

CANDLESTICK POINT

MAJOR PHASE 1 CP APPLICATION

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COMMISSION ON COMMUNITY INVESTMENT AND INFRASTRUCTURE RESOLUTION NO. 13-2016

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TABLE OF CONTENTS

1. INTRODUCTION.....	4
1.1 PURPOSE OF THIS APPLICATION	4
1.2 INTRODUCTION TO CANDLESTICK POINT-HUNTERS POINT SHIPYARD PHASE 2	6
2. PROJECT OVERVIEW.....	9
2.1 SURROUNDING LAND USES	10
2.2 EIR APPROVED PROJECT LAND USE.....	11
2.3 PARKS & OPEN SPACE.....	13
2.4 SIGHTLINES AND VIEW CORRIDORS.....	16
2.5 TRANSPORTATION.....	17
2.6 PROJECT PHASING & SCHEDULE OF PERFORMANCE.....	23
3. COMMUNITY BENEFITS	27
3.1 COMMUNITY BENEFITS PLAN	28
4. SUSTAINABILITY & ENVIRONMENT.....	31
4.1 SUSTAINABILITY.....	32
4.2 ENVIRONMENTAL MITIGATIONS	34
5. MAJOR PHASE 1 CP OVERVIEW	35
5.1 MAJOR PHASE SUMMARY.....	37
5.2 PHASING & SCHEDULE OF PERFORMANCE WITHIN MAJOR PHASE 1 CP	38
5.3 DEVELOPMENT BLOCKS.....	39
6. LAND USE & MASSING	41
6.1 LAND USE SUMMARY.....	42
6.2 HOUSING.....	44
6.3 SITE SECTIONS	45
6.4 BUILDING HEIGHTS, BULK & MASSING.....	46
6.5 STREET WALL	52
6.6 BUILDING GROUND FLOOR TREATMENT.....	54
7. OPEN SPACE	55
7.1 MAJOR PHASE 1 CP PARKS & OPEN SPACE	56
7.2 ALICE GRIFFITH NEIGHBORHOOD PARK.....	60
7.3 BAYVIEW HILLSIDE OPEN SPACE / JAMESTOWN WALKER SLOPE	62
7.4 ALICE GRIFFITH COMMUNITY GARDEN	64
7.5 WEDGE PLAZA AND WEDGE PARK 2A.....	65
7.6 EARL STREET BOULEVARD PARK.....	68
8. TRANSPORTATION	69
8.1 TRANSPORTATION SUMMARY.....	70
8.2 STREET CROSS SECTIONS.....	74
8.3 PEDESTRIAN NETWORK.....	78
8.4 BICYCLE NETWORK	79
8.5 PUBLIC TRANSIT	80
8.6 ON-STREET PARKING.....	81
8.7 OFF-STREET PARKING & LOADING.....	82
9. UTILITIES	85
9.1 STORM WATER TREATMENT.....	86
9.2 STORM DRAIN.....	88
9.3 SANITARY SEWER.....	90
9.4 LOW PRESSURE WATER.....	91
9.5 AUXILIARY WATER SUPPLY	92
9.6 RECYCLED WATER	93
9.7 JOINT TRENCH.....	94
9.8 EXISTING CONDITIONS - GEOLOGY AND SOILS	95
10. PROPERTY OWNERSHIP & CONVEYANCE	97
10.1 EXISTING OWNERSHIP	98
10.2 PROPOSED SUBDIVISION MAPPING PROCESS	99
10.3 PUBLIC TRUST LANDS & CANDLESTICK POINT STATE RECREATION AREA AGREEMENTS	103
APPENDIX A – MAJOR PHASE 1 CP MITIGATION MONITORING REPORT (MMRP)	105
APPENDIX B – SCHEDULE OF PERFORMANCE	119
APPENDIX C – MAJOR PHASE 1 CP HOUSING DATA TABLE	123
APPENDIX D – MAJOR PHASE 1 CP PARKS & OPEN SPACE SCHEMATIC DESIGN	125
APPENDIX E – MAJOR PHASE 1 CP GEOTECHNICAL REPORT 138	
APPENDIX F – PRELIMINARY CONSTRUCTION PHASING	139
APPENDIX G – INSURANCE REQUIREMENTS	160
APPENDIX H – DDA MATERIAL CONDITIONS	168



LIST OF FIGURES AND TABLES

FIGURE 2.1 – VICINITY MAP WITH SURROUNDING LAND USES	10	FIGURE 7.12 – EARL STREET BOULEVARD PARK ILLUSTRATIVE PLAN	68	TABLE 2.1 – 2010 LAND USE SUMMARY	11
FIGURE 2.2 – PROJECT LAND USES	12	FIGURE 8.1 – PRIMARY STREETS	71	TABLE 2.2 – PARKS & OPEN SPACE AREAS	14
FIGURE 2.3 – PARKS & OPEN SPACE NETWORK	14	FIGURE 8.2 – STREET NETWORK	73	TABLE 3.1 – MAJOR PHASE 1 CP COMMUNITY BENEFITS COMPLIANCE ..	30
FIGURE 2.4 – VIEW CORRIDORS	16	FIGURE 8.3 – PEDESTRIAN CIRCULATION	78	TABLE 5.1 – DEVELOPMENT BLOCK AREAS	39
FIGURE 2.5 – TRANSPORTATION NETWORK	18	FIGURE 8.4 – BICYCLE NETWORK	79	TABLE 5.2 – DEVELOPABLE SUB-PHASE AREAS	39
FIGURE 2.6 – PEDESTRIAN CIRCULATION	19	FIGURE 8.6 – MUNI ROUTE 29 AND ROUTE 56 INTERIM ROUTES	80	TABLE 6.1 – LAND USE BY SUB-PHASE	43
FIGURE 2.7 – BICYCLE NETWORKS	20	FIGURE 8.5 – FINAL TRANSIT ROUTES	80	TABLE 6.2 – HOUSING	44
FIGURE 2.8 – TRANSIT NETWORKS	21	FIGURE 8.7 – ON-STREET PARKING	81	TABLE 6.3 – DEVELOPMENT BLOCK COVERAGE	50
FIGURE 2.9 – BUS RAPID TRANSIT	22	FIGURE 9.1 – APPROXIMATE PERCENT OF LINEAR STREET FRONTAGE REQUIRED FOR BIOFILTRATION FACILITIES	87	TABLE 6.4 – MASSING - ALL BUILDING TYPES	51
FIGURE 2.10 – CANDLESTICK POINT/HUNTERS POINT SHIPYARD PHASE 2 – MAJOR PHASES	23	FIGURE 9.2 – STORM DRAIN	89	TABLE 7.1 – MAJOR PHASE 1 CP PARKS AND OPEN SPACE ACREAGES ..	56
FIGURE 5.1 – CANDLESTICK POINT – MAJOR PHASES AND SUB-PHASES ..	38	FIGURE 9.3 – SEPARATED SANITARY SYSTEM	90	TABLE 8.1 – TRAVEL LANES - STREETS WITH TRANSIT	73
FIGURE 5.2 – DEVELOPMENT BLOCK DIMENSIONS	39	FIGURE 9.4 – LOW PRESSURE WATER	91	TABLE 8.2 – ESTIMATED ON-STREET PARKING & ADA PARKING	81
FIGURE 6.1 – MAJOR PHASE 1 CP LAND USE	42	FIGURE 9.5 – AUXILIARY WATER SUPPLY	92	TABLE 8.3 – MAXIMUM OFF-STREET PARKING	82
FIGURE 6.2 – LOCATION OF BELOW MARKET RATE LOTS	44	FIGURE 9.6 – RECYCLED WATER	93	TABLE 8.4 – BICYCLE PARKING SPACES FOR RESIDENTIAL USES	82
FIGURE 6.3 – SECTION A-A: SITE SECTION ACROSS CP CENTER	45	FIGURE 9.7 – JOINT TRENCH	94	TABLE 8.5 – BICYCLE PARKING SPACES FOR COMMERCIAL USES	82
FIGURE 6.4 – SECTION B-B: SITE SECTION THROUGH MAJOR PHASE 1 CP - STARTING FROM THE LEFT: AG-5, AG-4, AG-2 AND AG-1	45	FIGURE 9.8 – EXISTING CONDITIONS - GEOLOGY AND SOILS	95	TABLE 8.6 – REQUIRED CAR-SHARE/RESIDENTIAL	83
FIGURE 6.5 – BUILDING HEIGHTS	46	FIGURE 10.1 – EXISTING OWNERSHIP	98	TABLE 8.7 – REQUIRED CAR-SHARE/NON-RESIDENTIAL	83
FIGURE 6.6 – MAJOR PHASE 1 CP MASSING LOOKING NORTH	47	FIGURE 10.2 – CANDLESTICK POINT TENTATIVE TRANSFER MAP - FOR ILLUSTRATIVE PURPOSES ONLY	100	TABLE 10.1 – APPROXIMATE LOT SIZES	99
FIGURE 6.7 – MAJOR PHASE 1 CP MASSING	48	FIGURE 10.3 – SUB-PHASE CP-01 FINAL MAP	101		
FIGURE 6.8 – MAJOR PHASE 1 CP MASSING LOOKING SOUTH	49	FIGURE 10.4 – CANDLESTICK POINT TENTATIVE SUBDIVISION MAP - FOR ILLUSTRATIVE PURPOSES ONLY	102		
FIGURE 6.9 – DEVELOPMENT COVERAGE	50	FIGURE 10.5 – PUBLIC TRUST LANDS	104		
FIGURE 6.10 – APPARENT FACE	51				
FIGURE 7.1 – MAJOR PHASE 1 CP PARKS AND OPEN SPACE	56				
FIGURE 7.2 – ALICE GRIFFITH NEIGHBORHOOD PARK - ILLUSTRATIVE PLAN	60				
FIGURE 7.3 – ALICE GRIFFITH NEIGHBORHOOD PARK - SECTION 1	61				
FIGURE 7.4 – BAYVIEW HILLSIDE OPEN SPACE / JAMESTOWN WALKER SLOPE - ILLUSTRATIVE PLAN	62				
FIGURE 7.5 – BAYVIEW HILLSIDE OPEN SPACE- SECTION 1	63				
FIGURE 7.6 – ALICE GRIFFITH COMMUNITY GARDEN - ILLUSTRATIVE PLAN	64				
FIGURE 7.7 – WEDGE PLAZA - ILLUSTRATIVE PLAN	65				
FIGURE 7.8 – WEDGE PLAZA - SECTION 1	66				
FIGURE 7.9 – WEDGE PLAZA - SECTION 2	66				
FIGURE 7.10 – ADA COMPLIANT PAVING PATTERN, CUSTOM DESIGNED BENCHES, EUROPEAN POCKET PLAZA	66				
FIGURE 7.11 – EARL STREET BOULEVARD PARK CROSS SECTION	68				

1. INTRODUCTION

1.1 PURPOSE OF THIS APPLICATION

In January of 2014, implementation of the Candlestick Point-Hunters Point Shipyard Phase 2 project (Project) reached an important milestone – the approval of the first Major Phase Application (Major Phase 1 CP). This represented the first of the Project's eight Major Phases of development that will span the next twenty years.

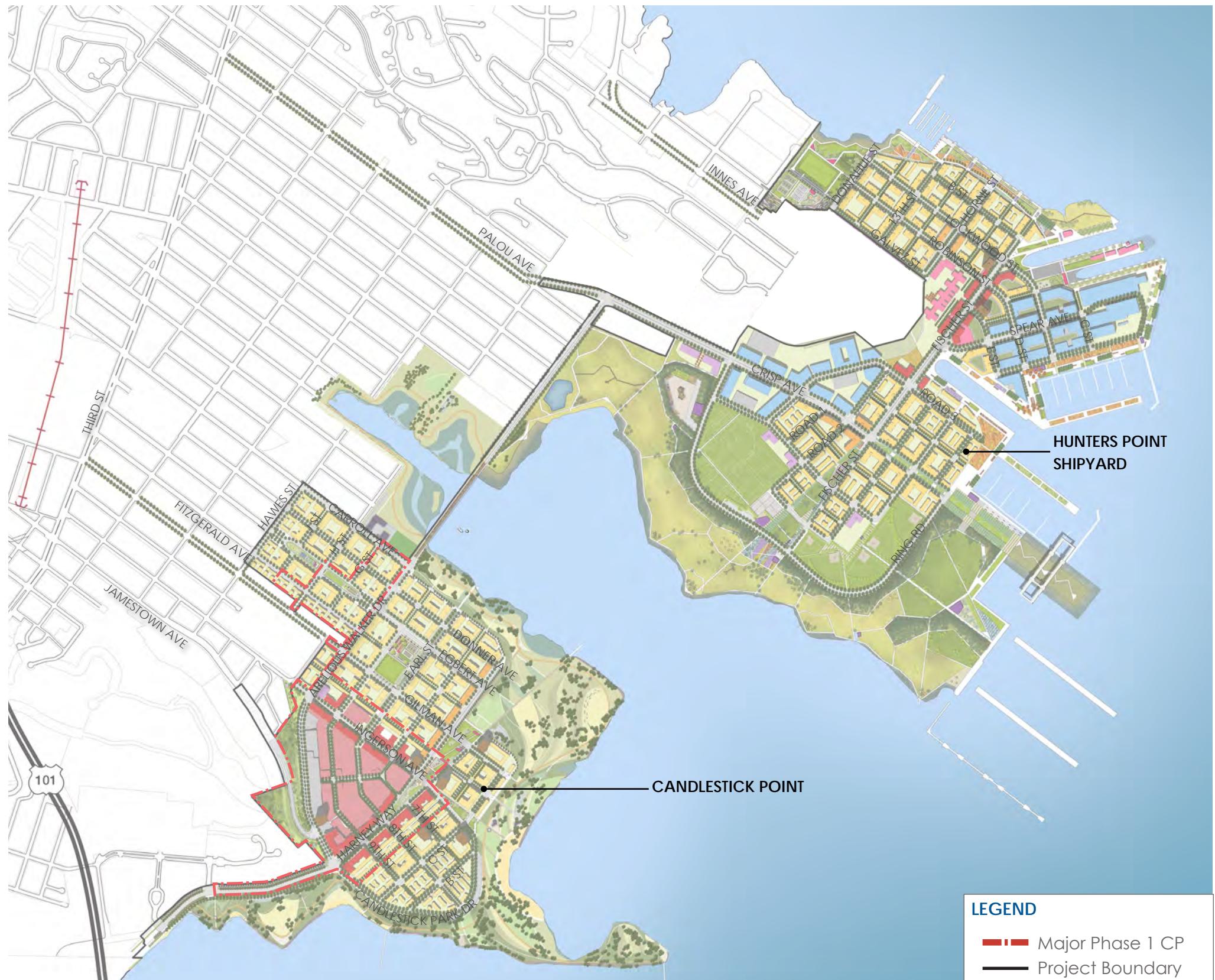
The Major Phase Application was submitted by the Developer in accordance with the Disposition and Development Agreement for Candlestick Point and Phase 2 of the Hunters Point Shipyard, dated June 3, 2010 (as amended, the "DDA"). The purpose of the Major Phase Application is to provide City staff and the community with a consolidated set of plans and reports for the specific geographic area that comprises the first Major Phase of development of the Project, which will occur on the Candlestick Point. In accordance with Section 22.7 of the DDA, Appendix G outlines insurance requirements required of the Developer in connection with this Major Phase.

Appendix H summarizes material conditions that must be satisfied under the DDA during the course of the Major Phase.

Since the approval of the Major Phase 1 CP Application, several Project refinements have been made in consultation with staff from the Office of Community Investment and Infrastructure (OCII), Successor Agency to the San Francisco Redevelopment Agency, multiple City multiple departments, and the Developer's consultant team. The updates to the plans were analyzed under the California Environmental Quality Act (CEQA) and a Fourth Addendum to the Project Environmental Impact Report (EIR) was prepared. The CEQA analysis determined that the conclusions reached in the Final EIR, certified on June 3, 2010, remain valid; that the proposed revisions to the project will not cause new significant impacts not identified in the EIR; and that no new mitigation measures will be necessary to reduce significant impacts. A complete summary of these revisions is set forth in a staff report to the OCII Commission prepared in support of this Major Phase Application. A brief summary of these refinements is below.

Concurrent with this Major Phase Application, the Developer has also prepared and submitted updates to the Candlestick Point Streetscape Master Plan and Design for Development (D4D) to ensure consistency between the Project's controlling documents. Updates to these Project documents and the Major Phase Application include the following:

- Interdepartmental refinements to all Candlestick Point public right-of-way cross sections
- Adjustment to the Sub-Phase CP-04 Boundary to increase block depths
- Tower shifts related to the increase in Sub-Phase CP-04 block depths and constructability of the original Sub-Phase CP-02 tower location
- Relocation of displaced on-street parking to CP Center garage
- Conversion of Office Space to Neighborhood Retail
- Height increases
- Removal of some bulb-outs, at the direction of the San Francisco Public Utilities Commission (SFPUC) and the San Francisco Fire Department (SFFD)



- Phasing of Harney Way improvements at the direction of the San Francisco Municipal Transportation Agency (SFMTA)
- Interdepartmental recommendation to revise the street cross section for Gilman Avenue

Updates to the Major Phase Application, Streetscape Master Plan and D4D were presented to the Hunters Point Shipyard Citizens Advisory Committee (CAC) for review and comment, as required by the design review and approval process for the Project. The CAC endorsed this updated Major Phase Application on September 14, 2015.

Street Cross-Sections – Post approval of the Major Phase Application, OCII engaged an inter-departmental team to review each of the cross-sections on Candlestick Point to ensure that appropriate clearances for emergency vehicle access were provided, while at the same time an intimate, neighborhood character was retained. These discussions culminated in the approval of a new set of street cross-sections for all of Candlestick Point in June 2014. The cross-sections are included in recent updates to the Major Phase Application, Infrastructure Plan, Transportation Plan, Streetscape Plan and Design for Development documents, and shown on the Candlestick Point Vesting Tentative Subdivision Map (No.7878). The new cross-sections for Sub-Phases CP-02-03-04 are included in this Major Phase Application. These cross sections will continue to be refined as the design process progresses.

Adjustment to the Sub-Phase CP-04 Boundary – The Developer is proposing to increase the depth of the blocks within Sub-Phase CP-04 and concurrently, adjust the Sub-Phase boundary for Sub-Phase CP-04. The Sub-Phase CP-04 block depths, approved June 3, 2010, are substantially less than other blocks at Candlestick Point because they were originally conceived to be predominately retail blocks with a service alley (the mid-block break) along the back of these blocks. After rethinking the development planned for these blocks in relation to Sub-Phases CP-02 and CP-03, the Developer now proposes to increase the block boundaries by the depth of a townhome. Townhomes could then line the mid-block break. This proposal allows greater variety in housing types and more efficient land use on these blocks.

One implication of this boundary adjustment is that block CPS 6b, an OCII affordable housing lot on the opposite side of the new boundary, has been reduced in size. However, block CPS 11a, another OCII lot, has increased in size. The Project Housing Map dated January 7, 2014, identifies 165 units on block CPS 6b. The same map identifies only 90 units on block CPS 11a.

As directed by OCII, this application identifies 150 units on block CPS 11a, 60 more units than were initially programmed for the site. A fit test of the housing programmed for CPS 6b showed that 148 of the 165 units programmed for the site could be accommodated with the proposed block dimensions. The additional units on block CPS 11a more than make up for the loss of 17 units on CPS 6b. The boundary adjustment will be reflected in Final Transfer and Subdivision Maps for these blocks.

Tower Locations – The proposed Tower G location at CP Center, (Sub-Phase CP-02), is outside the approved tower zone. Planning for the site revealed structural integration and construction timing concerns with a tower that was

collocated with the parking garage. The proposed shift moves the tower and its core structural components off the garage.

As described above, the Developer proposes to increase the depth of the blocks within Sub-Phase CP-04, and concurrently adjust the Sub-Phase boundary for Sub-Phase CP-04. This shift of the Sub-Phase CP-04 boundary results in an equivalent shift in the boundary of Sub-Phases just east of Sub-Phase CP-04, Sub-Phases CP-10 and CP-11. As a result, towers J and K within Sub-Phases CP-10 and CP-11 will shift as well.

Input from the California Department of Parks and Recreation (State Parks) was solicited during the review of these proposed tower locations. Visual and shadow analyses for the new proposed sites of Towers G, J, and K have been performed, reviewed, and accepted by the City, and are reflected in an updated D4D approved by the Planning Commission March 3, 2016.

