesmadurais@gmail.com	CHECKED BY B.P	APPROVED BY M.S
			P.R



R4 S (CH: 7575m to 8000m) = 425 M

### TYPE - E

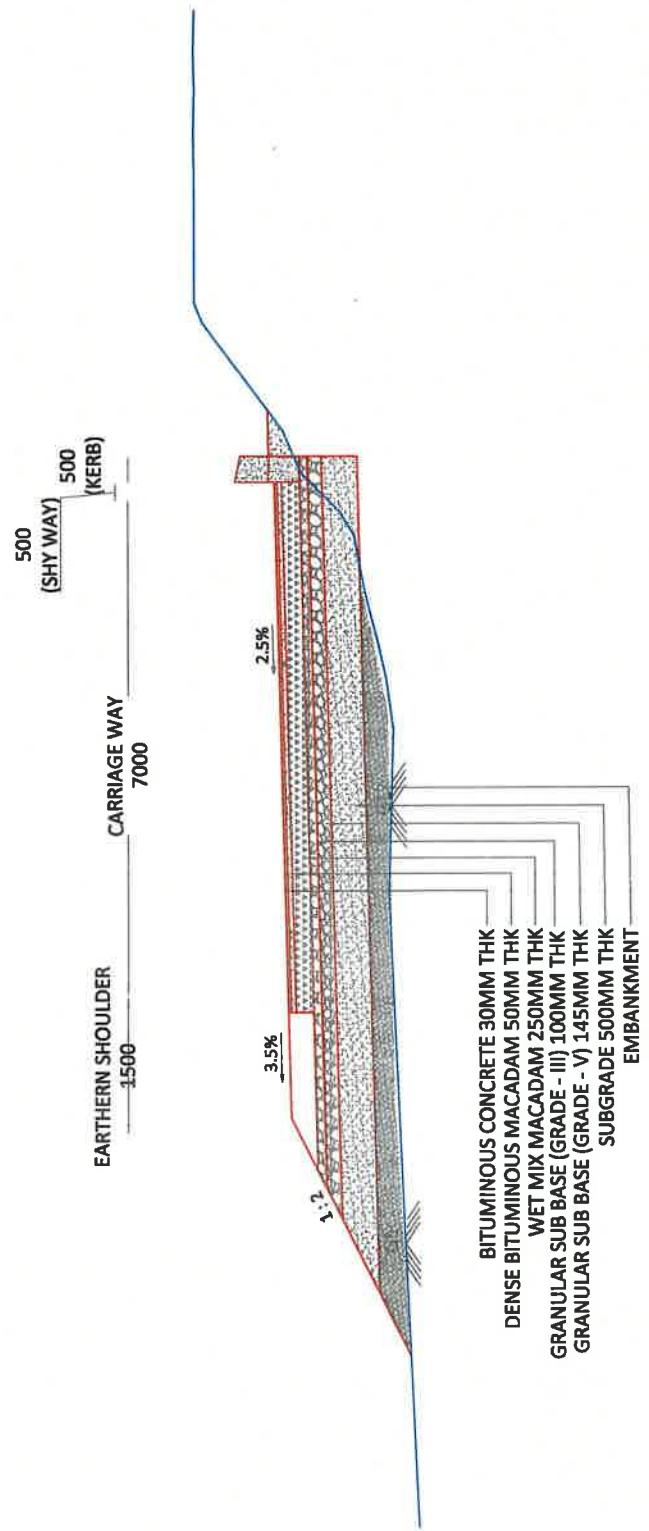
*12/21/24*  
Assistant Divisional Engineer (H)  
Construction And Maintenance  
Ullalampatti

