DISTRICT OF COLUMBIA GOVERNMENT OFFICE OF THE SURVEYOR

Washington, D.C., June 24, 2014

Plat for Building Permit of SQUARE 3020 LOT 33

Scale: 1 inch = 10 feet Recorded in Book 44 Page 41

Receipt No. 14-05815

Furnished to: TAIESHA EDWARDS

Surveyor, D.C.

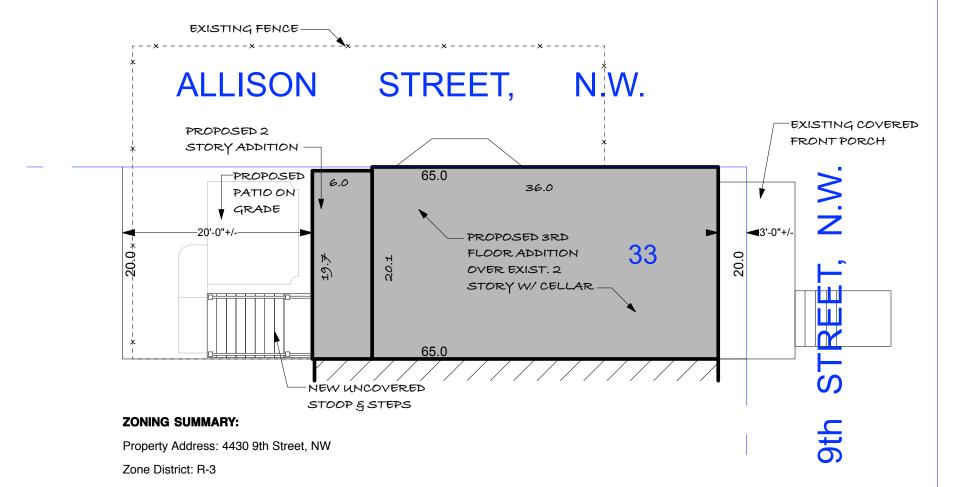
By: A.S.

I hereby certify that all existing improvements shown thereon, are completely dimensioned, and are correctly platted; that all proposed buildings or construction, or parts thereof, including covered porches, are correctly dimensioned and platted and agree with plans accompanying the application; that the foundation plans as shown hereon is drawn, and dimensioned accurately to the same scale as the property lines shown on this plat; and that by reason of the proposed improvements to be erected as shown hereon the size of any adjoining Lot or premises is not decreased to an area less than is required by the Zoning Regulations for light and ventilation; and it is further certified that all Lot divisions or combinations pending at the Office of Tax & Revenue are correctly depicted, and it is further certified and agreed that accessible parking area where required by the Zoning Regulations will be reserved in accordance with the Zoning Regulations, and that this area has been correctly drawn and dimensioned hereon. It is further agreed that the elevation of the accessible parking area with respect to the Highway Department approved curb and alley grade will not result in a rate of grade along centerline of driveway at any point on private property in excess of 20% for single-family dwellings or flats, or in excess of 12% at any point for other buildings. (The policy of the Highway Department permits a maximum driveway grade of 12% across the public parking and private restricted property.) Owner/Agent shall indemnify, defend, and hold the District, its officers, employees and agents harmless from and against any and all losses, costs, claims, damages, liabilities, and causes of action (including reasonable attorneys' fees and court costs) arising out of death of or injury to any person or damage to any property occurring on or adjacent to the Property and directly or indirectly caused by any acts done thereon or any acts or omissions of Owner/Agent; provided however, that the foregoing indemnity shall not apply to any losses, costs, claims, damages, liabilities, and causes of action due solely to the gross negligence or willful misconduct of District or its officers, employees or agents.

Date: ___

(Signature of owner or his authorized agent)

NOTE: Data shown for Assessment and Taxation Lots or Parcels are in accordance with the records of the Department of Finance and Revenue, Assessment Administration, and do not necessarily agree with deed description.



Existing Use: Single Family Residential / Row Dwelling* Proposed Use: Single Family Residential / Row Dwelling*

* Per definition in §199: "A one-family dwelling having no side yards."