On-Street Parking Replacement – Parallel parking stall dimensions in Section 143 of the 2010 D4D were used to estimate the number of total on-street parking spaces for the project (2010 Transportation Plan, Page 68). In the course of engineering Sub-Phases CP-02-03-04, the Developer and design team determined that the actual number of on street parking spaces is significantly reduced once other design considerations, such as fire hydrants and driveway cuts, are taken into account. Given the constraints to creating on-street parking, Sub-Phases CP-02-03-04 accommodates only 161 of the 430 on-street parking spaces initially estimated to be available within the site plan – a loss of 269 parking spaces. San Francisco Planning Department staff determined that the best way to make up for the lost parking is to add the 269 lost parking spaces to the garage below CP Center.

Conversion of Office Space to Neighborhood Retail – Site planning for Sub-Phases CP-02-03-04 revealed demand for neighborhood serving retail that exceeded the original entitlement of 125,000 gsf of space intended for that use. Given these market conditions, the Developer proposes to convert 15,500 gsf of office space to 6,000 gsf of neighborhood retail based on transportation, air quality, and Green House Gas analysis. This increase in the total amount of neighborhood serving retail will result in a robust neighborhood retail program which meets demand for shops and services in the new urban core of Candlestick Point.

Height Increases – The mixed use development along Harney Way and Ingerson Avenue (1 floor of retail with 4-5 stories of residences above), is zoned for a maximum height of 65 feet. The Developer proposes to allow these heights to increase to 80 feet, while at the same time restricting the number of residential floors above retail to five. The increased height is proposed to allow for greater retail floor to floor height and to allow variety in the architecture and design of the residential buildings.

The proposed D4D modifications include an increase in the height of the 220-room hotel from 65 feet to 80 feet. The increase in height would ensure consistency in the built form along Harney Way, and allow greater flexibility to design the building as an iconic entry statement to CP Center given its important location at the intersection of Arelious Walker Drive and Harney Way. The additional height would also allow for a taller floor-to-floor height at ground level, which would provide flexibility for different uses and amenities. No increase to the number of hotel rooms or the floor space (150,000 square

feet) is proposed.

A Film Arts Center is planned at CP Center on the corner of Harney Way and Ingerson Avenue. The building will frame a public plaza at the intersection and have high-quality architectural treatment that reinforces its central location. The iconic building will have active day and evening uses (such as retail and entertainment) that anchor development at CP Center and reinforce its community importance. The allowable height of this building has increased from 85 feet to 120 feet. This height increase will allow the building to be an architectural anchor in the neighborhood, and a focal point for the community.

Bulb-outs – At the direction of the San Francisco Public Utilities Commission, (SFPUC), and the San Francisco Fire Department, (SFFD), several bulb-outs planned along Ingerson Avenue and Harney Way have been removed from the Improvement Plans. SFPUC does not allow utilities underneath bulb-outs, and the bulb-outs interfere with SFFD turning requirements. In addition, the Better Streets Plan requires streets within commercial areas to be designed for vehicular access of a "SU-30" truck. This requirement has also been a factor in removing bulb-outs.

Harney Way Off-Site Phasing – The Candlestick Park Infrastructure Plan identifies Harney Way between Arelious Walker Drive and Thomas Mellon Circle as an off-site improvement. At present, there is uncertainty regarding the timing of the extension of Geneva Avenue and replacement of the US 101 / Harney Way interchange. It is likely that the interchange will not be constructed prior to operation of the Bus Rapid Transit (BRT) system, which would preclude the originally conceived BRT alignment. The San Francisco County Transportation Authority (SFCTA) is currently conducting a study to define an alternate BRT alignment that uses some combination of existing tunnels underneath US 101 at Blanken Avenue and Alana Way. Because that alignment may affect the way in which the BRT lanes are constructed along Harney Way, the SFCTA and the City would like to postpone reconstruction of Harney Way between Executive Park Boulevard East and Thomas Mellon Drive. This would mean that in Major Phase 1, Harney Way would be constructed between Arelious Walker Drive and Executive Park East only, although the sidewalk and Class I cycletrack would be completed all the way to Thomas Mellon Drive. The BRT lanes between Executive Park Boulevard East and Thomas Mellon Drive would then be constructed consistent with a permanent alignment, to be determined at a later date, but still prior to operation of the BRT.

Gilman Avenue Off-Site Cross Section – A survey of Gilman Avenue (including the location of utilities and driveways) revealed that the approved cross section would require either the reduction of sidewalk widths or the relocation of utility poles. To avoid these issues, the civil and traffic engineers employed by the Developer worked with staff from OCII, SF Planning, SFMTA and the San Francisco Department of Public Works (SFDPW) to design a cross section for Gilman Avenue that differs from that shown in the Infrastructure Plan. Fehr & Peers prepared analysis and findings associated with a revised concept for Gilman Avenue to provide 1 lane of traffic in each direction and a central right and left turn lane that will allow automobiles and buses to travel without the hindrance of turning movements. The new cross-section also provides a sidewalk "furnishing zone" for street trees and benches.

1. INTRODUCTION

1.2 INTRODUCTION TO CANDLESTICK POINT-HUNTERS POINT SHIPYARD PHASE 2

Candlestick Point-Hunters Point Shipyard Phase 2 will be a model of integrated planning and sustainable design. The site is located on 702 acres along the southeastern waterfront in San Francisco. The site includes Hunters Point Shipyard, the former location of the San Francisco 49er's Candlestick Park stadium, the Candlestick Point State Recreation Area (CPSRA), as well as the Alice Griffith public housing complex.

The Project seamlessly integrates new housing, retail, commercial and parks into adjacent neighborhoods with a new street grid that ties into existing City streets. New bike routes and the extension of the Bay Trail / Blue Greenway throughout the Project site tie the Project back to the city. Most importantly, the Project extends existing transit service, creates new transit services that connect the Project to Caltrain and BART, and provides new downtown shuttles from both Candlestick and the Shipyard.

In addition to the compact physical plan, the Project will be in the forefront of sustainable "green" development practices. The community is pre-certified LEED-ND Gold from the U.S. Green Building Council.



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2. PROJECT OVERVIEW

- 2.1 SURROUNDING LAND USES
- 2.2 EIR APPROVED PROJECT LAND USE
- 2.3 PARKS & OPEN SPACE
- 2.4 TRANSPORTATION
- 2.5 PROJECT PHASING & SCHEDULE OF PERFORMANCE



2. PROJECT OVERVIEW

2.1 SURROUNDING LAND USES

The Project site is part of the larger Bayview Hunters Point neighborhood, an area characterized by well-established residential neighborhoods, commercial uses, and industrial areas. Existing land uses in this neighborhood are described below by type of use: commercial/retail, civic and institutional, residential, industrial, and open space and recreation.

Commercial and retail uses are distributed throughout the neighborhood. Third Street, which includes neighborhood-serving retail shops and other commercial businesses, is the central north/south corridor through the community. This corridor includes a variety of shops, eating establishments, cleaners, beauty supply stores, hardware stores, groceries, and liquor stores. Bayview Plaza near Evans Avenue provides a cluster of retail uses, including a Walgreens, a copy shop, several restaurants, and offices. Along Bayshore Boulevard and in proximity to the I 280 and US 101 freeways in the northern part of the neighborhood are a number of auto-oriented retail uses, including large-scale commercial uses with off-street parking frontages, home improvement businesses, and fast food establishments.

A number of civic, institutional, religious, and social service uses are also centered on Third Street. Such uses include the Bayview Opera House and Plaza at Third Street and Oakdale Avenue, a central feature of the Bayview Hunters Point community; Bayview Hunters Point Multipurpose Senior Center; the Southeast Health Center; the Anna E. Waden Library; and the Southeast Community Facility, which houses a City College campus and a job training and career program and is a site for community meetings and civic events. Other institutional and social services, including the Bayview YMCA, are found on Hunters Point Hill.

Residential portions of the Bayview Hunters Point neighborhood are east and west of Third Street from US 101 to the Hunters Point Shipyard. A majority of the existing residential uses are single-family homes. However, there are older multi-family units distributed on the lower slopes of Bayview Hill and new multi-family units along Jamestown Avenue, Williams Avenue, and Innes Avenue. Mixed-use developments, including multi-family housing, are also being developed along the Third Street corridor. In addition, much of the residential development on Hunters Point Hill consists of multi-family housing units.

Industrial uses are found in the northern portion of the Bayview Hunters Point neighborhood, west and east of Third Street. This area includes many Production, Distribution, and Repair (PDR) uses and mixed-use development. Immediately west of Third Street and south of the Islais Creek Channel, large industrial uses, such as regional moving and storage companies and wholesale distributors are intermingled with a range of small, local businesses, such as auto parts distributors and bulk mail assembly services. The San Francisco Produce District is in this area.

Light industrial and PDR uses occupy the South Basin industrial area surrounding Yosemite Slough, extending west to US 101. The South Basin industrial area contains a variety of small-scale industrial uses, such as auto repair shops, food distributors, bulk warehouses, and recycling facilities. The India Basin Industrial Park, to the northwest of India Basin and the Hunters Point Shipyard and south of the Islais Creek Channel, includes a major distribution facility for the US Postal Service, light industrial, commercial service and multimedia businesses, and some retail businesses located at Bayview Plaza at the southeast corner of Third Street and Evans Avenue. Vacant parcels and buildings are distributed throughout all of the identified industrial areas.

Figure 2.1 – Vicinity Map with Surrounding Land Uses



2. PROJECT OVERVIEW

2.2 EIR APPROVED PROJECT LAND USE

This page represents the Project land use as entitled in 2010. As the Project is built out, exact square footages will vary. This is the case in Major Phase 1 CP. Refer to Section 6 for details on the land use program for Major Phase 1 CP.

Residential

The Project consists of 10,500 for-sale and rental residential units, including approximately 7,155 Market Rate Units and approximately 3,345 Below-Market Rate Units. The homes range in size from studios to five bedrooms. Housing types include two- and three-story townhomes over parking, four- to seven-story low-rise flats over podium parking, eight- to 21-story mid-rise flats, and 22- to 42-story high-rise towers. Commercial uses and community services will be located in the lower floors of some residential buildings.

Regional Retail

A regional retail center of up to 635,000 gross square feet (gsf) is proposed on the Candlestick Point. Retailers could include a variety of general merchandise, apparel, food service and restaurants, and entertainment related businesses to serve the regional market. Community services may also be allowed on sites designated for regional retail uses.

Neighborhood Retail

Neighborhood retail sites are designated at both the Candlestick Point and the Hunters Point Shipyard, and in addition, small-scale neighborhood retail uses could be established throughout the Project site depending on demand. The Project was originally entitled for 250,000 gsf of neighborhood retail. Since then, some office space has been converted to neighborhood retail, as discussed in Section 1.1 of this Major Phase Application. This space could include convenience goods (e.g., food, drugs and groceries) and personal services (e.g., laundry, dry cleaning, barbering, and shoe repair) for daily needs of the immediate neighborhood. The allocation of square footages is discussed in detail in Section 6.1 of this Major Phase Application.

Office

The Project was originally entitled for up to 150,000 gsf of office uses on the Candlestick Point. As market analysis showed demand for neighborhood retail at Candlestick Point that exceeded the Project's entitlement, some of this space was converted to the equivalent amount of Neighborhood Retail. The staff report accompanying this Major Phase Application describes this conversion in detail. The allocation of square footages is discussed in detail in Section 6.1 of this Major Phase Application.

Research and Development

Hunters Point Shipyard Phase 2 is the planned site of up to 3,000,000 gsf of Research and Development (R&D) space. The R&D facilities could serve a wide range of possible office, laboratory, and light industrial uses including emerging industries and technologies such as green technology and biotechnology. Hotel

A 220-room hotel is proposed on the Candlestick Point.

Artists' Studios/Arts Center

Up to 225,000 gsf of artists' studios and accessory neighborhood retail is proposed on the Hunters Point Shipyard and 30,000 gsf is anticipated to be dedicated for the construction of an arts center.

Community Facilities

Community serving uses are proposed at sites on both the Candlestick Point (up to 50,000 gsf) and the Hunters Point Shipyard (up to 50,000 gsf). Proposed uses include a fire station on 0.5 acre at the Hunters Point Shipyard. In addition, uses may include police facilities, healthcare, day-care, senior centers, library, recreation centers, and community centers.

Parks and Open Space

An estimated 327 acres of new public parks, sports fields, and other open space is planned for the Project.

Marina

A 300-slip marina is proposed at the Hunters Point Shipyard. A marina could include utilities at each slip and a sewage pump-out. Landside amenities could include a classroom facility to teach sailing, restrooms, and showers.

Performance Venue/Arena/Film Arts Center

The Project was originally entitled for a 10,000 seat, 75,000 gsf Performance Venue/Arena. After a significant amount of financial analysis and consultation with potential operators, a performance venue of this size was determined to be financially infeasible. Currently, a 42,000 gsf Film Arts Center for theatre film productions, film festivals, concerts, speaking engagements, or educational events, is proposed at the Candlestick Point. The balance of the original 75,000 gsf performance venue entitlement (33,000 gsf) will be developed in another building within Sub-Phase CP-02. The allocation of square footages is discussed in detail in Section 6.1 of this Major Phase Application.

Table 2.1 – 2010 Land Use Summary¹

LAND USE	CANDLESTICK POINT	HUNTERS POINT SHIPYARD 2	PROJECT TOTAL
Residential Density I (Units) (15-75 Units Per Acre)	922	1,275	2,197
Residential Density II (Units) (50-125 Units Per Acre)	3,893	2,235	6,128
Residential Density III (Units) (100-175 Units Per Acre)	600	455	1,055
Residential Density IV (Units) (175-285 Units Per Acre)	810	310	1,120
Total Residents Units	6,225	4,275	10,500
Neighborhood Retail (GSF)	125,000	125,000	250,000
Regional Retail (GSF)	635,000	-	635,000
Office (GSF)	150,000	-	150,000
Performance Venue (GSF) (10,000 Seats)	75,000 (10,000 Seats)	-	75,000 (10,000 Seats)
Hotel (GSF) 220 Rooms	150,000 220 Rooms	-	150,000 220 Rooms
Research & Development (GSF)	-	3,000,000	3,000,000
Total Commercial Area	1,135,000	3,125,000	4,260,000
Artists' Studio/Art Centre (GSF)	-	255,000	255,000
Community Use (GSF)	50,000	50,000	100,000
Total Parks & Open Space (AC)	106	221	327

¹ Distribution of units among residential densities is estimated above. This distribution will shift with the design process and market conditions.

2. PROJECT OVERVIEW

2.2 PROJECT LAND USE

Figure 2.2 – Project Land Uses



2. PROJECT OVERVIEW

2.3 PARKS & OPEN SPACE

Existing Parks and Open Space Outside of the Project Area

The existing and previously planned parks adjacent to the Project Site include urban, neighborhood parks such as Adam Rogers Park, Hilltop Park, Ridgetop Plaza, and Little Hollywood Park. In Hunters Point, Adam Rogers Park includes a community garden, basketball court, playground, and BBQ area. Hilltop Park has a skateboard park, amphitheater, playground and picnic tables. Ridgetop Plaza is a small plaza offering views of the area. Near Candlestick Point, Little Hollywood Park has a playground and basketball court. Milton Meyer Recreation Center in Hunters Point and Gilman Park in Candlestick Point primarily offer sports facilities with indoor and outdoor basketball, baseball, and tennis courts as well as children's play areas. The planned Hillside and Hilltop Parks in Hunters Point Shipyard Phase 1 provide areas for recreation, gathering, pedestrian connections and children's play. Pocket parks address neighborhood needs for open space.

Nearby natural park areas include India Basin Shoreline Park and Heron's Head Park to the north of Hunters Point, and Bayview Hill Park at the southern edge of Candlestick Point. Candlestick Point State Recreation Area, while largely within the Project Site, also includes the 34-acre Yosemite Slough, just outside of the Project Site. Yosemite Slough is being restored by the State Parks Department in partnership with the non-profit California State Parks Foundation. The partially completed restoration project will include 12 acres of tidal wetlands and marsh, habitat for shore birds, and connections to the Bay Trail/Blue Greenway.

Existing Parks Inside the Project Area

Candlestick Point State Recreation Area

Approximately 97 acres of the Candlestick Point State Recreation Area (CPSRA) are included within the Project Site. At the southern portion of the CPSRA, existing features include planting, pathways, a beach, fishing piers, picnic areas, parking, and restrooms. The northern portion of the CPSRA is less developed and includes native planting areas and gravel parking lots that have been used as parking for the 49ers on game days.

Bayview Hill

Bayview Hill offers dramatic views of San Francisco, San Bruno Mountain, and the Bay. The park is home to a diverse collection of plants and animals, including wildflower grasslands, several species of snakes and lizards, red-tailed hawks, and great horned owls, all of which visitors can observe along the walking path that begins at Key Avenue. A small portion of the southeast slope of the park is located within the Project boundaries, though steep slopes and quarry-faces currently preclude visitor access to this area.

Project Parks and Open Space Highlights

The Project will create a continuous network of interconnected recreational facilities, promoting the use of the existing parks, such as the Candlestick Point State Recreation Area, as well as new parks, sports fields, and active urban recreation uses. A network of pedestrian and bike pathways will connect Project uses to adjacent neighborhoods and ensure unrestricted public access to the parks and open space on the Project site and the San Francisco Bay shoreline.

Extensive Parkland

Approximately 327 acres will be dedicated to new and improved parks, open space, and habitat areas. These areas cover nearly half the site's acreage and represent San Francisco's largest park development since Golden Gate Park.

Neighborhood Parks

New neighborhood parks will serve existing and future neighborhood residents with places for community gathering and a broad range of outdoor recreation and leisure activities.

Sports Field Complex

A new Community Sports Field Complex will help to meet the City's unmet demand for lighted sports fields. The sports fields will accommodate youth, high school, and adult field sports and will be suitable for regional tournaments.

Cultural Heritage Park

The Heritage Park will relate the history of Hunters Point to visitors from throughout the Bay Area and beyond. Historic buildings within the park will be retained and may be used as museum spaces.

Trails Network

The San Francisco Bay Trail/Blue Greenway will provide a continuous recreational multi-use trail along the Candlestick and Hunters Point waterfront, filling a gap in the regional network planned to eventually encircle the entire Bay. Similarly, kayak and windsurf launch points will enhance access to the regionally-planned Bay Area Water Trail. For commuters and neighborhood cyclists, a secondary network of off-street multi-use trails will link parks and neighborhoods with the on-street bicycle network.

Habitat Enhancements

New parks, open space, and habitat restoration areas will support the biodiversity and ecology of the San Francisco Bay shoreline. The plan features new native grasslands, wetlands, extensive planting of native trees and shrubs, and a net removal of bay fill.

Green Infrastructure and Urban Sustainability

Parks and open space will be designed as "green infrastructure" integrating urban design and infrastructure with natural systems. Elements of this system could include ecological storm water treatment systems, vegetated parking, and street-side and median boulevard parks.



2. PROJECT OVERVIEW

2.3 PARKS & OPEN SPACE

Table 2.2 – Parks & Open Space Areas^{1,2}

Park Name	Acreage
Hunters Point	
Urban Parks	
(1) Northside Parks	12.8
(2) Waterfront Promenade North	7.3
(3) Heritage Park	16.0
(4) Waterfront Promenade South	24.6
(5) Grasslands Ecology Park	86.9
(6) Shipyard Wedge Park	4.6
(7) Shipyard South Park	0.8
Subtotal	153.0
Sports Fields, Waterfront Recreation & Education	
(8) Waterfront Recreation & Education Park	6.7
(9) Multi-use Fields	24.7
(10) Community Sports Field Complex	31.7
(11) Maintenance Yard	4.8
Subtotal	67.9
Other Parks & Open Space (*excluded from total acreage)	
(12) Horne Boulevard Park	0.6
(13) Shipyard Hillside Open Space	2.6
(14) Re-gunning Crane Pier Habitats	9.5
Subtotal	12.7
HUNTERS POINT SUBTOTAL	
	220.9
Candlestick Point	
Urban Parks	
Source: Vesting Tentative Subdivision Map No. 7878, June 19, 2014	
(15) Alice Griffith Neighborhood Park	1.44
(16) Candlestick Point North Neighborhood Park	3.11
(17) Wedge Park	3.74
(18) Mini Wedge Park	0.82
Subtotal	9.11
Other Parks & Open Space (excluded from total acreage)	
Source: Vesting Tentative Subdivision Map No. 7878, June 19, 2014	
(19) Earl Boulevard Parks	0.37
(20) Jamestown Walker Slope	3.88
(21) Bayview Hillside Open Space	2.85
Subtotal	7.10
CANDLESTICK POINT STATE RECREATION AREA	
Source: Parks, Open Space, and Habitat Concept Plan, Approved June 3, 2010	
(22) South Basin Shoreline (Grasslands South 1 [5.1 ac] + South 2 [5.3 ac])	10.4
(23) Candlestick Meadow (Bayview Gardens [9.5 ac] + The Last Rubble [24.5 ac])	34.0
(24) The Heart of the Park (Wind Meadow [11.3 ac] + Heart of the Park [15.5 ac])	26.8
(25) The Point	6.1
(26) The Neck	4.9
(27) Last Port	14.6
Subtotal	96.8
CANDLESTICK POINT SUBTOTAL	
	105.91
GRAND TOTAL	
	326.81

¹ Italics indicate park names from CPSRA General Plan, approved January 2013
² Parentheses indicate park name from Schedule of Performance

³ The park acreages within the area of Vesting Tentative Subdivision Map No. 7878, dated June 19, 2014 are based on the final street and block alignments, and have been measured to the nearest hundredth of an acre. The CPSRA and Hunters Point park acreages have not been measured to this level of precision, and are estimated to the nearest tenth of an acre.