*C-2*  
Divisional Engineer (H)  
C&M  
Madurai

DESIGN CONSULTANT :-	NAME OF WORK:-				
	SL.NO	DESCRIPTION	REV/NO	DATE	CLIENT:-
GS ASSOCIATES ENGINERS, HIGHWAY DESIGNERS, VALUERS, ETC-TECH CONSULTANTS No:12, Pushpanjali Garden, Shenbagaboo Street, Elli Nagar, Madurai, Tamilnadu, Ph. No: 9840737372 Email : <a href="mailto:gassociatesmadurai@gmail.com">gassociatesmadurai@gmail.com</a>	1	For Review	0		THE DIVISIONAL ENGINEER CONSTRUCTION AND MAINTENANCE HIGHWAYS DEPARTMENT MADRASI DIVISION

DESIGNED BY B.P	CHECKED BY M.S	APPROVED BY P.R	TYPICAL CROSS SECTION (RAMP PORTION)
			DWG NO : GSA/V/AIGA/TCS/007
			DATE :
			REV :

SHEET : A4  
SCALE : 1:100



12/7/24

Assistant Divisional Engineer (H)  
Construction And Maintenance  
Usilampatti

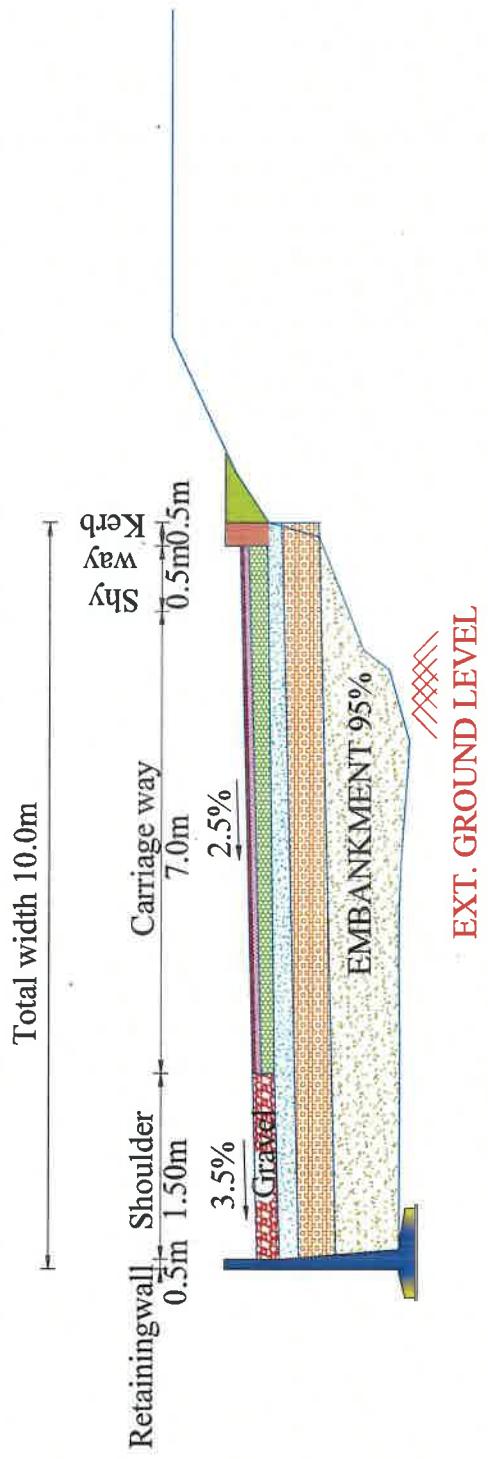
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DWG NO :	GSA/VAIGAI/TCS/006	
DATE :	REV. :	
SHEET : A4	SCALE : 1:100	

NAME OF WORK:-		CLIENT:-	
CONSTRUCTION OF VAIGAI RIVER NORTH BANK ROAD KM 0/0-8/0 FROM KAMARAJ BRIDGE TO VARANASI KANNIYAKUMARI ROAD (NH44)(NEAR SAMAYANALLUR).		THE DIVISIONAL ENGINEER CONSTRUCTION AND MAINTENANCE HIGHWAYS DEPARTMENT MADRASI DIVISION	
SL.NO	DESCRIPTION	REV.NO	DATE
1	For Review	0	