Maximum Allowable Height: 40 ft. / 3 Stories Existing Height: 22 ft. / 2 Stories plus Cellar Proposed Height: 31 ft. / 3 Stories plus Cellar

Min. Required Lot Area: 2000 sq. ft. Existing Lot Area: 1300 sq. ft.

Lot is Sub Standard in regards to Min. Required Lot Area

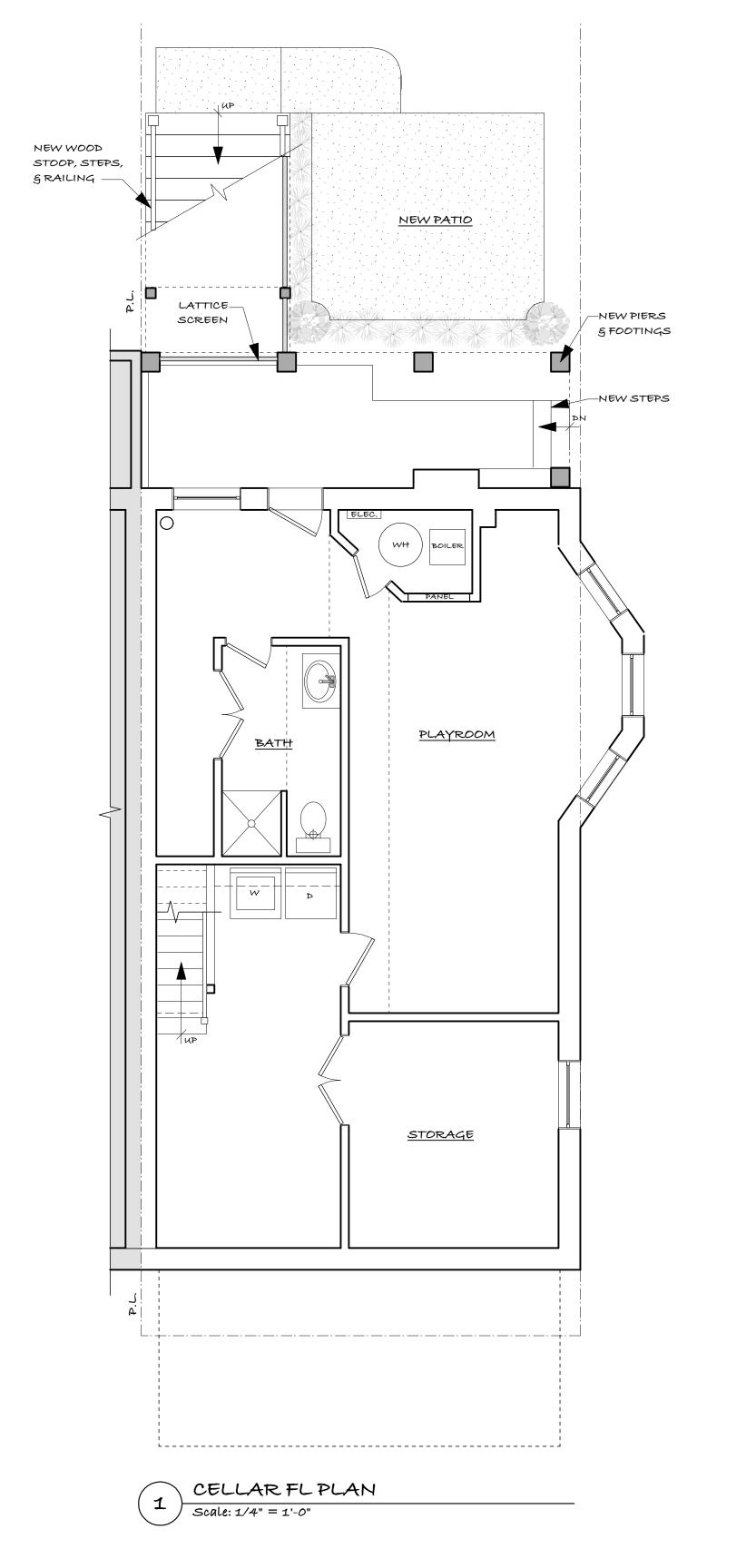
Min. Required Lot Width: 20.00 ft. Existing Lot Width: 20.00 Feet