Figure 2.3 – Parks & Open Space Network



2. PROJECT OVERVIEW

2.3 PARKS & OPEN SPACE

Candlestick Point State Recreation Area

As California's first urban state park, Candlestick Point State Recreation Area (CPSRA) provides access to open space, the Bay, and recreational opportunities in a highly urbanized area of San Francisco.

The shoreline of CPSRA is perhaps its most defining feature. The park skirts the western shore of San Francisco Bay for approximately 3.25 miles, offering access to the Bay and long-range scenic views. Visitors from the local and regional community engage in a wide range of day-use recreation activities, including trail use, picnicking, windsurfing, wildlife viewing, and beach use, among others.

The park stewards important natural and cultural resources. A rare open space resource in San Francisco's southeastern corner, CPSRA provides habitat for birds, small mammals, and other wildlife. The park's position along the Pacific flyway makes it a valuable stopover for migrating birds. CPSRA's history of use, from the Ohlone people, to Chinese fishing camps, to the filling of the Bay, enriches its story as the state's first urban state park.

The Project includes the reconfiguration of the boundaries of CPSRA, as well as park improvements and an ongoing source of funding for park operation and maintenance, as approved by Senate Bill 792 (SB 792). After reconfiguration, CPSRA will encompass 96.8 acres, which will be improved according to the CPSRA General Plan. The General Plan proposes park improvements and new facilities throughout seven geographic areas within the park, as described below:

Grasslands North & South

This area of the existing CPSRA is largely undeveloped and has been used for game-day stadium parking. A new Grasslands North & South area could be improved with native grasslands, glade lawns, and earthworks shaped to provide shelter from the wind and enhance views. Site features could include overlooks, restrooms, and parking.

Bayview Gardens North

Bayview Gardens North was formerly developed as a boat launch, but siltation of the South Basin has caused this use to be abandoned. The existing paved parking area is used for gameday stadium parking. Located between the bay and the proposed Bayview Gardens / Wedge Park, the Bayview Gardens North area offers the greatest integration of urban and naturalized open spaces anywhere in the open space system and will be a strong visual gateway to the CPSRA and the bay. Bioswales, storm water 'Eco-Gardens,' and a potential salt-marsh restoration are central features of this area.

The Last Rubble

Until recently, the Last Rubble area was characterized by large piles of rubble and debris, remnants of the site's previous use as a dumping ground. The California Integrated Waste Management Board completed a rubble and debris removal project in April 2009. As a result of this, the majority of the rubble and debris was either removed or crushed on site. This area of the CPSRA remains underutilized and is not currently programmed for recreation, with the exception of a walking path. As the Last Rubble Area will be located adjacent to a substantial urban population, this area could be transformed into a new center for the CPSRA, with a wide variety of program elements. The park ranger station/visitor's center could be located here as well as a "Great Meadow" for passive recreation and park events. Other features may include parking, picnic areas, overlook terraces, restrooms, and a restaurant/café.

Wind Meadow

The Wind Meadow includes part of the existing CPSRA, including the Main Beach. This area will be reconfigured to meet the new urban development edge and interface with the Mini-Wedge Neighborhood Park. This area will contain a secondary entry and parking lot, and gateway entry kiosk for the CPSRA. Features here may include new restrooms, picnic areas, waterfront overlooks, expanded tidal wetlands, and access to the water.

The Heart of the Park

The Heart of the Park is part of the existing developed CPSRA. New park area will be added and the existing landscape structure will be retained and enhanced. Planting and overall aesthetics will be improved, pedestrian pathways will be renewed and added, and program areas will be developed for greater use. Site features could include upgraded restrooms, overlook terraces, large and small group picnic areas, and an interpretive amphitheater.

The Point

The landscape of The Point will be revitalized with improvements focused on pedestrian circulation, safety and way finding; intensifying areas for increased use; improving the overall park aesthetics and landscape ecology; and reconnecting visitors to the bay shoreline. Native grasslands and shorelines will be restored and stabilized, providing areas for activities such as strolling, picnics, kite flying, and fishing.

The Neck

The Neck is a narrow, eroded section of the CPSRA that includes a beach and pier. Park area will be added here to increase the width of the park and provide a continuous park experience along the shoreline. New features here could include a parking lot, windsurf/kayak launch, overlook, and picnic areas.

Last Port

The landscape of the Last Port will be revitalized with improvements focused on pedestrian circulation, safety and way finding; intensifying areas for increased use; improving the overall park aesthetics and landscape ecology; and reconnecting visitors to the bay shoreline. Native grasslands and shorelines will be restored and stabilized, providing areas for activities such as strolling, picnics, kite flying, fishing, and direct access to the bay for swimming, kayaking, and windsurfing.

2.4 SIGHTLINES AND VIEW CORRIDORS

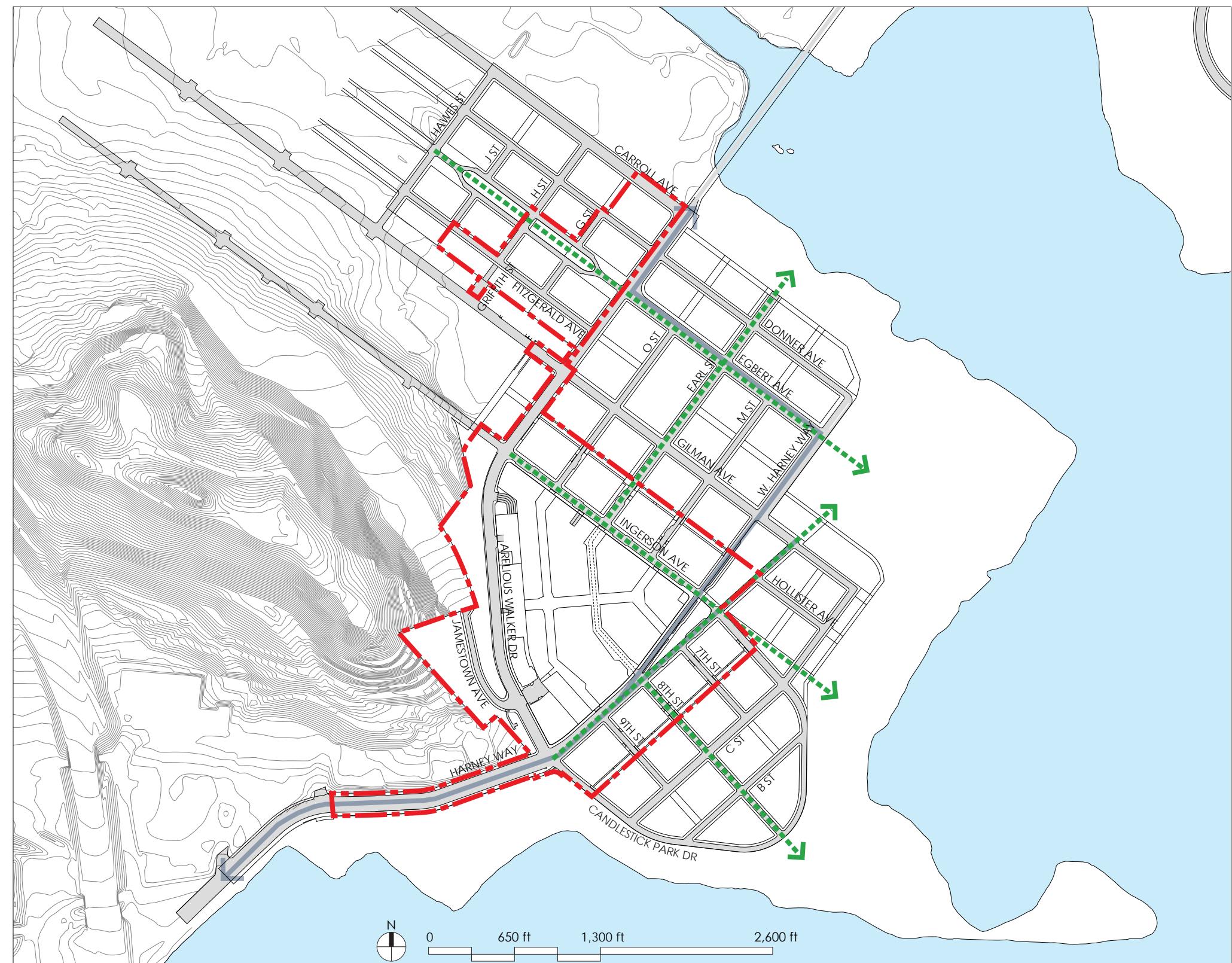
Sightlines from the community to the Bay and other important landmarks should be maintained and reinforced. View corridors bring a host of benefits; they provide important links to the Candlestick Point State Recreation Area, they act as a wayfinding tool, and they bring a sense of connectivity to the community. Figure 2.4 identifies significant view corridors within Major Phase 1 CP.

Each neighborhood will have a special place (e.g. neighborhood park) and an important street (or streets) which lead to the water. The interconnected network of public spaces is both a connective tissue and tool to develop the character of each neighborhood. Each place hosts the opportunity to develop narratives (historic, cultural, etc.) through wayfinding or artwork installations.

Special places, neighborhood parks, important streets such as The Spine and retail routes, view corridors and points of contact with the waterfront become opportunities for art installations to be embedded in the overall streetscape plan.

The Public Trust Exchange Agreement does not identify any view corridors on Candlestick Point.

Figure 2.4 – View Corridors



2.5 TRANSPORTATION

The street and circulation network for the Project is designed to facilitate efficient movement of people and goods throughout and beyond the community, but also establishes a welcoming public realm and community character. To that end, the street network extends the existing grid of the adjacent Bayview neighborhood, using typical Bayview block sizes.

In addition, a new bridge at Yosemite Slough will create a critical linkage between Hunters Point Shipyard, Candlestick Point, and regional transportation hubs such as US 101, Caltrain, BART, and Muni Metro.

In keeping with the City's Transit First, Complete Streets, and Better Streets policies, the street system is designed to prioritize walking, bicycling, and transit use; support the use of streets as public spaces for social interaction and community life; and provide green spaces that enhance the City's ecological function.

More information regarding the Project's transportation network can be found in the Infrastructure Plan, Transportation Plan, and Streetscape Master Plan.

2. PROJECT OVERVIEW

2.5 TRANSPORTATION

Vehicular Network

Existing roadways will be improved and new facilities built to ensure efficient vehicle circulation within the site and connections to regional traffic facilities.

- **Harney Way** will provide the primary auto access between Candlestick Point and US 101. The Project will improve and reconfigure the roadway to provide at least two auto lanes in each direction, left-turn lanes where appropriate, two bus rapid transit (BRT) lanes, a cycletrack and sidewalk. This will provide efficient auto access between the Candlestick Point site and US 101, and portions of the City to the west. Figure 2.5 shows two possible BRT route continuations for the Harney Way off-site improvements. SFMTA is currently conducting a study of the BRT route which will determine the configuration of the Harney Way off-site improvements.
- **Gilman Avenue** will have an enhanced streetscape design, including street trees, sidewalk plantings, furnishings and paving treatments will enhance pedestrian safety and visually tie together the proposed project with the greater Bayview neighborhood. Between Arelious Walker Drive and Third Street, Gilman Avenue will be re-configured to provide four new signals, one travel lane in each direction, and a center turn lane.
- **Ingerson Avenue** and **Jamestown Avenue** are two primary routes between the Project site and the Third Street retail core in the Bayview neighborhood. These two avenues are planned to be resurfaced and restriped as part of the second Major Phase for the Candlestick Point.
- **Carroll Avenue, Ingalls Street, Thomas Avenue and Griffith Street** form an important route for automobile travel corridor between Candlestick Point and Hunters Point Shipyard will be improved to provide two lanes in each direction during peak periods.
- **Palou Avenue** is a “Transit Priority Street” and will receive streetscape improvements during the second Major Phase of Hunters Point Shipyard. In addition, six new traffic signals will be installed at major intersections to provide transit priority through the corridor.
- **Innes Avenue** provides the primary auto access between Hunters Point Shipyard and US 101 and Interstate 280, as well as the northern portions of San Francisco. The avenue includes two lanes of travel in each direction, parking and sidewalks. The Project will provide streetscape improvements to Innes Avenue to create an attractive gateway into the Project Site. These improvements are planned as part of the first Major Phase for the Hunters Point Shipyard.

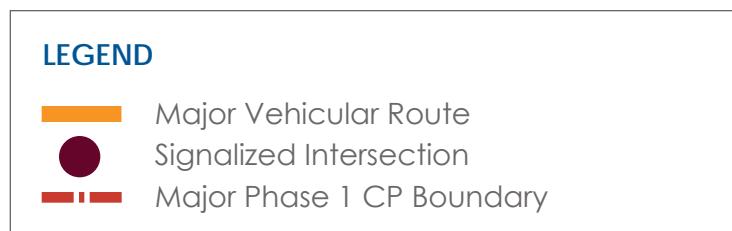
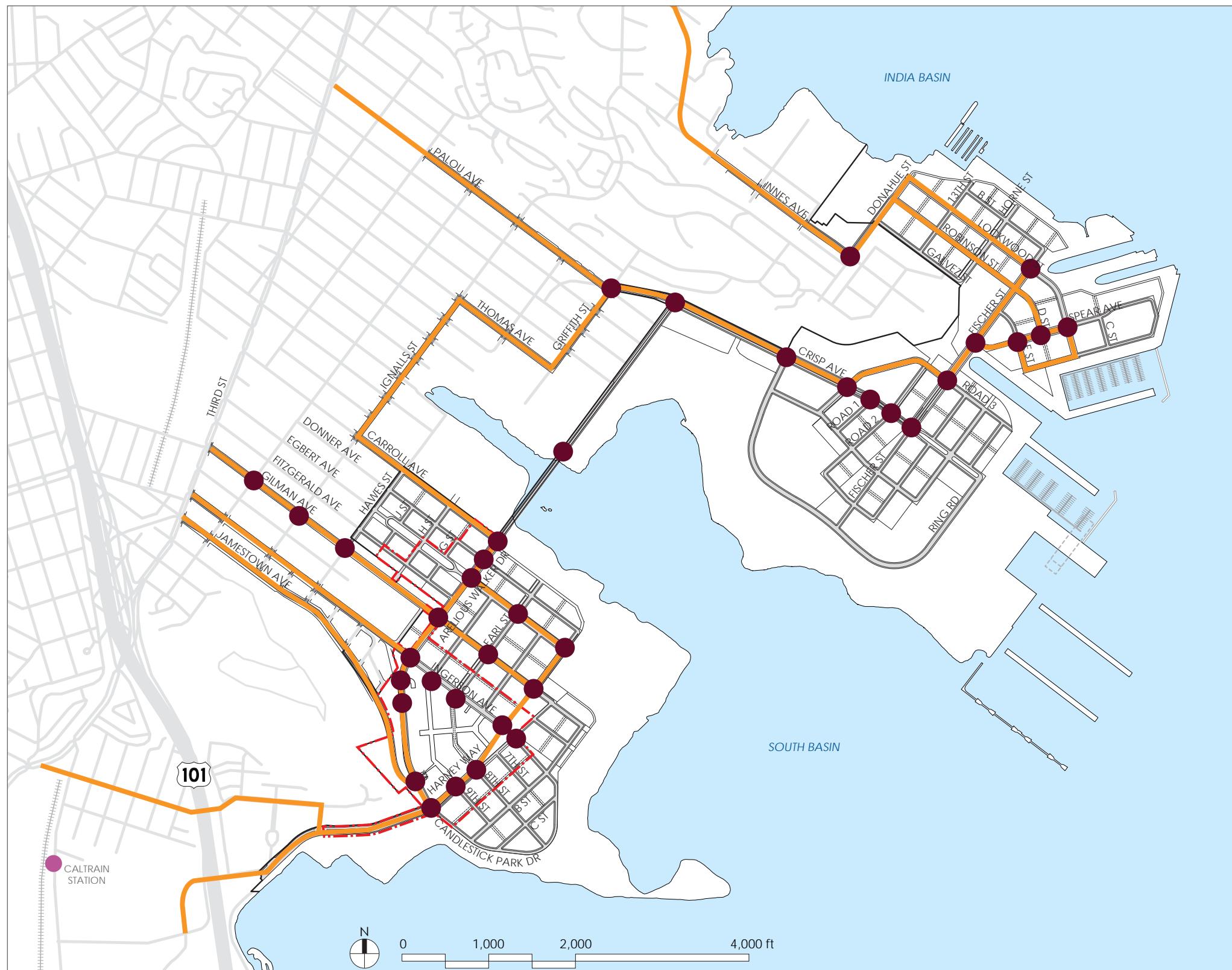


Figure 2.5 – Transportation Network



2. PROJECT OVERVIEW

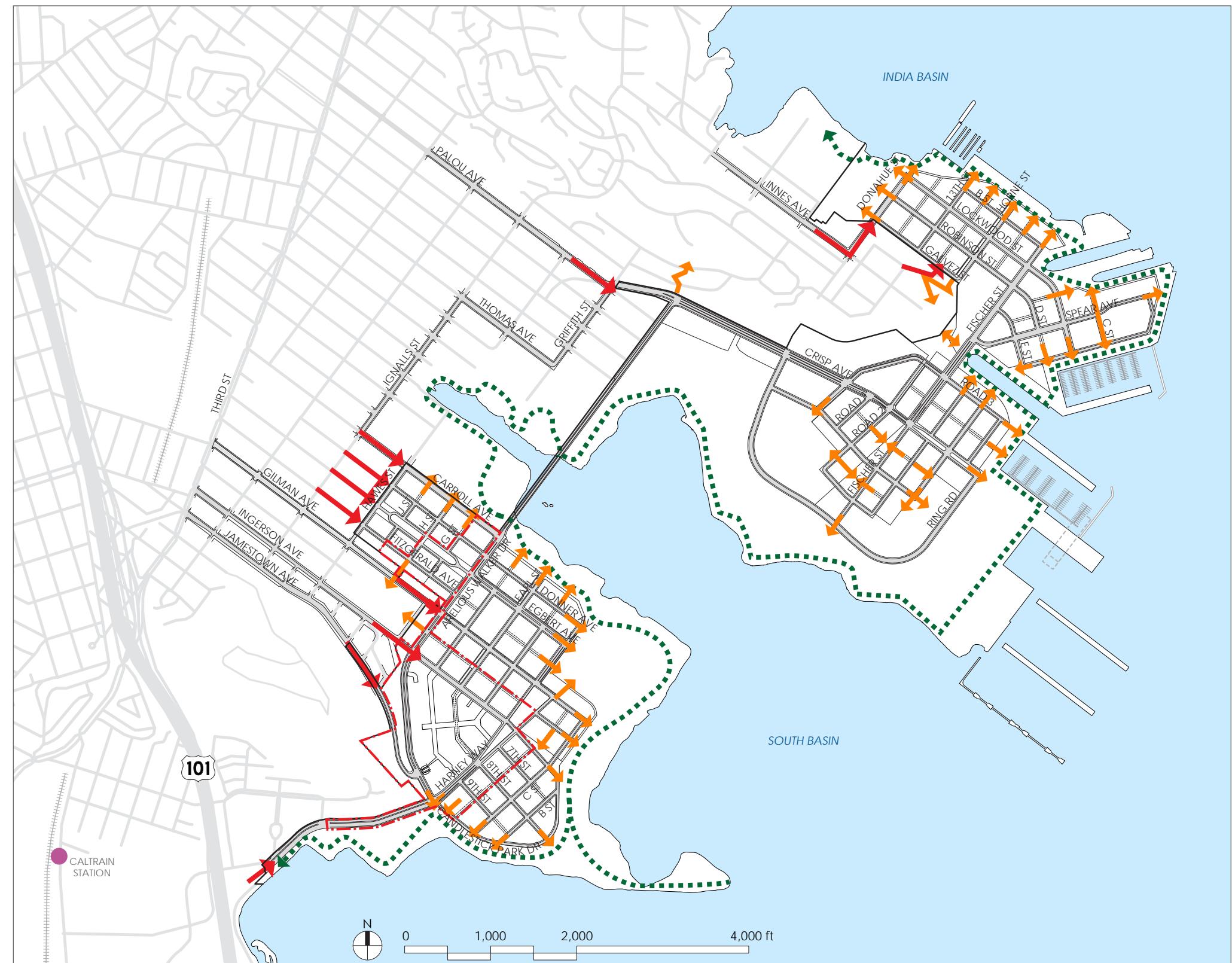
2.5 TRANSPORTATION

Pedestrian Network

The Project is designed to actively encourage the use of walking as a primary travel mode. Smaller blocks will decrease the average distance that pedestrians are required to walk, thereby increasing the likelihood that local trips will be made by foot, rather than by car. Further, the sidewalk system within the project site has been designed to provide generous 12-foot sidewalk zones throughout, increasing to 15-foot sidewalk zones near busier retail areas.