DESIGN CONSULTANT :-	APPROVED BY
GS ASSOCIATES ENGINEERS, HIGHWAY DESIGNERS, VALUERS, ETC-TECH CONSULTANTS No:12, Pushpanjali Garden, Shenbagapoo Street, Ettai Nagar, Maduravoyal, Madurai - 625014 Ph. No.: 9840073732 Email : gsassociatesmadurai@gmail.com	P.R
DESIGNED BY	CHECKED BY
B.P	M.S

## TYPICAL CROSS SECTION

LH<sub>4</sub> CH: 7/800 - 7/900



EXT. GROUND LEVEL

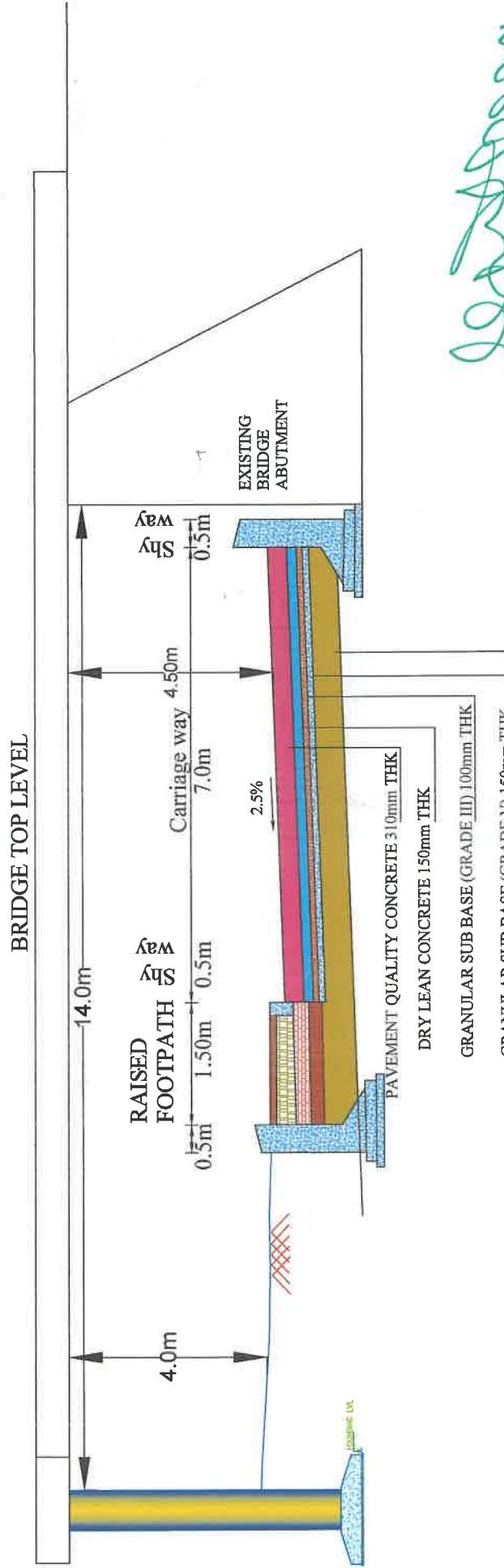
Assistant Divisional Engineer (H)  
Construction And Maintenance  
Usilampatti

Divisional Engineer (H)  
C&M  
Maturai

LEGEND	
	- BITUMINOUS CONCRETE
	- DENSE BITUMINOUS MACADAM
	- WET MIX MACADAM
	- GRANULAR SUB BASE III & V
SCALE:-	NOT TO SCALE

## TYPICAL CROSS SECTION

CH: 7/550 - 7650 (LHS)