The new Yosemite Slough Bridge will be an important pedestrian connection between Candlestick Point and Hunters Point Shipyard.

Figure 2.6 – Pedestrian Circulation



2. PROJECT OVERVIEW

2.5 TRANSPORTATION

Bicycle Network

The Project will be served by an expanded network of bicycle routes. The street network is designed to connect the Project area to surrounding neighborhoods, and to increase bicycle access to new destinations and regional transit. The bicycle network within the Project includes Class I, Class II, and Class III facilities. Class I bikeways are bike paths with exclusive right-of-way for use by bicyclists or pedestrians. Class II bikeways are bike lanes striped within the paved areas of roadways and established for the preferential use of bicycles, while Class III bikeways are bike routes that allow bicycles to share travel lanes with vehicles. Overall, whether they are bicycle routes or not, all new neighborhoods streets are designed to emphasize slow auto speeds (15-25 mph) and encourage shared use of the street for autos and bicycles.

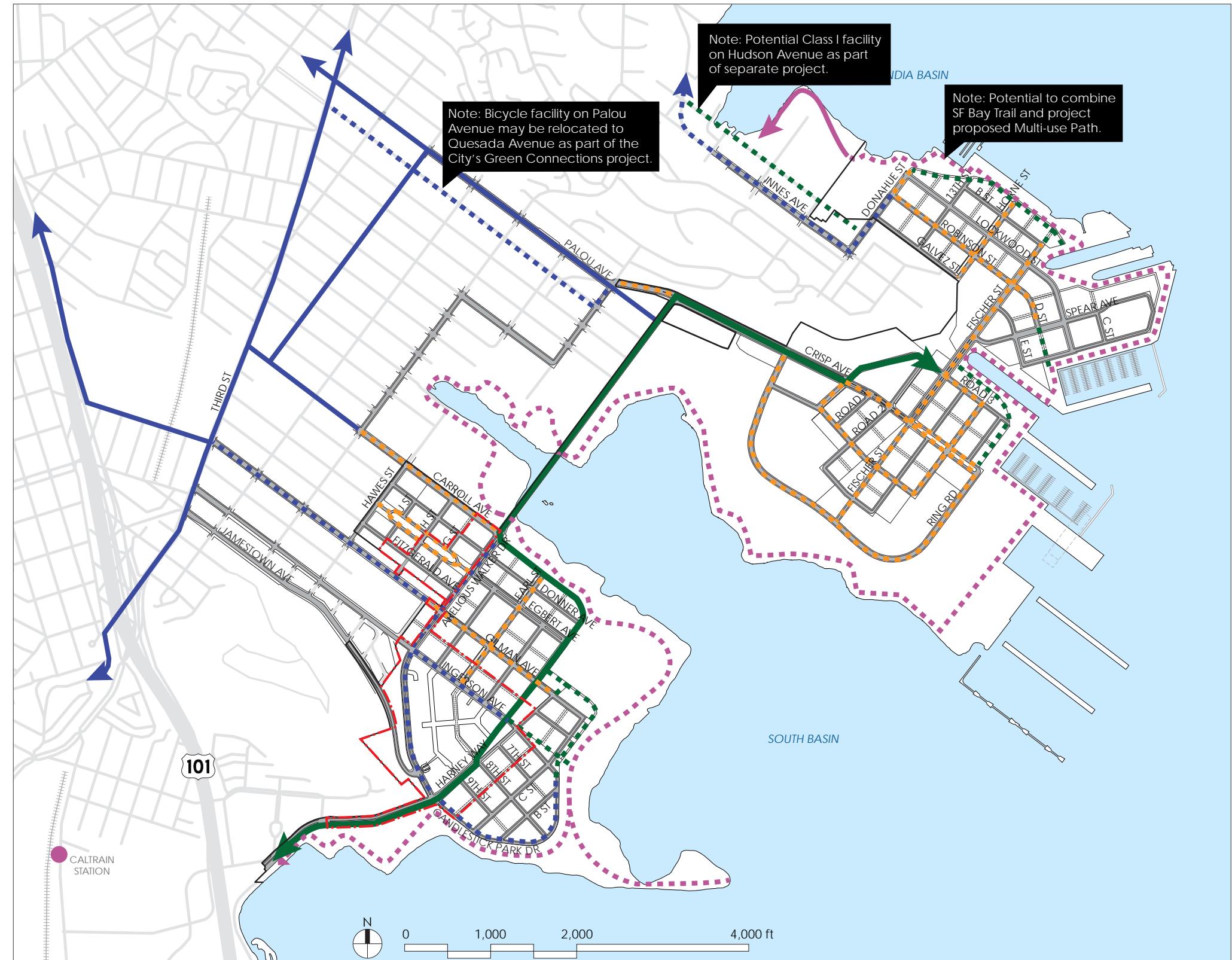
The Bay Trail / Blue Greenway forms a continuous off-street recreation route along the shoreline, connecting the Shipyard with Candlestick. Project improvements will provide a missing link in the Bay Trail / Blue Greenway. The trail will provide a mixed-pedestrian and Class I bicycle facility. Construction of the Bay Trail/Blue Greenway will be part of later phases of the development per the Schedule of Performance.

Bicycle racks are provided on streets, with high concentrations near retail, parks, and transit stops. New buildings will also provide bicycle parking at levels consistent with the approved Design for Development.

The proposed bicycle network is illustrated in Figure 2.7.

LEGEND	
Proposed Routes	Existing Route
Cycletrack	Bike Class III
Bike Class I	Bay Trail / Blue Greenway
Bike Class II	
Bike Class III	
Bay Trail / Blue Greenway	Major Phase 1 CP Boundary

Figure 2.7 – Bicycle Networks



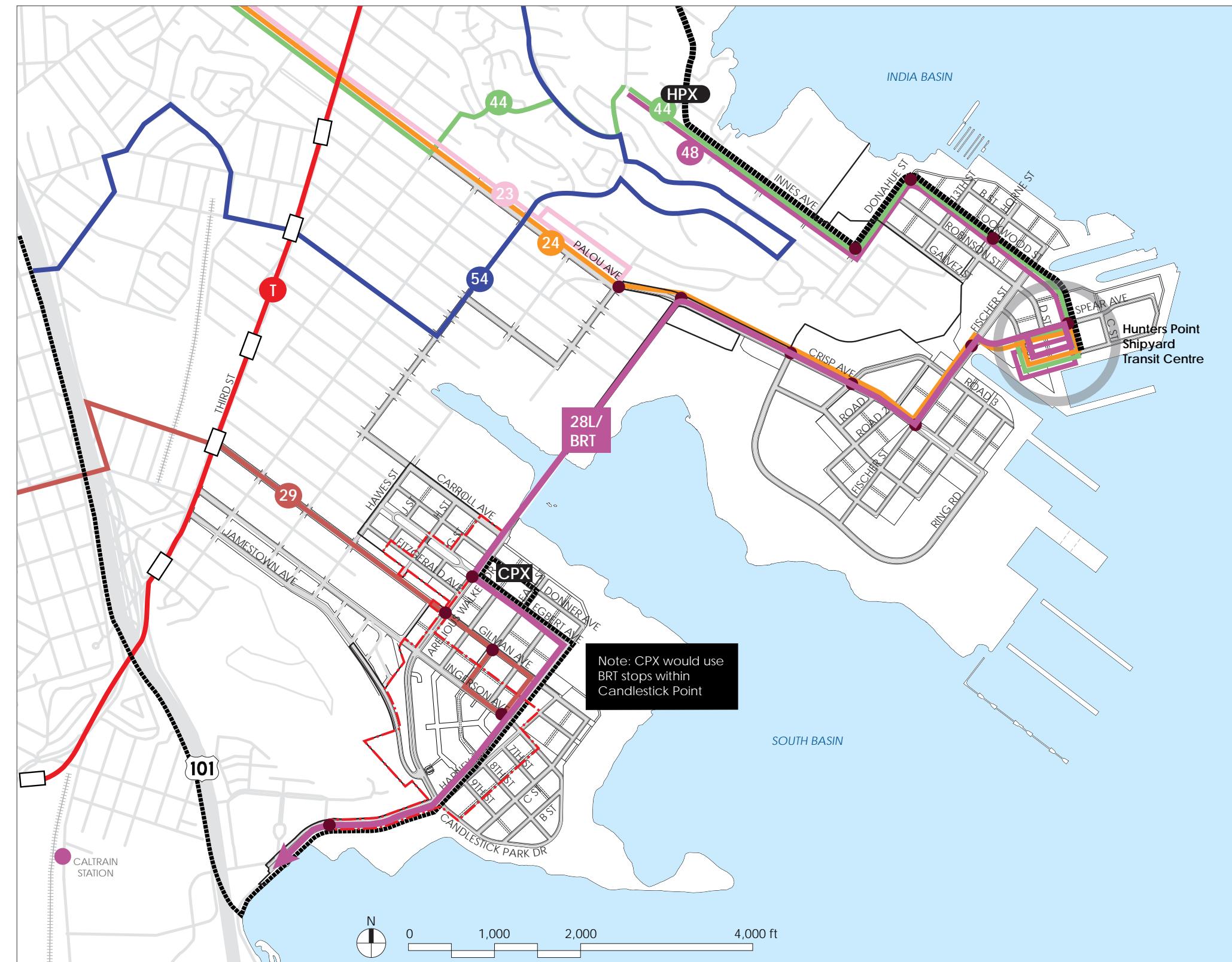
2. PROJECT OVERVIEW

2.5 TRANSPORTATION

Transit Network – MUNI Routes

The Project is intended to achieve a near doubling of the current mode share of transit in the vicinity of Candlestick Point and Hunters Point Shipyard. At full buildup the Project will include substantial improvements to the transit network, including route extensions and service frequency improvements. Improvements to transit service as a result of the Project will improve neighborhood, city, and regional transit access to the waterfront and the associated increase in frequencies will offer improved service to existing users along the routes serving the Candlestick Point.

Figure 2.8 – Transit Networks



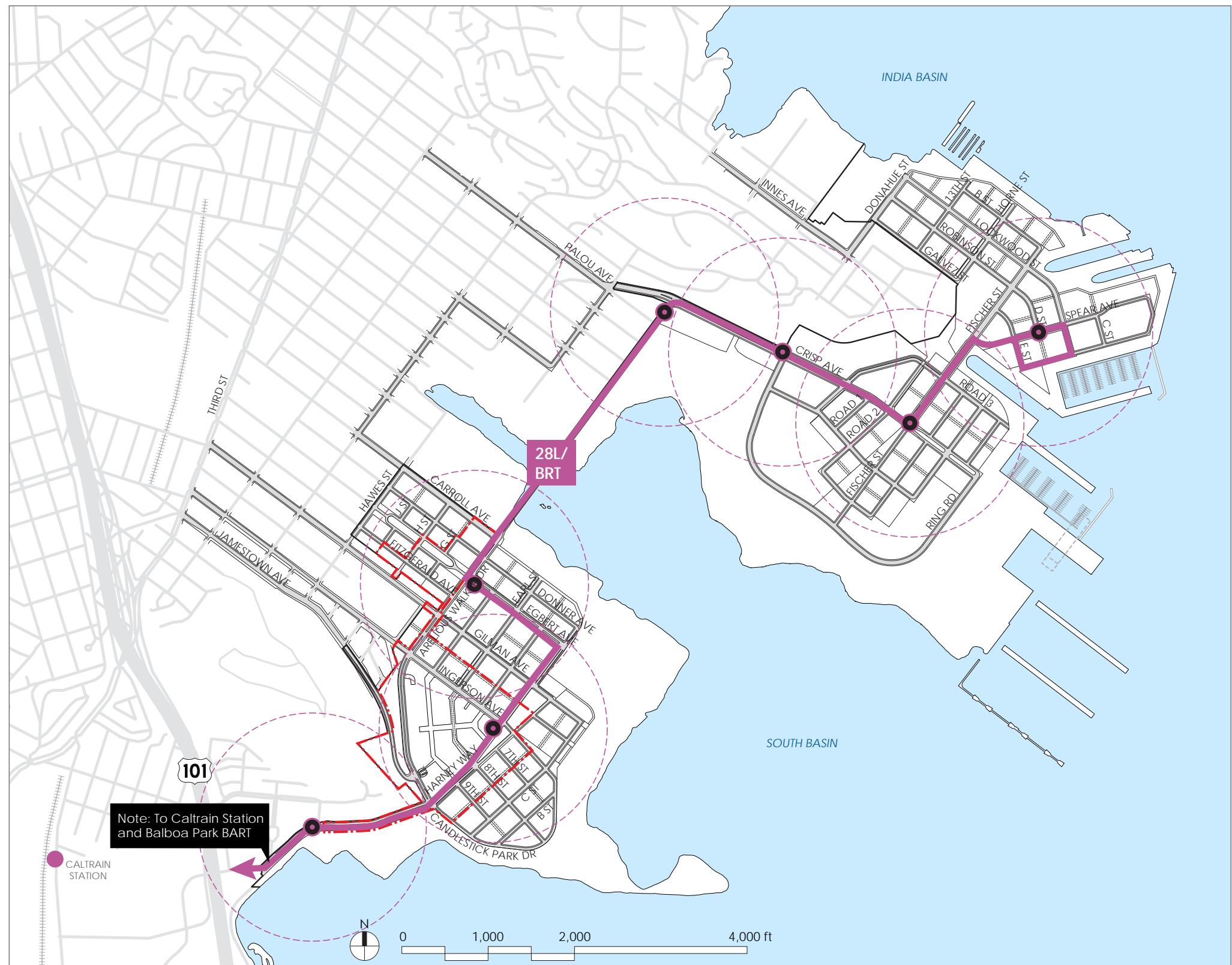
2. PROJECT OVERVIEW

2.5 TRANSPORTATION

Transit Network – BRT

A new Bus Rapid Transit (BRT) system that runs in a dedicated right of way, shown in Figure 2.9, will connect the Candlestick Point and Hunters Point Shipyard with regional transit connections to the T-Third Muni Metro, Caltrain Bayshore Station, and BART and Muni Metro at the Balboa Park Station. This BRT will run on a new bridge across the Yosemite Slough.

Figure 2.9 – Bus Rapid Transit



2. PROJECT OVERVIEW

2.6 PROJECT PHASING & SCHEDULE OF PERFORMANCE

Project Phasing

The Project is planned to be built in eight Major Phases over a span of approximately twenty years. Each Major Phase contains an area in the Candlestick Point and an area in the Hunters Point Shipyard, which are treated in separate Major Phase Applications. This results in eight Major Phase Applications, four in Candlestick Point and four in Hunters Point Shipyard. Each Major Phase is divided into Sub-Phases. The Project includes a total of thirty five Sub-Phases – eighteen in the Candlestick Point and seventeen in the Hunters Point Shipyard. A Sub-Phase Application will be submitted for each Sub-Phase within a Major Phase, and the approval of each Sub-Phase will follow (or be concurrent with) the approval of the applicable Major Phase Application. Multiple Sub-Phases may be included in one consolidated Application, per the DDA.

Schedule of Performance

The Schedule of Performance establishes dates for submittal of the Major Phase and Sub-Phase Applications, as well the Commencement and Completion of certain Associated Public Benefits and Infrastructure components of the Project. Listed within the Schedule of Performance are the various Open Space elements, off-site improvements, and other key improvements associated with each Major Phase and Sub-Phase. The entire Schedule of Performance reflecting Major Phase 1 CP, future Major Phases and their associated Sub-Phases can be found in Appendix B.

The original submission of this Major Phase Application fulfilled the obligation to submit a Major Phase Application by October 1, 2013. Since the original approval of the Major Phase 1 CP Application on January 7, 2014, several refinements have occurred that precipitated an update to the Major Phase Application. The Major Phase 1 CP Application is being resubmitted to the OCII Commission for approval of these refinements.

This update to the Major Phase Application includes refinements to the Project Phasing and Schedule of Performance. The affected areas include Jamestown Avenue, Harney Way, Wedge Park 2, and Gilman Avenue. These changes are reflected in Figure 2.10 and Appendix B.

Several Associated Public Benefits and Community Benefits are planned for Major Phase 1 CP, which are outlined in the Major Phase 1 CP Overview section and described in greater detail in subsequent sections.

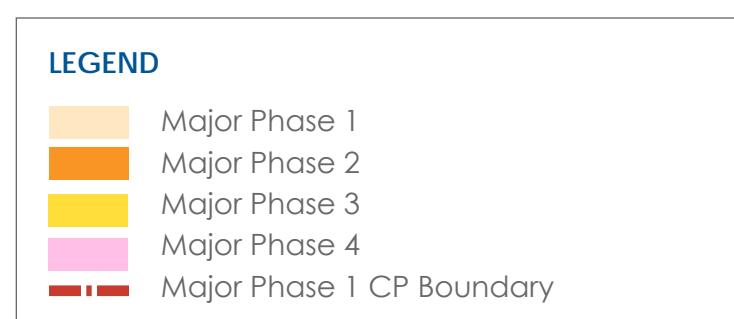
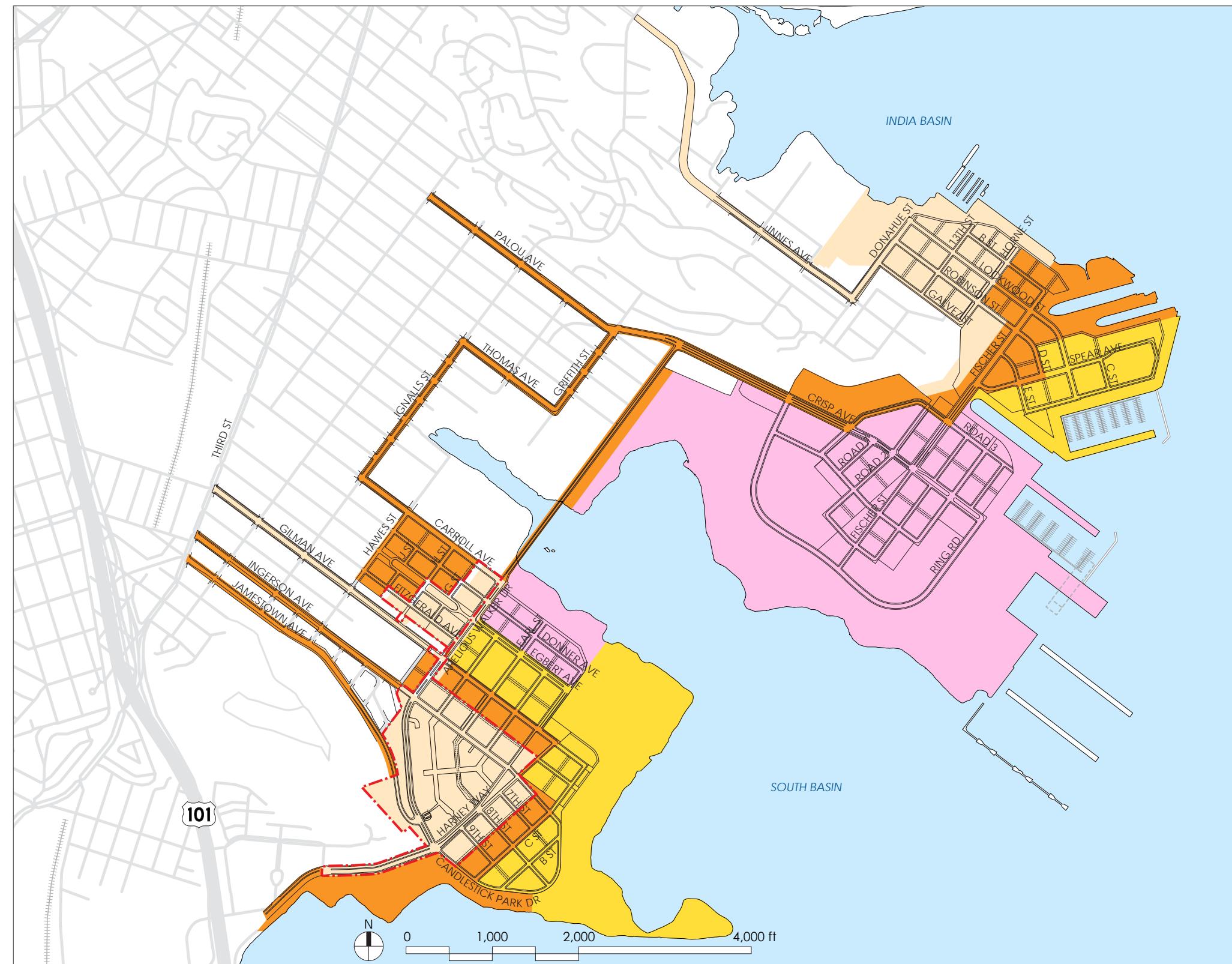


Figure 2.10 – Candlestick Point / Hunters Point Shipyard Phase 2 – Major Phases



The development of Major Phase 1 CP will be in compliance with the Schedule of Performance and all proposed changes to the Schedule of Performance.

Refinements to Project phasing and the Schedule of Performance affect the following locations:

Jamestown Avenue: Per the 2010 Candlestick Point Infrastructure Plan, the on-site Jamestown Avenue improvements are planned to extend from Arelious Walker Drive to the southeastern corner of the property line of 833-989 Jamestown Avenue (approximately Jamestown Avenue at Griffith Street). The proposed improvement plans once included re-grading Jamestown from Arelious Walker Drive to Griffith Street to achieve a more gradual road profile. However, as pointed out by the SFDPW Task Force, these proposed improvements would have created a grade difference between Jamestown Avenue and the property owned by Jamestown Realty. In this scenario, creating a driveway for the Jamestown Realty property would require grading into the site, resulting in a reduction of developable land.

The scope of the improvements was reduced to avoid impacting the Jamestown Realty property. The Developer still plans to re-grade the street to achieve the required road alignment. However, the re-grading no longer extends all the way to the intersection of Jamestown Avenue and Griffith Street. Rather, the improvements associated with Sub-Phases CP-02-03-04 limit the work on Jamestown to approximately 1,000 feet closer to Arelious Walker Drive along Jamestown Avenue. The boundary between Major Phases 1 and 2 shown in Figure 2.9 represents the limit of work for the Sub-Phases CP-02-03-04 Jamestown Avenue improvements. The SFDPW Task Force was actively involved in determining how to avoid impacting the Jamestown Realty property.

The portion of Jamestown Avenue that fronts the Jamestown Realty property will be resurfaced and restriped. This will avoid a grade difference between the street and the Jamestown Realty property.

The off-site Jamestown Avenue improvements, as defined by the Infrastructure Plan, includes resurfacing and restriping Jamestown Avenue between the southeastern property line of 833-989 Jamestown Avenue and the easternmost curb returns on Third Street. This work is to occur in association with Sub-Phase CP-09 in the second major phase of Candlestick,

per the Schedule of Performance.

Because the scope of work for the portion of Jamestown Avenue that fronts the Jamestown Realty Property is the same as the scope of the Jamestown Avenue off-site improvements, both pieces of work will be constructed at the same time: in the second Major Phase at Candlestick Point. The resurfacing of these portions of the roadway will be completed such that Jamestown Avenue will form one continuous street before and after the off-site improvements are completed.

Harney Way: At present, there is uncertainty regarding the timing of an extension of Geneva Avenue and the replacement of the US 101 / Harney Way interchange. These two projects have the potential to change BRT alignment on Harney. The Harney Way off-site improvements have been split into two phases to allow the ultimate BRT Route to be finalized. In the event that these projects do change BRT alignment on Harney Way, this change in phasing to the Harney Way improvements will prevent wasteful duplication of work.

Wedge Park 2: The development of Sub-Phases CP-02-03-04 requires the construction of a pump structure and Muni layover located in Wedge Park 2. To account for this, Wedge Park 2 is now split into two pieces-Wedge Park 2a, and Wedge Park 2b. Wedge Park 2a is included in Sub-Phase CP-03, and Wedge Park 2b is included in Sub-Phase CP-07.

Gilman Avenue: The first addendum to the EIR accelerated the off-site Gilman Avenue improvements from Sub-Phase CP-03 to Sub-Phase CP-02.

Sub-Phase CP-02: The Application Outside Date for Sub-Phase CP-02 is proposed to shift from December 2014 to December 2015 to match the Application Outside Date for Sub-Phases CP-03 and CP-04. Aspects of the planned development program in Sub-Phases CP-02, CP-03 and CP-04 changed; these changes did not conform to the existing D4D, Streetscape Plan, and Major Phase 1 CP Application. Consequently, these documents had to be amended; and these amendments had to be reviewed and approved by multiple City departments. This led to a delay in the Sub-Phase CP-02 application and development program. This shift of the Sub-Phase CP-02 date will also make the Schedule of Performance reflect the fact that a Sub-Phase Application was submitted concurrently for Sub-Phases CP-02, CP-03 and CP-04.

Refinements to the Candlestick Point Infrastructure Plan

Several refinements have been made to the Infrastructure Plan in response to the iterative design process and conversations with various City agencies.

At the direction of SFMTA, fewer BRT stops are proposed in this Major Phase Application than are referenced in the Infrastructure Plan.

The utility systems detailed in the Infrastructure Plan have been refined through the design process. These refinements are to be expected as designs move from a schematic level to a more detailed level (construction drawings).

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3. COMMUNITY BENEFITS

- 3.1 COMMUNITY BENEFITS PLAN
- 3.2 CORE COMMUNITY BENEFITS AGREEMENT

3. COMMUNITY BENEFITS

3.1 COMMUNITY BENEFITS PLAN

The Project includes a robust Community Benefits Plan (Exhibit G of the DDA) that is designed to ensure that the social goals and objectives of the Project are delivered to the Bayview-Hunters Point neighborhood and the City at large. The Community Benefits Plan outlines a series of programs and funding opportunities that are targeted at improving the quality of life in five key areas: Education; Community Health and Wellness; Community Facilities; Business Development and Community Asset Building; and Community Funding. Table 3.1 tracks the Developer's compliance with the Community Benefits Plan.

Education

As part of the Project, contributions totaling \$3,500,000 will be made to the Lennar Bayview Scholarship Fund to assist residents of the Bayview-Hunters Point Community with tuition and expenses associated with higher learning. A part of this scholarship fund will also be used to fund the Will Bass Memorial Educational Travel Scholarship which provides funding for educational travel to Africa or Asia. As part of the approval of the Major Phase Application for Major Phase 1 CP in January 2014, Developer made an initial contribution of \$500,000 to the Scholarship Fund. Additional contributions of \$300,000 will be made 60 days after the Developer is credited for each thousandth residential unit (i.e., 1,000th, 2,000th, etc.).

Summary of Community Benefits for Major Phase 1 CP

Development of Major Phase 1 CP is expected to include approximately 2,214 units (1,275 market-rate units) of housing. Based on this estimate, the following Community Benefits are anticipated to be provided:

Program	Payment Milestone	Total Funding	Funding Associated w/ Major Phase 1-CP Development
Scholarship Fund	Major Phase 1 CP Approval (2014), 1,000th, & 2,000th Unit Land Transfer	\$ 3.5 million	\$ 1,100,000 [\$ 500,000 paid to date]
Education Improvement Fund	Major Phase 1 CP Approval (2014), 1,000th, & 2,000th Unit Land Transfer	\$ 10 million	\$ 2,400,000 [\$ 500,000 paid to date]
Wellness Contribution	Major Phase 1 CP Approval (2014) & Wellness Center Development Milestones	\$ 2 million	\$ 100,000 [\$ 100,000 paid to date]
Southeast Health Center	Major Phase 1 CP Approval (2014)	\$ 250,000	\$ 250,000 [\$ 250,000 paid to date]
Construction Assistance Fund	Annual contribution for 10 years following Major Phase 1 CP Approval (2014)	\$ 2.5 million	\$ 700,000 [\$ 265,000 paid to date]
Credit Support Contribution	Major Phase 1 CP Approval (2014)	\$1 million	\$ 250,000 [\$250,000 paid to date]

In addition to the Scholarship Fund, contributions totaling \$10,000,000 will also be made to the Lennar Bayview Education Improvement Fund. These funds can be used to support enhancements to educational facilities within the Bayview-Hunters Point Community. The initial contribution of \$500,000 was made upon approval of the Major Phase Application for Major Phase 1 CP in January 2014. Subsequent payments of \$950,000 are due after the Developer is credited for each thousandth residential unit (i.e., 1,000th, 2,000th, etc.).

Community Health and Wellness

In an effort to improve access to healthcare for the Bayview-Hunters Point residents, the Project includes a \$2,000,000 Wellness Contribution to be used for the expansion of the Southeast Health Center, or in the event that the funds are not needed or not exhausted for the Southeast Health Center, for the creation or expansion of the Center for Youth Wellness. The initial contribution of \$350,000 was provided upon the approval of the Major Phase CP Application for Major Phase 1 CP in January 2014. Those funds will be used for the predevelopment expenses associated with providing the Wellness Facilities.

The remaining funds will be provided when predevelopment and construction milestones of the Southeast Health Center are achieved.

Community Facilities

The Community Benefits Plan provides for an allocation of Community Facilities Space and Lots. The amount of Community Facilities Space is based on the amount of retail space developed within the Project, but will not exceed 65,000 square feet in total. 35,000 square feet of Community Facilities Space has been allocated within Major Phase 1 CP. Some specific uses that are contemplated for this space are:

- **International African Marketplace** – An indoor African Marketplace that will serve as an African-themed, festive setting for the display and sale of arts, crafts, sculptures, fabrics and clothing. In addition, a space will be provided within the park program to serve as an occasional outdoor venue for a similarly themed marketplace.
- **Library Reading Rooms** – Library reading rooms and automated book pick-up and drop-off locations will be provided.
- **Candlestick Point State Recreation Area** – Approximately 3,000 square feet for a welcoming or information center for the CPSRA, not in Major Phase 1 CP.

The Project includes approximately 4.8 acres of land identified as Community Facility Lots. The use of these lots is left to the discretion of OCII, but they should be used to enhance the quality of life of residents of the Bayview Hunters Point community. This Major Phase Application includes one Community Facilities Lot at the corner of Arelious Walker Drive and Ingerson Avenue - CPN 1a.

3. COMMUNITY BENEFITS

3.1 COMMUNITY BENEFITS PLAN

The Community Benefits Plan includes opportunities to expand the involvement of the local business community in the financial success of the Project. The three principal programs that provide these opportunities are:

- **Community Builder Program** – During the build out of the Project, five hundred (500) units will be made available for development by or with the assistance of Community Builders selected from a pool of qualified builders. OCII will make every effort to involve community partners in the development of not less than three hundred (300) units of the affordable housing developed by OCII.
- **Contractor Assistance** – To further the opportunities for local community involvement in the development of the Project, the Community Benefit Plan includes additional financing for construction assistance activities including: (i) technical support to contractors seeking work on the Project with respect to the public bidding process or other public benefits; (ii) workshops to address issues relevant to the construction industry (e.g., worksite safety, accounting, legal, etc.); and (iii) a trucking program for operators residing or based in the Bayview-Hunters Point community. The total funding for these activities is \$2,500,000. The initial payment of the \$250,000 per year obligation became due with the commencement of infrastructure on Sub-Phase CP-01 and payments will continue on an annual basis during which construction continues on the Project. The Developer has spent \$265,000 over two years, a shortfall of \$235,000. Any shortfall at the end of the 10 year term will be paid to the Community Benefits Fund, or the Developer will continue the Construction Assistance Program (CAP) beyond the 10 year term. The program benefits local contractors, with priority given to those from District 10. In addition to construction assistance, the Developer will pay \$1,000,000 to the OCII for a surety bond and credit support program connected to the Project. The initial contribution to this program of \$250,000 was paid after approval of the Major Phase 1 CP Application. The remaining \$750,000 is due in three equal payments upon the approval of the next three Major Phase applications.
- **Community Real Estate Broker Program** – For each residential development constructed within the Project by the Developer or an affiliate of the Developer, a good faith effort will be made to assist the local brokerage community to secure the sale of the units. This will include: (i) first opportunities to preview and show units to their clients; (ii) invitations to marketing events for the units; (iii) marketing materials for distribution to clients; (iv) opportunities to participate in homebuyer workshops for the units; and (v) an additional 1% incentive for Community Brokers on the Market Rate Sales, on top of the 2.5% commission for all properties, both Below Market Rate and Market Rate.

Community Benefits Funding

As part of the Community Benefits Plan, OCII, in partnership with the Legacy Foundation for Bayview Hunters Point, will establish a Community Benefits Fund that can be used for a wide range of programs within the Bayview Hunters Point community – including social services, affordable housing, education, the arts, and public safety. As Market Rate Units within the Project are sold one-half of one percent (0.5%) of the initial sale price of each unit will go to the Community Benefits Fund at the close of escrow.

Table 3.1 – Major Phase 1 CP Community Benefits Compliance

CHHPS2 Community Benefits Plan Element	Summary of Requirement	Applicable to Major Phase 1 CP	Compliance Status	Note
Construction Assistance Program (CAP)	Developer will provide \$2.5 million (\$250,000/year for up to 10 years).	Yes	Compliant	To date, the Phase 2 CAP has been operating for almost two years, during which the Developer has expended approximately \$265,000, resulting in a shortfall of approximately \$235,000. In accordance with the Phase 2 CBA, the Developer will either provide the resulting shortfall to the Legacy Foundation, or continue the CAP beyond the 10 year term until the totality of the \$2.5 million obligation is satisfied.
Community Builder Program	Developer will make available 500 units for development by Community Builders.	No	N/A	Given that the residential units proposed within Major Phase 1 CP are all associated with either the early phases of development at Alice Griffith or the mixed-use development of the Candlestick Point regional retail center, it is the Applicant's request that these residential units be excluded from the pool of Community Builder Units for this Major Phase Application. This will require future phases of the Project to include a higher portion of Community Builder Lots to achieve the Project-wide goal of 500 units.
Interim African Marketplace	Developer will make space available for International African Marketplace.	Yes	Compliant	To be constructed within Sub-Phase CP-04.
Community Facilities Space	Developer (or Vertical Developers, as applicable) shall make available to the Agency 7.5% of the aggregate retail space in the Project, but not to exceed a maximum of 65,000 gross square feet.	Yes	Compliant	Community Facilities Space strategy created in collaboration with OCII which allocates 35,000 square feet within Major Phase 1 CP.
Community Facilities Lots	Developer (or Vertical Developers, as applicable) shall complete the Infrastructure for the approximately four and eight tenths (4.8) acres of land identified on the Development Plan as "Community Facilities Lots".	Yes	Compliant	Community Facilities Lots strategy created in collaboration with OCII for parcel CPN1a which is within Major Phase 1 CP.
Community Real Estate Broker Program	For each Residential Project in which Vertical Developer is Developer or its Affiliate, Vertical Developer shall use good faith efforts to provide licensed brokers and salespersons with offices in BVHP with the first opportunity to preview and show units, invitations to marketing events, marketing materials, and opportunities at homebuyer workshops.	Yes	Compliant	No units available to date for this program.
Scholarship Fund	Developer shall contribute (or cause the contribution of) Three Million Five Hundred Thousand Dollars to the Lennar Bayview Scholarship Fund.	Yes	Compliant	Developer has made first \$500,000 payment in accordance with the Phase 2 DDA. The funds are being held in the Community Benefits Fund.
Education Improvement Fund	Developer shall contribute (or cause the contribution of) Ten Million Dollars to the Lennar Bayview Education Improvement Fund to be used to support education enhancements within Bayview Hunters Point.	Yes	Compliant	Developer has made first \$500,000 payment in accordance with the Phase 2 DDA. The funds are being held by OCII.
Community Health and Wellness Contributions	Developer shall contribute (or cause the contribution of) Two Million Dollars to be used to expand, develop, finance and/or create a center focused on the health and well being of children, youth and their families, which center may include the Southeast Health Center.	Yes	Compliant	Developer has made first \$100,000 Wellness Contribution and the \$250,000 contribution for the Southeast Health Center in accordance with the Phase 2 DDA. The funds have been transferred to the Department of Public Health.
Community Benefits Fund	OCII shall establish and maintain a the Community Benefits Fund to be funded under this Article 6 and as otherwise specified in the DDA (including this Community Benefits Plan). 100% of the Community Benefits Fund shall be reinvested by OCII, in consultation with the Legacy Foundation, in the Project Site and Bayview Hunters Point.	Yes	Compliant	The initial home closings for Phase 2 and associated payments to the Community Benefits Fund are currently anticipated in late 2018.

Note: This table summarizes requirements of the Community Benefits Plan, but it does not describe them fully or comprehensively. Full details can be found in the Community Benefits Plan (Exhibit G of the Disposition and Development Agreement).

4. SUSTAINABILITY & ENVIRONMENT

4.1 SUSTAINABILITY

4.2 ENVIRONMENTAL MITIGATIONS



4. SUSTAINABILITY & ENVIRONMENT

4.1 SUSTAINABILITY

The Sustainability Plan, a part of the DDA, provides seven "sustainability focus areas" that define sustainability goals for the Project. Below we list the sustainability focus areas, and discuss how each of them will be addressed in Major Phase 1 CP.

Economic Vitality and Affordability

Enhance the competitiveness of the region and restore the vitality of the Bayview by fostering a vibrant local economy and supporting a mixed-income community.

This sustainability goal is being realized in Major Phase 1 CP through the mixed-use, mixed-income development program. Housing will be provided for households with wide-range of incomes, and many employment opportunities will be provided at CP Center, a regional commercial destination that will be built in Sub-Phase CP-02.

Community Identity and Cohesion

Create a strong sense of community by integrating the new neighborhood with the rich culture and diverse history of the existing neighborhood.

The development in Major Phase 1 CP will connect both physically and culturally to the existing neighborhood. The existing community will immediately be integrated into the new development, as the first housing to be built will be replacement units for the current residents of the Alice Griffith Public Housing. Physical cohesion with the broader neighborhood will be created by extending the existing street grid through the Alice Griffith neighborhood and into the heart of Candlestick Point. Historical and cultural stories of the area will be told through signage and public art programs.

Public Well-Being and Quality of Life

Provide a healthy and safe neighborhood with sufficient community facilities, parks, essential services and public spaces to engender a high quality of life for residents of all ages and abilities.

Parks and open space are an important component of Major Phase 1 CP. As with all future Major Phases, parks and open space are scheduled to be built ready for use as neighborhoods are occupied (Mitigation Measure RE-2). As described in the Community Benefits section, the Community Facilities Lot in Major Phase 1 CP will also enhance the quality of life for residents of the community.

Another component of public well-being is resilience in the face of climate change. The project site will be graded so that finished floor elevations are 3.5 feet above the Base Flood Elevation (BFE), and streets and pads are 3 feet above BFE to allow for potential sea level rise (Mitigation Measure HY-12a.1).



4. SUSTAINABILITY & ENVIRONMENT

4.1 SUSTAINABILITY

Accessibility and Transportation

Significantly improve accessibility to the site and reduce traffic impacts on the surrounding area; promote walking and cycling as the primary modes of transportation within the development.

In keeping with the City's Transit First, Complete Streets, and Better Streets policies, the street system in Major Phase 1 CP is designed to prioritize walking, bicycling and transit use. Transit service will be extended to the site to ensure that project is well-served by transit from the outset. This includes extension of the 29 Sunset to the retail center, with frequency of service doubled from every 10 minutes to every 5 minutes during peak periods. Additionally, although the BRT system may not be implemented until subsequent Major Phases, the 56 Rutland may be extended to serve the retail center and augment the 29 Sunset service, by providing a direct connection to the T Third light rail, Bayshore Caltrain Station, and the 9 San Bruno bus line.

Resource Efficiency

Implement a whole-systems approach to energy conservation efficiency and sustainable supply that minimizes the need for fossil fuels.