12/7/24  
Assistant Divisional Engineer (H)  
Construction And Maintenance

Usilampatti

J. S.  
12/7/24

Dimensional Engineer  
Highways and  
Mandurai

NOT TO SCALE



**भारतीय राष्ट्रीय राजमार्ग प्राधिकरण  
(सड़क परिवहन और राजमार्ग मंत्रालय)**  
**National Highways Authority of India  
(Ministry of Road Transport & Highways)**

उप महाप्रबंधक (तकनीकी) सह परियोजना निदेशक के कार्यालय

परियोजना कार्यालय इकाई

Office of the Deputy General Manager (Tech) cum Project Director

Project Implementation Unit

प्लॉट नंबर : 3, सूर्या टवर्स, दूसरी मंजिल, पूर्व पहले स्ट्रीय, के.के. नगर, मदुरौ - 625020, तमिलनाडु.

Plot No: 3, Surya Towers, 2nd Floor, 1st East Street, K.K.Nagar, Madurai - 625 020, Tamilnadu.



दुरभाष / Phone : +91 452 2587750

वेबसाइट / Website : <http://www.nhai.org>

E-mail : [nhai\\_madurai\\_piu@yahoo.co.in](mailto:nhai_madurai_piu@yahoo.co.in)

[mad@nhai.org](mailto:mad@nhai.org)

NHAI/PIU/MDU/NOC/NH-44 & (TN-05)/2024/ 1380

6<sup>th</sup> July, 2024

To

1. The Project Head, M/s. MKTL Projects, Madurai	2. The Project Manager, M/s. DS Toll road projects, Dindigul
3. The Team Leader, M/s. Artefact Projects Ltd. in association with Renaissance IOT LLP, Door No.8/503, Ground Floor, TNHB Thoppur Site, Kappalur, Thirumangalam Taluk, Madurai - 625 008.	4. The Team Leader, M/s. Theme Engineering Services (P) Ltd, Door No.M1/200, 13 <sup>th</sup> Cross Street, R.M. Colony, Dindigul - 624 001.

Sub: NHAI-PIU, Madurai - Tolling, Operation, maintenance & Transfer of 4 laning of Madurai - Kayathar section from Km.0/000 to Km.116+500 of NH-44 (Old No.7) - Requesting NOC for connecting the proposed Vaigai North Bank road with National Highways (NH-44) near Samayanallur - Comments sought - Reg.

Ref: Divisional Engineer, Construction & Maintenance (H), Madurai Lr.No. 78/2024/JD02, dt.25.06.2024.

Sir,

With reference to the subject proposal received from DE (H), regarding the proposed connection of Vaigai North Bank road with National Highways near Samayanallur, a site Inspection has been conducted by the undersigned on 04.07.2024. Following which certain observation has been communicated for compliance.

2) Meanwhile, the proposal is also forwarded herewith to the Concessionaire & Independent Engineer to furnish their comments along with the consent for no objection certificate for the subject proposal in 7 days, please.

Yours faithfully,

(Keerthi Bharadwaj) 6/7/24  
Project Director,  
PIU-Madurai.

Copy to:

The Divisional Engineer (H), Construction & Maintenance, Madurai.



**भारतीय राष्ट्रीय राजमार्ग प्राधिकरण  
(सड़क परिवहन और राजमार्ग मंत्रालय)**  
**National Highways Authority of India  
(Ministry of Road Transport & Highways)**

उप महाप्रबंधक (तकनिकी) सह परियोजना निदेशक के कार्यालय

परियोजना कार्यालय इकाई

Office of the Deputy General Manager (Tech) cum Project Director

Project Implementation Unit

प्लॉट नंबर : 3, सूर्या टवर्स, दूसरी मंजिल, पूर्व पहले स्ट्रीय, के.के. नगर, मदुरै - 625020, तमில்நாடு.

Plot No: 3, Surya Towers, 2nd Floor, 1st East Street, K.K.Nagar, Madurai - 625 020, Tamilnadu.