Many steps are being taken in Major Phase 1 CP to reduce demand for natural resources including water, power, and building materials. Potable water demand will be reduced with the installation of the recycled water (RCW) system. The RCW system, which is new infrastructure for the City, will reuse grey water primarily for irrigation and toilet flushing. The use of climate appropriate vegetation will further reduce the demand for water to irrigate landscaping.

Energy conservation will be accomplished in Major Phase 1 CP through the sustainable design of infrastructure and buildings. All new buildings will include ENERGY STAR appliances (Mitigation Measure MM GC-3) and will be designed to exceed Title 24 (2008) energy standards by at least 15%. Streetlights will be efficient light emitting diode ("LED") fixtures (Mitigation Measure GC-4).

Material waste will be reduced during the construction and operations of Major Phase 1 CP. A Site Waste Management Plan (SWMP) will be produced to describe how the Project shall minimize waste generation beyond the methods required by existing City regulatory policies, with the goal of achieving a diversion rate of at least 72 percent (Mitigation Measure UT-7a).

Environment and Habitat

Protect and, wherever possible, enhance parks, natural habitats, soils, water bodies, air and climate.

Environmental protections are widespread in Major Phase 1 CP. Extensive storm water treatment facilities will filter runoff from the Project Site prior to discharging to the Bay. Air quality will be closely monitored during construction to maintain healthy levels of emissions and dust.

Plants and animals will be protected by providing new and improved habitats and through monitoring of existing habitats. For example, nesting birds and burrowing owls will be monitored as required by Mitigation Measures BI-6a , BI-6a.1 and BI-6b. Significant trees will also be preserved and/or replaced, as required by Mitigation Measure BI-14a.

Utilize Advanced Information and Communications Technologies (ICT)

Integrate Information and Communications Technologies (ICT) such as smart grid and cellular broadband infrastructure into the development to allow residents to better manage energy and water resources, bolster local economic activity, and improve access to real time information, and facilitate community communications and activity.

High speed wireless internet access will be provided within the common areas of Major Phase 1 CP, which will encourage communication, commerce, and access to online resources.

4. SUSTAINABILITY & ENVIRONMENT

4.2 ENVIRONMENTAL MITIGATIONS

The Final Environmental Impact Report (Final EIR) for the Candlestick Point Hunters Point Shipyard Phase 2 project, certified in June 2010, was prepared in conformance with the requirements of the California Environmental Quality Act (CEQA). The purpose of the EIR was to identify the significant environmental impacts of the Project, to identify alternatives to the Project, and to indicate the manner in which those significant effects could be mitigated or avoided.

The EIR evaluates the Project's environmental effects at a project level and examines all phases of the Project, including planning, construction, and operation, as well as the direct, indirect, and cumulative impacts that might result. It is anticipated that each discretionary approval related to the implementation of the Project will rely on this EIR and will not require preparation of subsequent environmental documentation, unless otherwise required by CEQA.

Mitigation Monitoring and Reporting Program

The Mitigation Monitoring and Reporting Program (MMRP) has been established to provide for the monitoring of mitigation measures required of the Project, as set forth in the Final EIR. Prior to the issuance of building permits, while detailed development plans are being prepared for approval by OCII and/or City staff, OCII and/or City staff will be responsible for ensuring compliance with mitigation monitoring applicable to the project construction, development, and design phases.

The status of all applicable mitigation measures is included in APPENDIX A.





5. MAJOR PHASE 1 CP OVERVIEW

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- 5.1 MAJOR PHASE SUMMARY
 - 5.2 PHASING & SCHEDULE OF PERFORMANCE
 - 5.3 DEVELOPMENT BLOCKS



5. MAJOR PHASE 1 CP OVERVIEW

5.1 MAJOR PHASE SUMMARY

The first Major Phase at Candlestick Point is comprised of sixteen blocks of new development, more than 2,000 homes, 1.1 million square feet of mixed commercial uses, and up to 50,000 square feet of community uses. The development will be dominated by two significant projects: the redevelopment of a major portion of the San Francisco Housing Authority (SFHA) Alice Griffith public housing site and the construction of CP Center, a mixed-use destination featuring housing, retail and entertainment.

The Alice Griffith project will replace the existing 256 public housing units currently on the site without displacing residents, and provide additional affordable rental housing units. CP Center will include regional retail, hotel, entertainment, and residential uses on the site where Candlestick Park Stadium once stood.

A series of infrastructure improvements will be support and this development, including new streets, utilities, and open spaces. Public spaces will serve neighbors and visitors alike with a mix of gathering places for a range of active and passive pursuits. The new neighborhoods in Major Phase 1 CP will be easily accessible, with roadway improvements to serve automobiles, bicycles, pedestrians, and public transit.

Major Phase 1 CP is scheduled to be built in five Sub-Phases over a span of approximately seven years. Infrastructure construction began on Sub-Phase CP-01 in late 2014.

Subsequent to the approval and execution of the DDA and the initial approval of this Major Phase Application in January 2014, several refinements were made to the Project as part of the preparation of the CP-02-03-04 Sub-Phase Application. These refinements were made in consultation with City staff, OCII staff, and the Developer's consultant team and are described in Section 1.1. A complete description of the revisions can be found in an OCII staff report to the OCII Commission in support of this Application.

The Major Phase 1 CP Application has been updated to reflect the project refinements made in the preparation of the CP-02-03-04 Sub-Phase Application. The updated Major Phase 1 CP Application is brought before the OCII Commission for Approval of these refinements. Approval of the updated Major Phase 1 CP Application will enable the OCII Director to Approve the CP-02-03-04 Sub-Phase Application.



5. MAJOR PHASE 1 CP OVERVIEW

5.2 PHASING & SCHEDULE OF PERFORMANCE WITHIN MAJOR PHASE 1 CP

Major Phase 1 CP Phasing

Major Phase 1 CP is comprised of five Sub-Phases, which are scheduled to be built over the next 7 years. The Sub-Phases are delineated in Figure 5.1 and the program of land uses for each can be found in Table 6.1.

The Development of Major Phase 1 will commence with Sub-Phase CP-01, to be followed by Sub-Phase CP-02. Sub-Phases CP-03 and CP-04 will follow Sub-Phase CP-02, and the development of Sub-Phase CP-05 will conclude Major Phase 1. Most of the Candlestick Park site that is not under construction will be used for temporary parking and staging areas. Some interim infrastructure improvements will be necessary, which are detailed in subsequent Sub-Phase Applications and permit sets. See Appendix F for preliminary construction phasing information.

Schedule of Performance

Below are the public improvements associated with Major Phase 1 as required by the Schedule of Performance. These improvements are outlined in the Project Overview section and described in greater detail in subsequent sections (see Appendix B).

Open Space Lots:

- Bayview Hillside Open Space
- Jamestown Walker Slope
- Wedge Park 1
- Wedge Park 2a
- Alice Griffith Neighborhood Park 1

More information about the open spaces planned for Major Phase 1 CP can be found in the Parks and Open Space section of this application.

Off-Site Street Improvements:

- Gilman Avenue
- Harney Way 1

More information about the off-site street improvements planned for Major Phase 1 CP can be found in the Transportation section of this application.

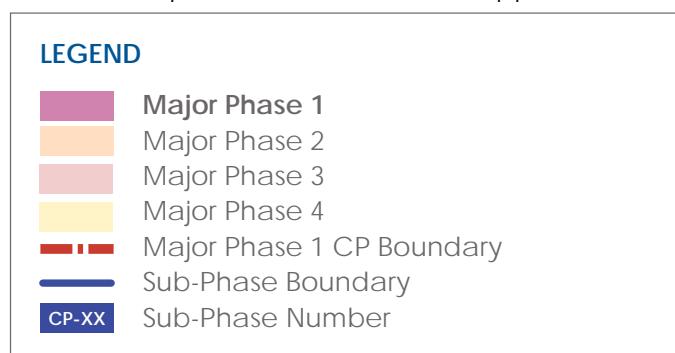
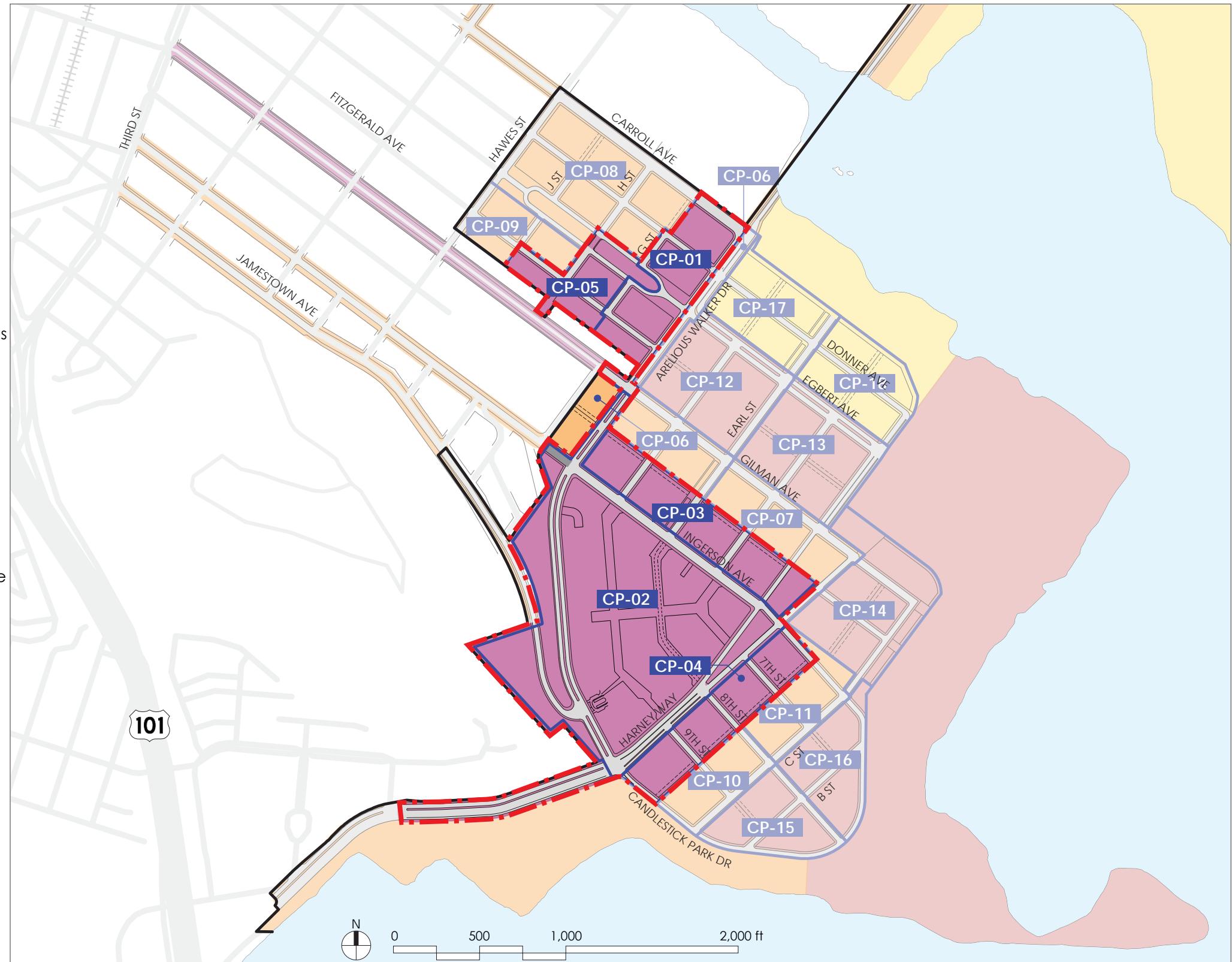


Figure 5.1 – Candlestick Point – Major Phases and Sub-Phases



5. MAJOR PHASE 1 CP OVERVIEW

5.3 DEVELOPMENT BLOCKS

Table 5.1 – Development Block Areas¹

NEIGHBORHOOD	BLOCK NUMBER	SUB-PHASE	AREA (SQ FEET)
Alice Griffith	1	CP-01	62,555.5
	2	CP-01	53,730
	4	CP-01	53,431.5
	5	CP-01	35,490
	8	CP-05	31,897.32
	9	CP-05	44,499.4
	14	CP-05	34,834.8
Candlestick Point North	1a	CP-03	63,021.14
	2a	CP-03	57,190.4
	10a	CP-03	57,190.4
	11a	CP-03	63,713.68
Candlestick Point Center	1	CP-02	971,364
Candlestick Point South	6a	CP-04	50,153.3
	8a	CP-04	52,722.4
	9a	CP-04	54,243
	11a ²	CP-04	66,542
TOTAL			1,752,578.84

¹These block dimensions represent developable area, they exclude the area of the mid-block break.

²Block CPS 11a is irregularly shaped, it is not a rectangle. The area shown in Table 5.1 represents the true size of the block.

Table 5.2 – Developable Sub-Phase Areas

SUB-PHASE	TOTAL AREA (SQ FEET)
CP-01	205,207
CP-02	971,364
CP-03	241,115.62
CP-04	223,660.70
CP-05	111,231.52
TOTAL	1,752,578.84

Figure 5.2 – Development Block Dimensions



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6. LAND USE & MASSING

- 6.1 LAND USE SUMMARY
- 6.2 HOUSING
- 6.3 SITE SECTIONS
- 6.4 BUILDING HEIGHTS, BULK & MASSING
- 6.5 STREET WALL
- 6.6 BUILDING GROUND FLOOR TREATMENT



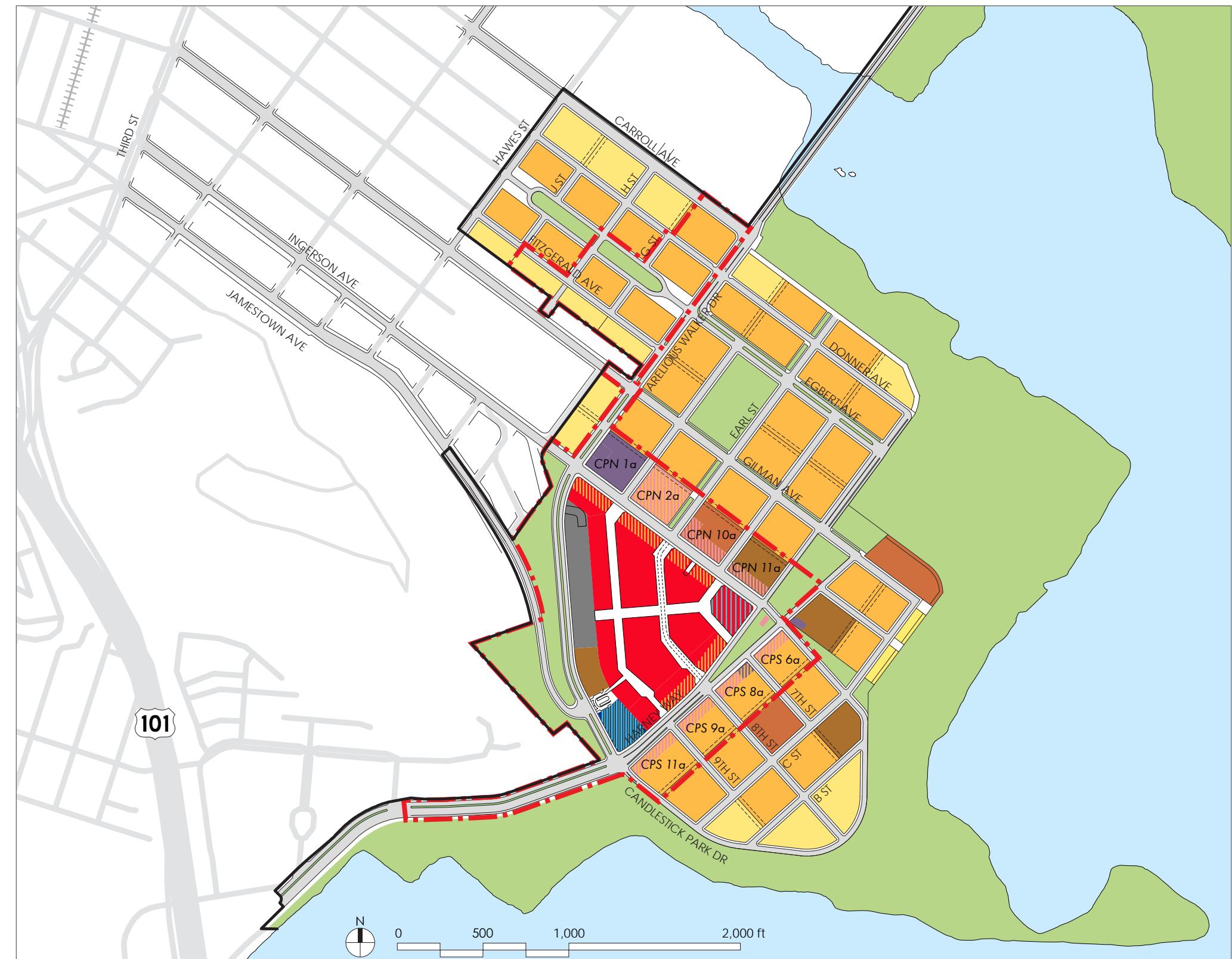
6. LAND USE & MASSING

6.1 LAND USE SUMMARY

The first Major Phase on Candlestick Point (1 CP) includes residential development in the Alice Griffith neighborhood, and a mix of residences, neighborhood retail, regional retail, office space, and entertainment uses at the CP Center. The CP Center site is also approved for a hotel. A mix of local serving retail, housing and offices is proposed along Ingerson Avenue and Harney Way. In addition, a Community Facilities block is sited at the corner of Arelius Walker and Ingerson. The Wedge Plaza, Wedge Park 2a, and Alice Griffith Neighborhood Park 1 will be newly constructed, and Bayview Hillside Open Space and James town Walker Slope will be improved. Streetscape improvements are proposed along Gilman Avenue and Harney Way. A map depicting Major Phase 1 CP is shown in Figure 6.1.

It should be understood that the land uses shown in this Major Phase application are conceptual and will be refined in response to the design process and market conditions. Sub-Phase applications for the Sub-Phases within Major Phase 1 will provide revised and more detailed land use plans.

Figure 6.1 – Major Phase 1 CP Land Use



6. LAND USE & MASSING

6.1 LAND USE SUMMARY

Table 6.1 – Land Use by Sub-Phase

LAND USE	SUB PHASE CP-01	SUB PHASE CP-02	SUB PHASE CP-03	SUB PHASE CP-04	SUB PHASE CP-05	SUB PHASE CP-14	MAJOR PHASE 1 CP TOTAL	CANDLESTICK POINT ENTITLEMENT
Residential Density I (units) (15-75 Units Per Acre)	19	-	0	-	37	-	56	-
Residential Density II (units) (50-125 Units Per Acre)	306	350	130	540	142	-	1,468	-
Residential Density III (units) (100-175 Units Per Acre)	-	-	140	-	-	-	140	-
Residential Density IV (units) (175-285 Units Per Acre)	-	220	330	-	-	-	550	-
Total Residents Units	325	570	600	540	179	-	2,214	6,225
Neighborhood Retail (GSF)	-	1,000 ⁽¹⁾	65,000	65,000	-	-	131,000	131,000
Regional Retail (GSF)	-	635,000	-	-	-	-	635,000	635,000
Office (GSF)	-	134,500	-	-	-	-	134,500	134,500
Performance Venue	-	75,000	-	-	-	-	75,000	75,000
Hotel (GSF)	-	150,000 (220 Rooms)	-	-	-	-	150,000 (220 Rooms)	150,000
Research & Development (GSF)	-	-	-	-	-	-	-	-
Total Commercial Area (GSF)	-	995,500	65,000	65,000	-	-	1,125,500	1,125,500
Community Use (GSF)	-	1,000	41,000	5,000	-	3,000 ²	47,000	50,000
Total Parks & Open Space (AC)	0.0	7.5	1.1	0.0	0.72	-	9.32	-

¹The 1,000 gsf of neighborhood retail in Sub-Phase CP-02 is reserved for a cafe in the Wedge Plaza.

²The 3,000 gsf entitlement reserve for Community Use is for the CPSRA Welcome Center.

6. LAND USE & MASSING

6.2 HOUSING

The first Major Phase of development at Candlestick Point is planned to include approximately 2,214 residential units. More than 900 units are anticipated to be below market rate, which greatly exceeds the overall project target of approximately 32% below-market rate units. The below market-rate units included in Major Phase 1 CP are comprised of several housing types, which will serve a broad range of incomes:

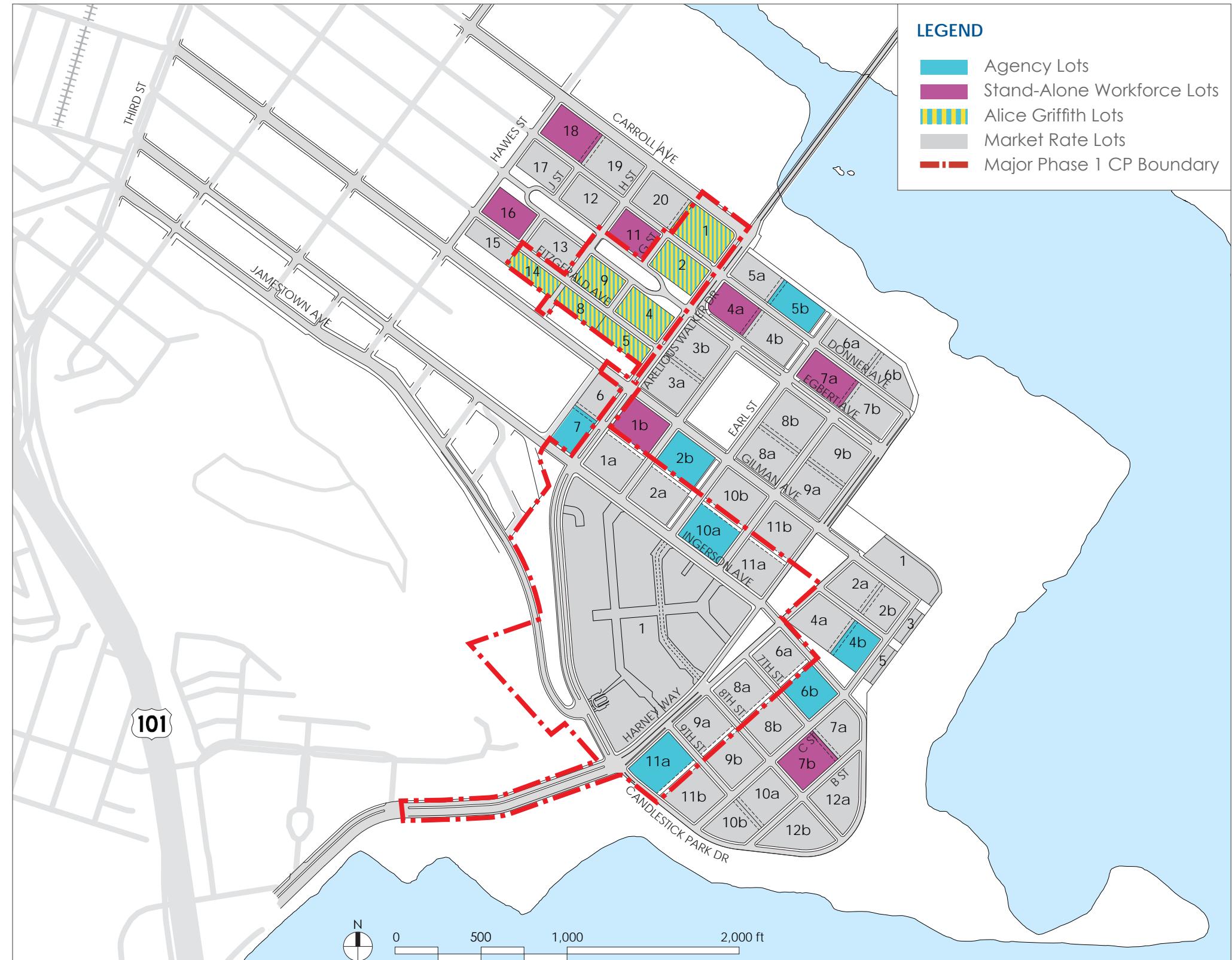
- Alice Griffith Replacement Units** – The affordability of the Alice Griffith units is determined by the Federal Department of Housing and Urban Development. The below market-rate housing includes the 1:1 replacement of all 256 public housing units at Alice Griffith. The Project provides for the phased replacement of these public housing units so that residents will be able to move directly into new units without having to relocate off-site.
- Agency Affordable Units** – These units will serve households earning up to 60% Area Median Income (AMI), the federal metric used to determine housing affordability.
- Inclusionary Units** – These units will serve households earning between 80% and 120% of AMI. Of the total units on each Market Rate Lot, between 5% and 20% will be Inclusionary Units.
- Workforce Units** - These units will serve households earning between 121% and 160% of AMI. Up to 40% of the total units on each Market Rate Lot may be Workforce Units.

A summary of the housing in Major Phase 1 CP is shown in Table 6.2. The proposed location of Below-Market Rate lots is shown in Figure 6.2. Additional housing data can be found in Appendix C.

Table 6.2 – Housing

	SUB-PHASE CP-01	SUB-PHASE CP-02	SUB-PHASE CP-03	SUB-PHASE CP-04	SUB-PHASE CP-05	MAJOR PHASE 1 CP TOTAL
Alice Griffith Units	220	-	-	-	36	256
Agency Affordable Units	105	-	140	150	143	538
Workforce Units	-	-	-	-	-	-
Inclusionary Units	-	58	47	40	-	145
Market Rate Units	-	512	413	350	-	1,275
Total Housing Units	325	570	600	540	179	2,214

Figure 6.2 – Location of Below Market Rate Lots



6. LAND USE & MASSING

6.3 SITE SECTIONS

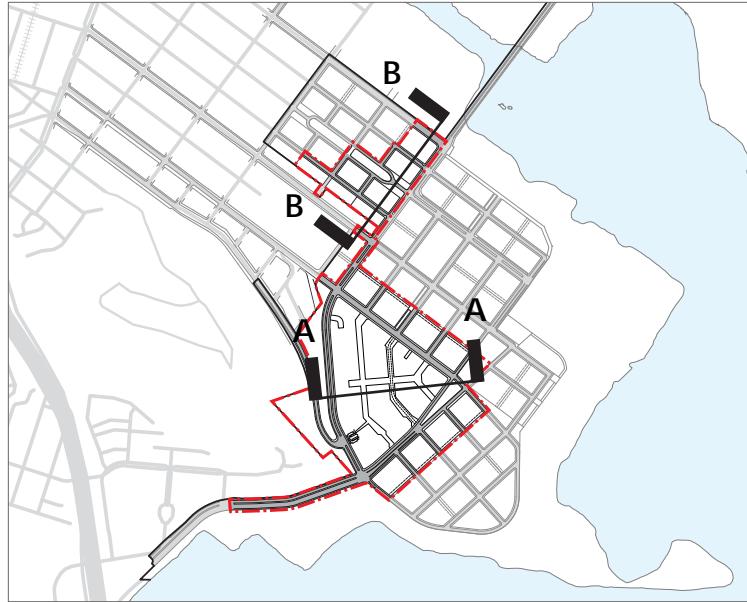


Figure 6.3 – Section A-A: Site Section Across CP Center

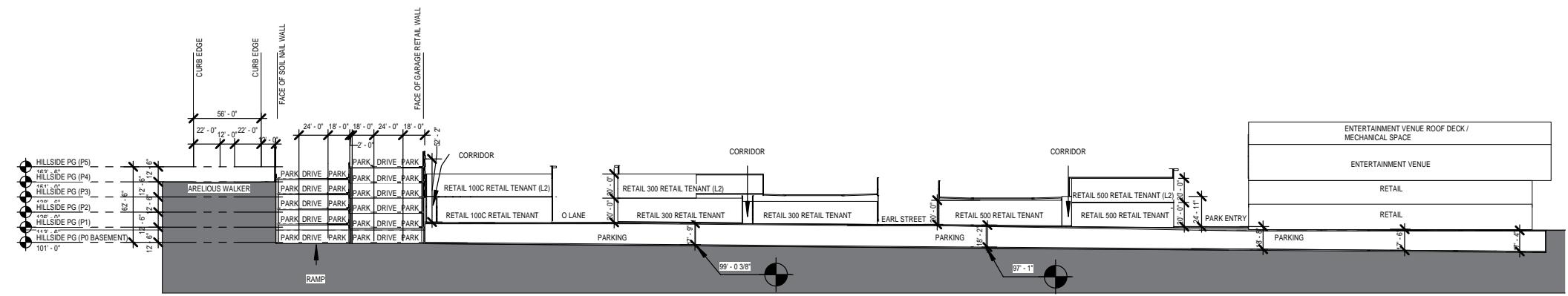
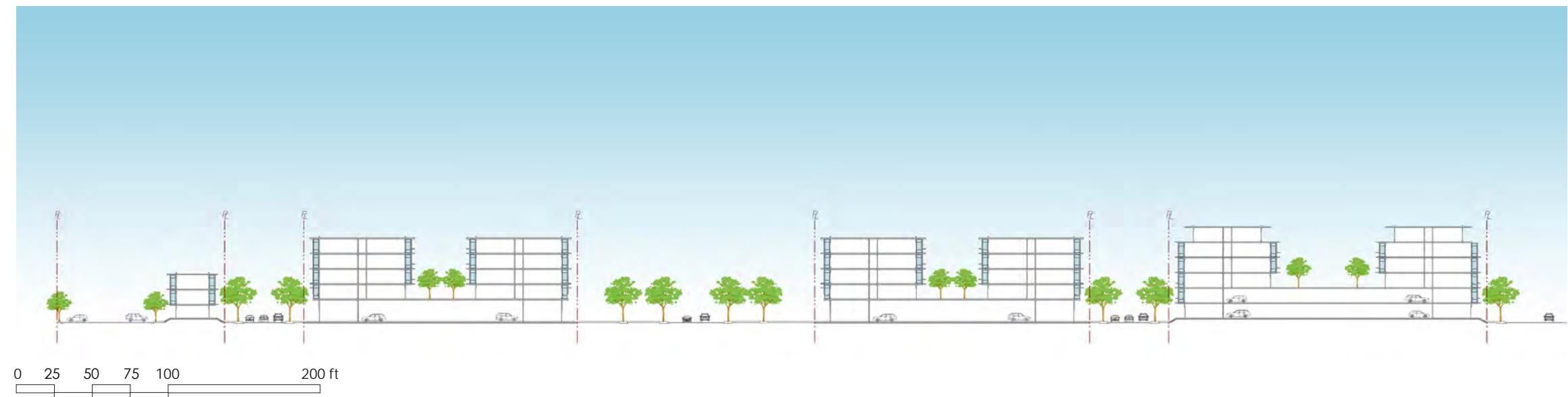


Figure 6.4 – Section B-B: Site Section Through Major Phase 1 CP - Starting from the left: AG-5, AG-4, AG-2 and AG-1



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

Building heights, bulk, massing and other detailed Project design specifications are controlled by the Design for Development (D4D), a document governing all development in the Project area.

Design principles include:

- Locating lower density building forms nearest to the existing Bayview community,
- Formation of skylines for aesthetic effect as viewed from throughout the City and surrounding area,
- Creating landmarks for urban place-making,
- Clustering higher density near important nodes,
- Linking highest density with adjacency to transit stops,
- Minimizing impacts on public open space, and
- Framing view corridors including the view from Bayview Hill.

Larger buildings are placed in strategic locations to emphasize street walls and frame the public realm. Towers are located at key intersections, facilitating wayfinding, while creating a scenic skyline from afar. While OCII and the Developer believe that the tower configuration illustrated represents the optimal development scenario, vertical development of the Project will occur over years and a minimal amount of flexibility in tower locations is needed to ensure that the Project is able to respond to changing construction technologies, community priorities, site-specific urban design goals, and real estate market demands.

The D4D allows for this basic flexibility while adhering to the tower location principles described above by creating allowable "tower zones" for high-rise buildings.

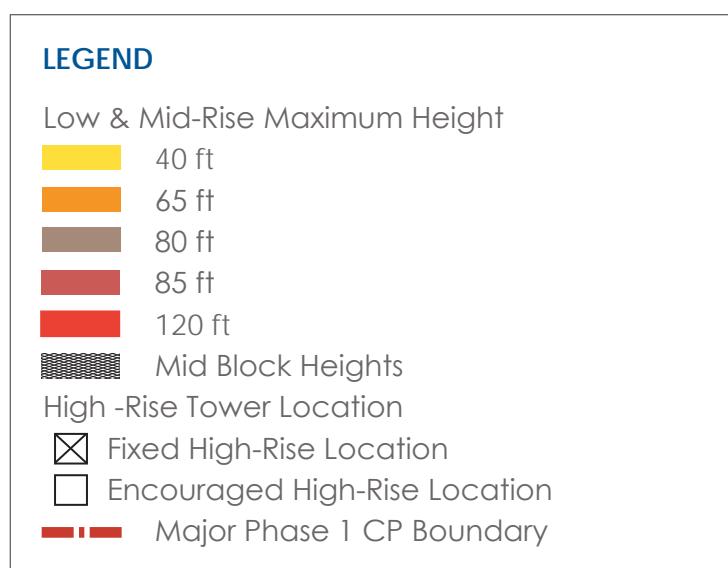


Figure 6.5 – Building Heights



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

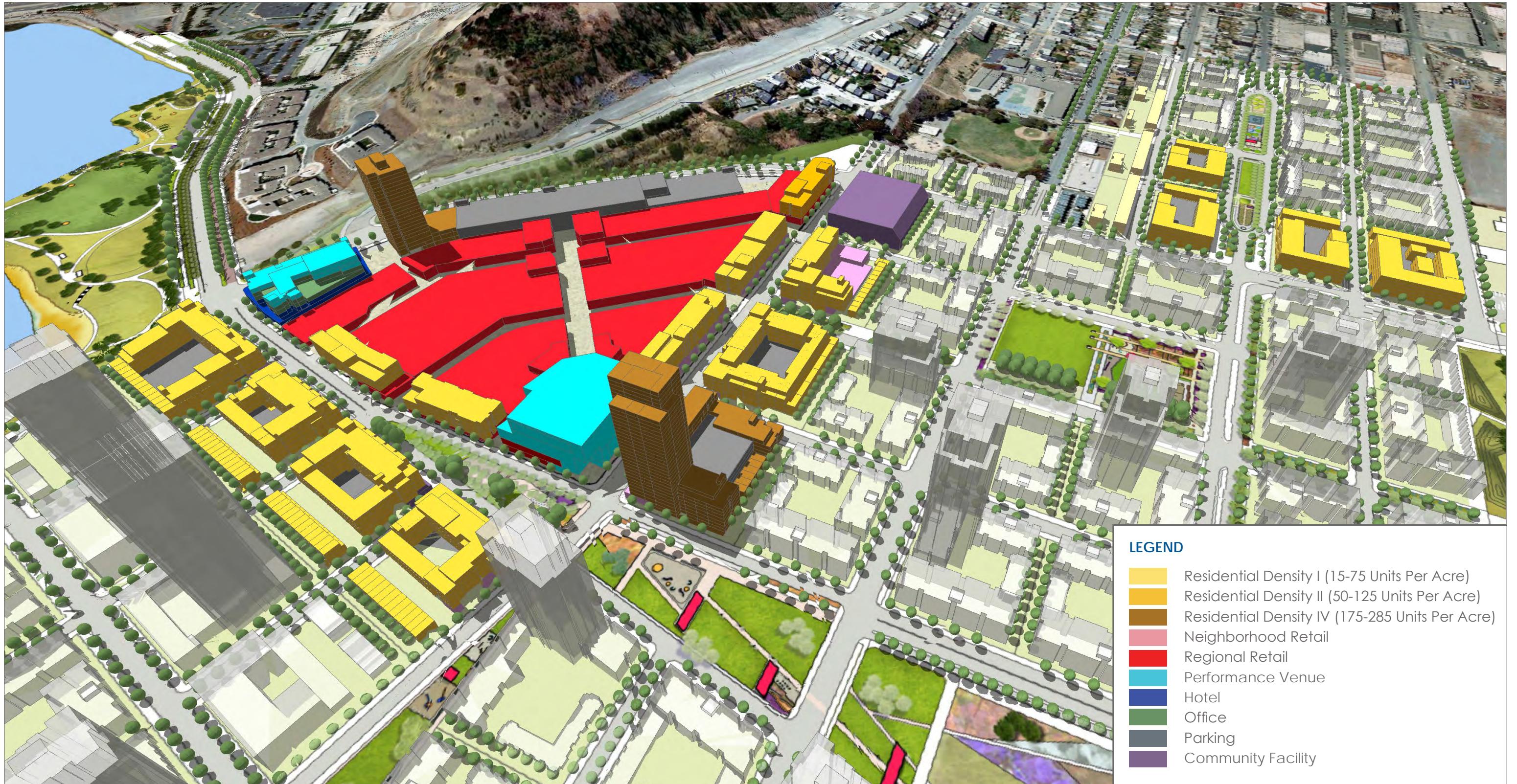
Figure 6.6 – Major Phase 1 CP Massing Looking North



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

Figure 6.7 – Major Phase 1 CP Massing



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

Figure 6.8 – Major Phase 1 CP Massing Looking South



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

Intent

The following standards governing bulk and massing intend to facilitate building shapes that fit comfortably within their surroundings, are friendly and unimposing to pedestrians, achieve an attractive urban form, and are interesting. The mass of buildings should be shaped in such a way as to create fine-grained forms, reinforce the street and block pattern, and protect surrounding views and sunlight.

Standards

Development Block Coverage – Block coverage by all habitable and non-habitable buildings, including projections and structured parking, is limited as indicated in Table 6.3. A development block is defined as all land inside the legal property line. For the purpose of calculating coverage, the area of the block shall be exclusive of required setbacks and mid-block breaks. Notwithstanding the parcel coverage standards, individual buildings within the parcel shall not exceed the sizes set forth in Table 6.4.

Bulk Controls – Maximum floor plate sizes, plan lengths, and diagonals to limit the bulk of buildings are listed in Table 6.4. The maximum diagonal dimension shall be measured between the two points of a building's longest diagonal separation.

Massing Controls – Controls of apparent faces and stepback of upper floor(s) to limit the massing of buildings are also listed in Table 6.4.

Apparent Face – The unbroken plane of a building or ‘apparent face’ shall not exceed a maximum length without being broken by a change – either an offset in the horizontal plane, or a change in fenestration and/or material, or both in the case of high-rise buildings. There are different standards for the base section and upper section of the building to reflect the desire for a finer grain of building articulation at the street level. The base is defined low- and mid-rise buildings as the first 20 feet height minimum; for high-rise buildings as the first 35 feet height minimum. See Table 6.4.

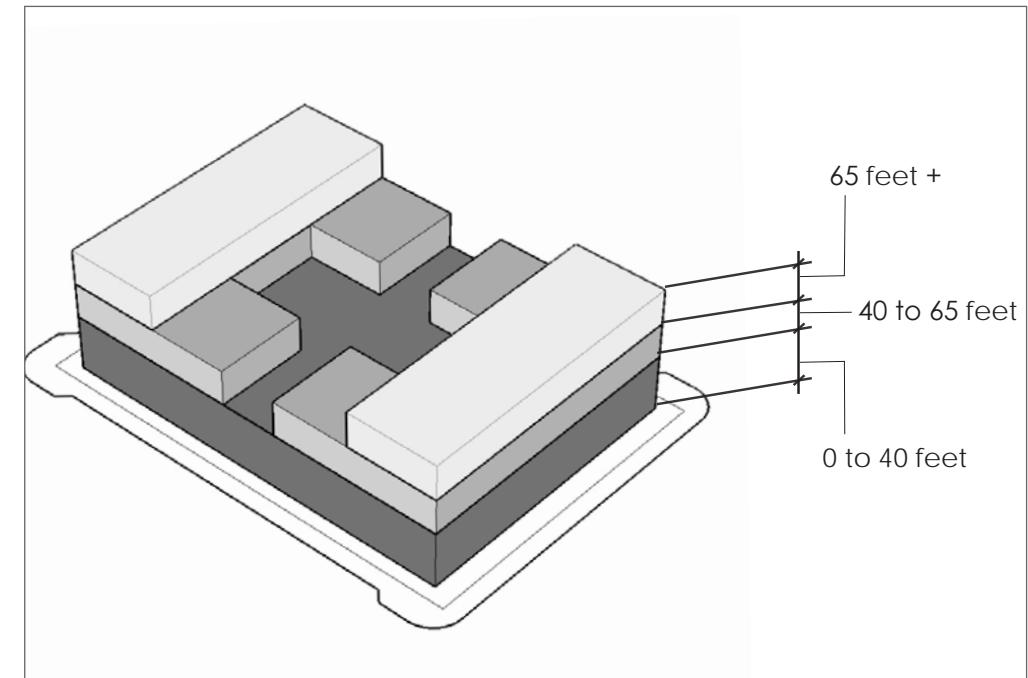
Upper Floor(s) Stepback – The upper floor(s) of low and mid-rise buildings above a specified height shall step back a minimum of 20% of the floor plate size relative to the floor immediately below, as defined in Table 6.4 - Massing, All Building Types and Table 4.5 of the D4D.