दुर्भाष / Phone : +91 452 2587750

वेबसाइट / Website : <http://www.nhai.org>

E-mail : [nhai\\_madurai\\_piu@yahoo.co.in](mailto:nhai_madurai_piu@yahoo.co.in)

[mad@nhai.org](mailto:mad@nhai.org)

NHAI/PIU/MDU/NOC/NH-44/2024/ 1377

4<sup>th</sup> July, 2024

To

The Divisional Engineer,  
Construction & Maintenance (H),  
Madurai.

Sub: NHAI-PIU, Madurai - Tolling, Operation, maintenance & Transfer of 4 laning of Madurai - Kayathar section from Km.0/000 to Km.116+500 of NH-44 (Old No.7) - Requesting NOC for connecting the proposed Vaigai North Bank road with National Highways (NH-44) near Samayanallur - Reg.

Ref: Divisional Engineer, Construction & Maintenance (H), Madurai Lr.No. 78/2024/JDO2, dt.25.06.2024.

Sir,

With reference to Joint Inspection conducted with ADE, Highways on 04.07.2024, regarding the proposed connection of Vaigai North Bank road with National Highways near Samayanallur following observations were noted:

**I. Traffic towards Virudhunagar on NH-44 towards State Highway:**

- The proposed road width is 7m with 1.5m of Earthen Shoulder. Due to the high embankment location, the proposed free slopes may not fit within the existing NH right-of-way (ROW).
- The ROW has not been delineated in the drawing.
- Merging/diverging points are not indicated in the drawing.
- The location being on MJB Approaches, quadrant pitching/protection works may be at risk of damage.

**II. Traffic from State Highway Joining NH-44 (towards Dindigul):**

- The Finished Road Level (FRL) is proposed beneath the High Flood Level (HFL).
- Existing levels of the MJB structure, HFL, and proposed road top level must be clearly depicted in the drawing.

- Given the road is proposed beneath the HFL, any necessary NOC from Irrigation/WRD should be provided.
- The proposed vertical clearance of 3.55 meters from the Girder Soffit level may not be adequate for traffic movement of large freight vehicles on NH-44.
- Similar issues were observed with the proposed road width of 7m with 1.5m of Earthen Shoulder due to its high embankment location.
- The proposed free slopes may not fit within the existing NH ROW.
- ROW details and merging/diverging points not indicated in the drawing.
- As the location is on MJB Approaches, there is a risk to quadrant pitching/protection works.

2) Additionally, the overall layout plan of the proposed road starting from Kamaraj Bridge connecting NH-44 needs to be submitted.

3) The compliance to the above observations may be submitted at the earliest so as to take necessary action by this Office as per the NHAI Policy circular issued on "**Grant of Permission for Construction of Flyover/ Underpass/ Foot over bridge/ Canal crossing proposed by State Govt. Agencies across National Highways dated 03.11.2020**", please.

Yours faithfully,

  
(Keerthi Bharadwaj) 4/1/2024  
Project Director,  
PIU-Madurai.

Copy to:

1. District Collector, Madurai for information, please
2. Regional Officer, NHAI, Madurai - for kind information, please.
3. M/s. MKTL Projects, Madurai for information.
4. M/s. Artefact Projects Ltd. in association with Renaissance IOT LLP for information.
5. M/s. DSTRL, Dindigul - for information.
6. M/s. Theme Engineering Services Pvt Ltd., Dindigul for information.

\* Imp Jnct (pl putup)  
DM(T) | st mb | st ss  
Highways Department

From  
 Thiru. M. Mohanagandhi, A.M.I.E.,  
 Divisional Engineer,  
 Highways, C&M,  
 Madurai.

To  
 The Project Director,  
 National Highways Authority of India  
 Plot No. 3, Surya Towers, 2<sup>nd</sup> Floor,  
 East Street, KK Nagar,  
 Madurai – 625 020.

NHAI PIU MADURAI	
IN: No : 9933	DATE : 26/6/24
PD	
DM(T)	✓
LO	
SE	✓

Lr. No. 78/2024/JDO2

Date : 25.06.2024

Sir,

*Subject: Request for No Objection Certificate (NOC) for Connecting proposed Vaigai North Bank road with National Highways (NH-44) near Samayanallur - reg*

*Ref: Government of Tamilnadu GO.(Ms) No.32 Highways Minor Ports (HS1) department dated 04.03.2024.*

The proposed "Construction of Vaigai North Bank Road from Km 0/0 to Km 8/0, from Kamarajar Bridge to Varanasi – Kanyakumari Road (NH-44) near Samayanallur," has been administratively sanctioned by the Government of Tamil Nadu vide GO (Ms) No. 32 Highways Minor Ports (HS1) Department dated 04.03.2024. The estimate has been technically sanctioned on 04.03.2024 for the amount of 176 Crore . This road is proposed to connect with NH-44 by providing a service road at Km 2/8 of the Madurai – Kanyakumari stretch.

We hereby request a No Objection Certificate (NOC) to connect the proposed road with National Highway NH-44 at Km 2/8 of Madurai – Kanyakumari stretch. This connection is critical for improving the transportation network. A four-lane road already exists from Viraganur Junction to Kamarajar Bridge along the Vaigai North Bank, spanning a length of 8.70 km and is in public use. The proposed road is a continuation of the existing Vaigai Bank Road and will serve as a bypass to avoid congested city traffic when crossing Madurai. It will enhance connectivity between the Madurai Ring Road in the east and NH-44 in the west of Madurai.

The road begins at Kamarajar Bridge and ends at Srinagar - Kanyakumari Road (NH-44) at the backside of Samayanallur, spanning a total length of 8.00 km. It connects the abandoned NH Varanasi-Kanyakumari Road (SHU-100) near Kamarajar bridge and National Highway NH-44, Srinagar – Kanyakumari Road, near the Vaigai River Bridge at Samayanallur. The connection with NH-44 is proposed as service roads of 400 meters in length on either side of NH-44. One service road on the left side provides access to the Vaigai North Bank Road, while another service road on the right side provides access to NH-44 from the Vaigai North Bank Road. The Vaigai North Bank Road crosses NH-44 under the bridge vent adjacent to the Samayanallur side abutment.

This connection with NH-44 is intended to:

1. Provide seamless integration between the State Highway and the National Highway network.
2. Facilitate the smooth and efficient movement of goods and passengers.
3. Reduce travel time and improve road safety.
4. Boost economic activities by improving access to markets and resources.

The proposed connection will adhere to the Indian Roads Congress (IRC) standards. As a safety measure, we will ensure the installation of appropriate signage, road markings, pedestrian crossings, safety barriers, and implementation of traffic calming measures at critical points, all in accordance with IRC standards.

We kindly request the National Highways Authority of India to grant the No Objection Certificate (NOC) for the proposed connection of the Vaigai North Bank Road with the Srinagar – Kanyakumari National Highway (NH-44).

Enclosed with this proposal is the General Arrangement Drawing for your reference and consideration.



25/6/24  
Divisional Engineer  
Highways C&M  
Madurai

**NAME OF WORK: CONSTRUCTION OF VAIGAI RIVER NORTH BAK ROAD KM 0/0 - 8/0 FROM KAMARAJAR BRIDIGE TO VARANASI KANNIYAKUMARI ROAD (NH44) (NEAR SAMAYANALLUR)**



Athisayam Park Side ( Right Side ) Service Road Longitudinal Section



NAME OF WORK: CONSTRUCTION OF VAIGAI RIVER NORTH BAK ROAD KM 0/0 - 8/0 FROM KAMARAJAR BRIDGE TO VARANASI KANNIYAKUMARI ROAD (NH44) (NEAR SAMAYANALLUR)



### Left Side Service Road Longitudinal Section