Podiums – High-rise buildings may have a podium, defined as a base whose plan dimensions are greater than those of the floors above. The podium height for high-rise buildings shall not exceed the podium height limit provided in Table 4.3 of the D4D. All podium floors with a maximum height (distance to ground) below 85 feet shall not be subject to the bulk controls (maximum floor plate, maximum plan length and maximum diagonal) for high-rise buildings shown in Table 6.4 - Massing, All Building Types. All podium floors with a maximum height of 85-105 feet shall be subject to the bulk controls for mid-rise buildings of 85-105 feet shown in Table 6.4 - Massing, All Building Types. Notwithstanding these exceptions, the podium shall be subject to massing controls and all other applicable regulations.

Table 6.3 – Development Block Coverage

DEVELOPMENT BLOCK COVERAGE	
HEIGHT (FT)	COVERAGE
0 – 40	100%
40 – 65	75%
65 +	50%

Figure 6.9 – Development Coverage



6. LAND USE & MASSING

6.4 BUILDING HEIGHTS, BULK & MASSING

Figure 6.10 – Apparent Face

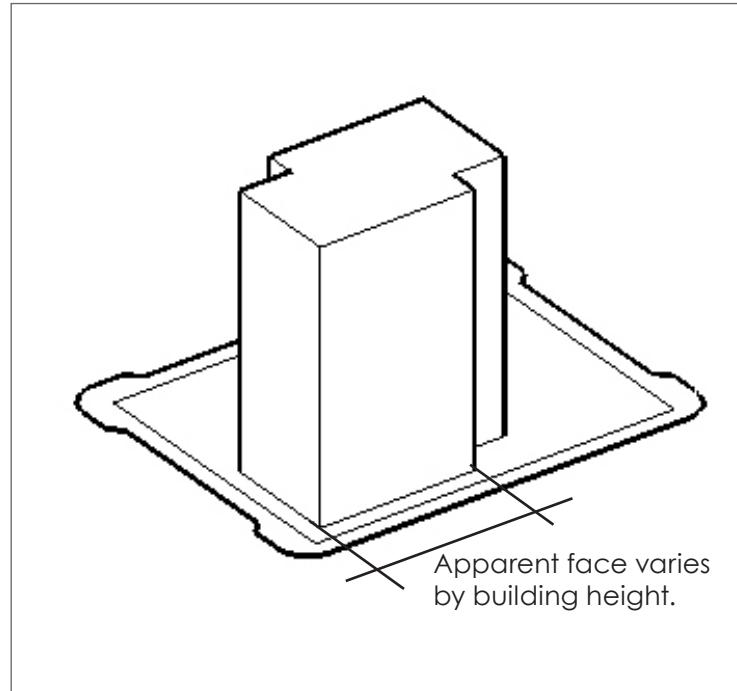


Table 6.4 – Massing - All Building Types

BUILDING LENGTHS AND SIZES									
BUILDING TYPE	LOW-RISE	MID-RISE		HIGH-RISE				LANDMARK BUILDING	
BUILDING HEIGHT	MAX 65 FT	ABOVE 65 FT TO MAX 85 FT	ABOVE 85 FT TO MAX 105 FT	ABOVE 105 FT TO MAX 180 FT	ABOVE 180 FT TO MAX 240 FT	ABOVE 240 FT TO MAX 350 FT	ABOVE 350 FT	MAX 120 FT	
MASSING CONTROLS	Max Floor Plate	n/a	n/a	15,000 sq ft	12,000 sq ft	10,500 sq ft	12,000 sq ft	12,500 sq ft	50,000 sq ft
	Max Plan Length	n/a	n/a	210 ft	140 ft	140 ft	140 ft	145 ft	250 ft
	Max Diagonal	n/a	n/a	n/a	170 ft	160 ft	170 ft	175 ft	350 ft
	Max Apparent Face - Base ¹	30 ft							
	Min Change in Apparent Face – Base ¹	Offset in the horizontal plane of minimum 2 ft depth and 3 ft length OR a major change in fenestration and/or material							
	Max Apparent Face – Above Base ¹	30 ft	100 ft	100 ft	105 ft	100 ft	105 ft	110 ft	250 ft
	Min Change in Apparent Face – Above Base ¹	Offset in the horizontal plane of minimum 1 ft depth and 1 ft length OR a major change in fenestration and/or material		Offset in the horizontal plane of minimum 10 ft depth and 10 ft length OR a major change in fenestration and/or material					
BULK CONTROLS	Upper Floors Stepback	Floors above 55 ft: 20% of floor plate directly below Abutting Mid Block Break: Floors above 35 ft - 1:1.2 plane	Floors above 65 ft: 20% of floor plate directly below Abutting Mid Block Break: Floors above 35 ft - 1:1.2 plane	Floors above 85 ft: 20% of floor plate directly below Abutting Mid Block Break: Floors above 35 ft - 1:1.2 plane	n/a				
	High-rise Shaping	n/a			Additional standards regulating segmentation of the high-rise elevation and floor plan.				n/a
	Massing Image ²								

* Note: Mid-rise buildings above 85 feet to a maximum of 105 feet are only applicable in the Shipyard South R&D Option.

¹The base is defined as a minimum of the first 20 feet in height for low- and mid-rise buildings; and as a minimum of the first 35 feet in height for high-rise buildings.

²Massing images for high-rise do not show podiums, which are permitted.

6. LAND USE & MASSING

6.5 STREET WALL

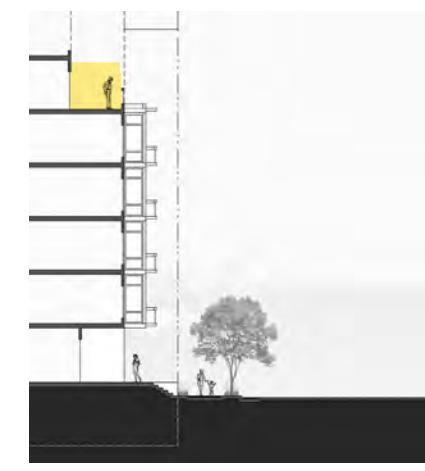
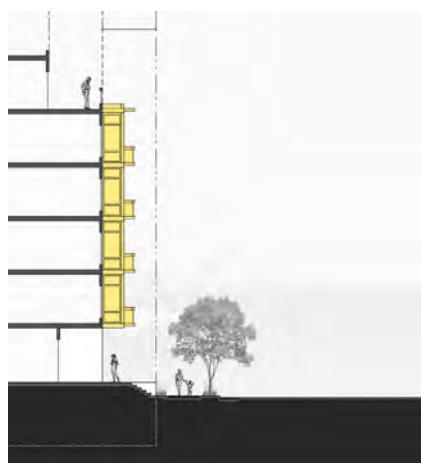
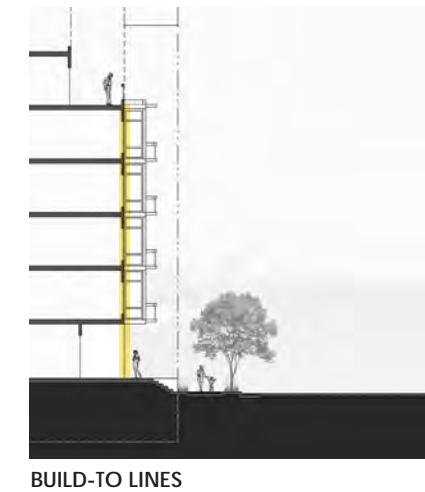
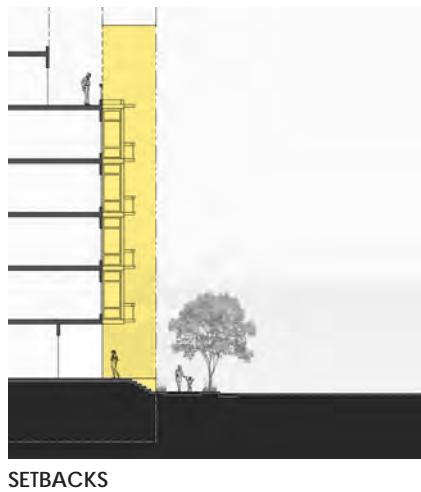
The street wall section of the D4D has a definition of the key controls, sets forth the standards, and concludes with a series of cross sections that illustrate the standards by building use.

Intent

In order to control the quality and character of the block edges and street walls, and for controlling the expression of the mass of the buildings, standards for building uses are set forth for:

- A Setbacks
- B Build-to lines
- C Projections
- D Stepbacks

As a means of controlling the quality of the at-grade environments these streetwall controls also include considerations for grade separation, retail space heights and depths, and underground parking.



A – Setback

A building setback is the minimum required distance between the property line and the nearest face of the building. Setbacks apply to the ground floor use of a building. Setback zones, where specified, should be used for the purpose of landscaping or for active uses such as patios and entrance areas. The D4D calls for extensive setbacks throughout the community affording a comfortable and pleasant pedestrian experience that will be a departure from the development practices of most other San Francisco neighborhoods where buildings typically abut against or are close to the property line.

Standards

Residential Setbacks – A minimum setback of 10 feet to building face is required for residential buildings to allow for the provision of private landscaping and street facing patios and stoops. The setback shall not vary along the predominant wall of a building once established (aside from minor variation which are described in Build-To Percentages). In cases where residential blocks are fronted by sidewalks with a 6 feet throughway, a public easement may be employed in the setback to provide a wider throughway. At the time a Sub-Phase Application is submitted, OCII may request that the Developer grant a public easement up to a maximum of 2 feet within the 10-foot residential setback to create an 8-foot throughway.

Exceptions:

1. Residential use that is located above retail use (i.e. mixed-use) may extend to property line.
2. Portions of a residential building that are adjacent to or across the street from a park/open space shall have a minimum setback of 6 feet.
3. The street side of CP South blocks 3 and 5, due to the shallow block depth, shall have a minimum setback of 5 feet, not in Major Phase 1 CP.

Mixed-Use/Commercial Setbacks – There are no required setbacks for mixed-use/commercial buildings, except for parking structures, which shall have an 18 inch setback.



PRECEDENT – RESIDENTIAL SETBACK PROVIDES PRIVATE OPEN SPACE ZONE



RETAIL HAS NO SETBACK IN ORDER TO STRENGTHEN THE RELATIONSHIP WITH SIDEWALK

B – Build-To Line

Build-To Lines are intended to ensure that buildings are situated at or close to setback lines in order to create and maintain defined street walls. Street walls are important in the framing and animation of the public right of way. A successful development of street wall will create defined 'outdoor rooms' which will invite greater activity of residents and visitors alike.

The build-to line is expressed as a percentage of the setback line for building faces that front a public street. For instance, with a 70% build-to line, 70% of all building faces fronting a public street must meet the setback, while no more than 30% of building faces may be behind the setback.

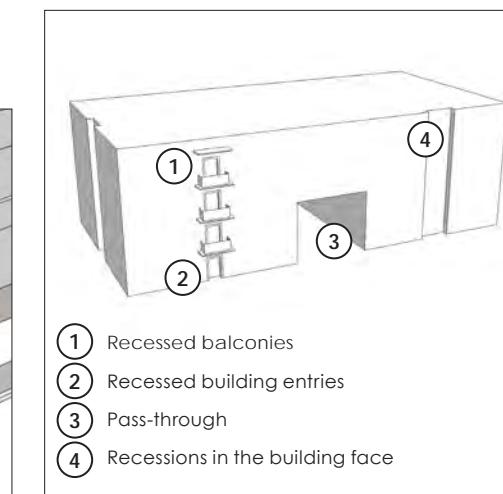
Standards

The build-to line standard for residential buildings is 70% and for mixed-use and commercial buildings is 85%.

Exemptions – Minor variations excluded from the calculation of the minimum build-to percentage are:

For retail uses, recesses including entrances, walk-up window or street patio area shall not be allowed on more than 50% of the total frontage of the building and no recess shall be greater than 12 feet in depth.

- Recessed balconies.
- Recessed building entries to a maximum depth of 8 feet.
- Pass-through up to 2 floors in height.
- Recession in the building face for the purpose of building articulation.
- Stepback on the top floor or top two floors.
- Stepback for high-rise sculpting.



PRECEDENT – RECESSED BALCONIES EXEMPTED FROM BUILD-TO CALCULATIONS

MINOR VARIATIONS EXCLUDED FROM BUILD-TO LINE CALCULATIONS

6. LAND USE & MASSING

6.5 STREET WALL

C – Stepback

A stepback is that portion of a building that must be stepped back from the setback line. Typically, this is regulated for the upper floor(s) of mid-rise buildings as a means of sculpting their mass.

Standards

Upper Floor(s) Stepback – The upper floor(s) of low and mid-rise buildings above a specified height shall stepback a minimum of 20% of the floor plate of the floor immediately below the specified height. The stepback requirement shall apply to:

- Any floor(s) of a Low Rise Building with a maximum height above 55 feet;
- Any floor(s) of a Mid Rise Building with a maximum height between 65 feet to a maximum of 85 feet; and
- Any floor(s) of a Mid Rise Building with a maximum height between 85 feet to a maximum of 105 feet (Shipyard South R&D Option only – see Section 8 of Candlestick Point D4D).

Where abutting a Mid-Block Break that is a Pedestrian Mews or Vehicular Laneway, any portion of a low or mid-rise building above 35 feet shall step back at a plane ratio of 1:1.2 (see Table 4.6 and Figure 4.12 of Candlestick Point D4D).

Allowable uses with the stepback roof area include usable open space, landscaping, and railings. Mechanical space is not allowed.



PRECEDENT – STEPBACK AT TOP FLOOR

D – Projection

A projection is that portion of a building that projects beyond the main building face. There are a number of types of projections as described below.

Standards

Habitable Projections – Habitable space within a projection means a portion of the building enclosed by walls and a roof. Typically this will be a bay window, corner element, or regularly occurring bay that extends through some or all floors of a building. A habitable space may project 3 feet beyond the building face, either into a setback zone or the public realm. No individual habitable projection may exceed 15 feet in length. All projections shall have a minimum clearance to the sidewalk of 9 feet.

Non-habitable Projections – non-habitable projections are spaces utilized by residents that are not enclosed by walls and a roof. Non-habitable spaces include all usable balconies, which may extend no more than 6 feet into a setback, or common open space or 3 feet into the public realm. No individual non-habitable projection may exceed 15 feet in length. All projections shall have a minimum clearance of 9 feet to the sidewalk.

Cumulative Projections – The cumulative total of all types of projections shall not exceed 67% of the building face.

Other Projections – Other allowable projections include:

- Decorative elements such as belt courses, cornices, sills and eaves to a maximum 2 feet 6 inches beyond the setback.
- Decks, patios and steps at the first floor of occupancy may project to the property line but not beyond.
- Fences, railings, chimneys, awnings and canopies may project to the property line but not beyond.
- Retail signs, canopies and awnings may project 5 feet beyond property line; a minimum 9 feet vertical clearance to the sidewalk shall be maintained.
- Sustainable elements such as solar shades.



PRECEDENT – BAY WINDOW PROJECTIONS WITHIN SETBACK ZONE

6. LAND USE & MASSING

6.6 BUILDING GROUND FLOOR TREATMENT

Residential

Several key characteristics of residential buildings will differentiate Candlestick from many San Francisco neighborhoods. In particular, the lower floors of residential buildings are intended to engage the street by having activated ground floor uses and lush landscaping in setbacks, helping to animate the streets and create a vibrant pedestrian oriented neighborhood.

Standards

Ground Floor Unit Entrances – Ground floor units fronting public streets, parks, or along pedestrian mews shall have an access point along the fronting building face in addition to the main access from interior corridor, lobby, or parking structure. Entrances shall occur at intervals no greater than 30 feet, and may be ganged together.

Grade Separation – Ground floor units shall be elevated between 2 feet and 4 feet above the street for privacy.

Townhome Garages – Street fronting townhome garages are prohibited on public streets. Any townhomes that incorporate garages along a mid-block break shall engage the mid-block break /street with design characteristics to limit the visual presence of garage doors, emphasizing the garage as secondary to the main entrance and front yard. The maximum number of garage doors per unit is one with a maximum width of 8 feet. Side-by-side garages are prohibited.

Guidelines

Freestanding Townhome Form ('Tuck-under') – Freestanding townhomes may be designed with individual character, or in a consistent style. Modular rhythm should be emphasized through the use of common elements such as bay windows, door recesses materials and fenestration. Variety in form at the pedestrian level is encouraged. Townhomes that form the base of a multi-story building should have elements and proportions that tie them to the building above.

Residential Courtyards – Residential courtyards that may be accessed or at least viewed from public streets and mews are encouraged.



PRECEDENT – RESIDENTIAL COURTYARD



PRECEDENT – GRADE SEPARATED PATIO

Retail

Retail should engage and enliven the street. Emphasis should be placed on using glazing and creating an architectural rhythm at the ground plane.

Standards

Setbacks – There are no required setbacks for commercial buildings.

Build-to Line – 85% of the building face shall be built to the property line. Patio spaces, entrances, publicly accessible plazas and walk-up windows are exempted provided they are stepped back no further than 12 feet from the property line and cumulatively for no more than 25% of the building face.

Projections – Projections are permitted for awnings, canopies, signage and lighting to a maximum of 5 feet into the public right-of-way provided they have a minimum of 9 feet clearance to the sidewalk.

Sidewalk Relationship – Retail buildings shall be oriented to and meet the sidewalk at grade.

Storefronts Shall promote pedestrian interest at the ground level and provide visual connection to the store interior with:

- Store frontage shall have at least 60% glazing; glazing shall be transparent. Large multi-story retailer's upper floor levels shall also meet this glazing requirement.
- Outdoor displays and patios are encouraged, but shall maintain a minimum 6 feet wide clear pedestrian zone within the public sidewalk.
- Interior displays shall provide visual permeability into store interior.

Store Height and Depth – All retail spaces along both sides of Harney Way and Ingerson Avenue at CP Center shall be a minimum of 20 feet height and a minimum average of at least 35 feet in depth exclusive of service corridors. Minimum depth shall not apply to storefront liners of large format retail uses. All other retail uses shall have a minimum height of 15 feet.

Façade Articulation – Retail bays shall be no wider than 30 feet in order to create a fine-grained pattern of shops. Where a larger retailer is anticipated, bays can be combined; however the bay articulation shall be maintained. The impact of large retail stores can be mitigated by 'wrapping' exterior façades with smaller retail stores, thereby breaking up the façade and reducing large expanses of blank walls.

Blank Walls – Areas without entries or windows are prohibited on pedestrian oriented retail streets and paseos, except at building service areas and areas where floor elevation is not within 48 inches to sidewalk elevation due to grades (i.e. steep sections of Arelious Walker Drive). Blank walls shall be no longer than 8 feet along other retail street frontages. Display windows are not considered blank walls, provided they allow visual access into store interior.

Guidelines

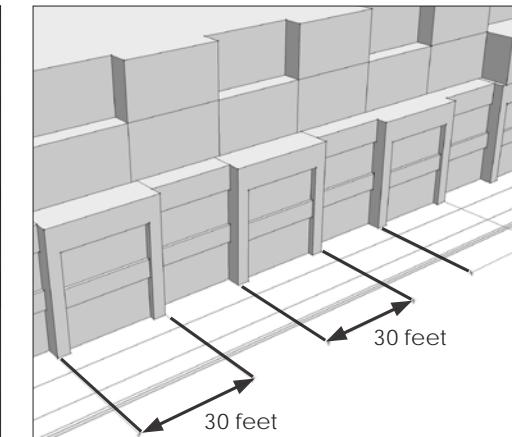
Entrances – Retail entrances should be easily identifiable and distinguishable from residential entrances. They should be reinforced with such elements as recessed doorways, awnings, special lighting, fenestration, color and materials, and special paving. Multiple entrances to larger stores are encouraged.

Materials – Façades should be designed with high-quality materials that offer color, variety, and visual interest to the pedestrian (such as stone, tile masonry, brick or terra-cotta).

Canopies / Awnings – Canopies or awnings should be provided for the sun, wind and rain protection of pedestrians. Their design should be integrated with the building architecture. Permanent materials are encouraged over vinyl or fabric.



CANOPIES AND BUILDING RECESSES CREATE INVITING SPACES



STOREFRONT BAYS ARTICULATED AT REGULAR INCREMENTS



PRECEDENT – RETAIL ENTRANCES SHOULD BE CLEARLY DISTINGUISHABLE



PRECEDENT – INTEGRATED CANOPY AND DISTINCT BAYS