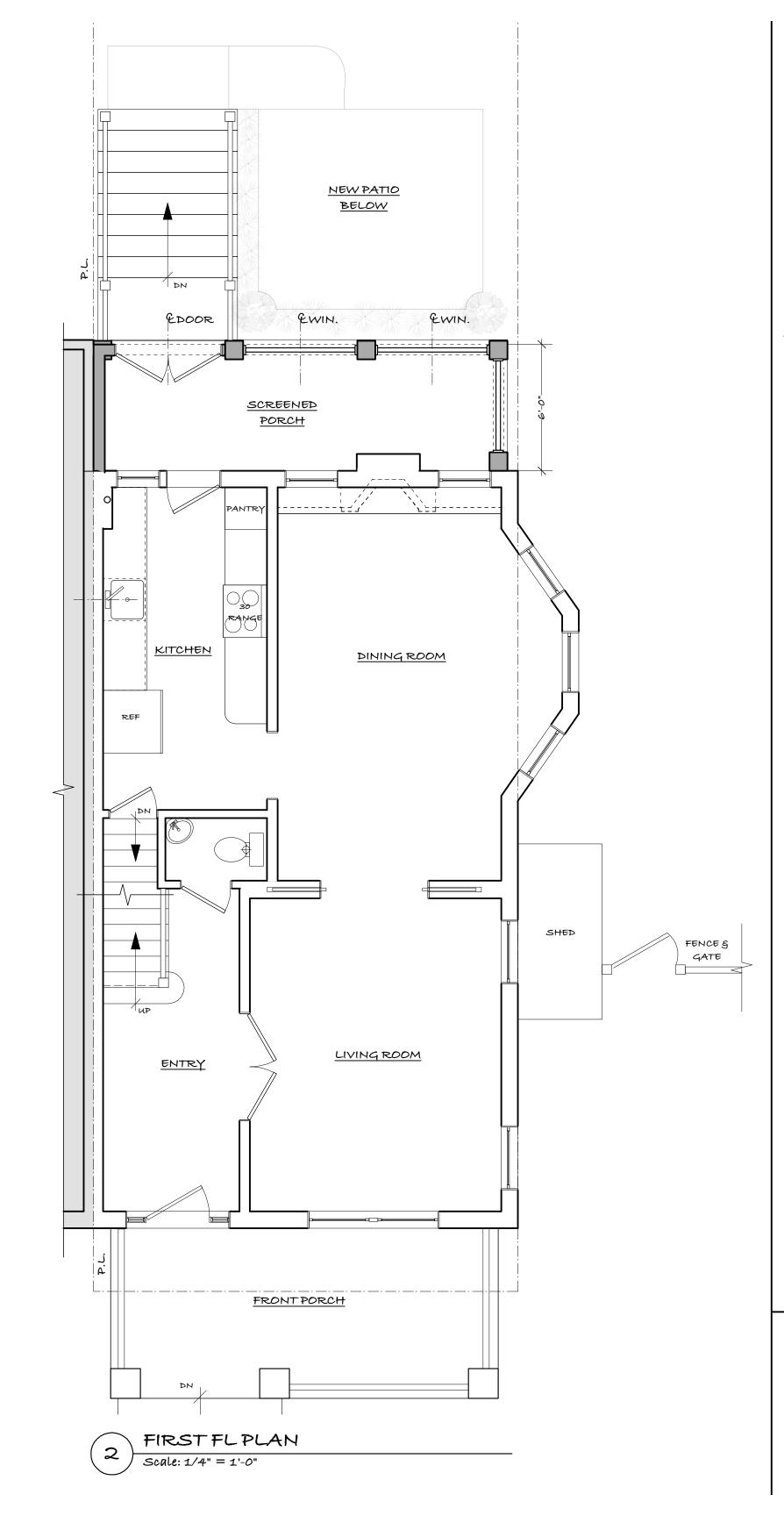
Max. Percentage of Lot Occupancy: 60% / 780 sq. ft. Existing Lot Occupancy: 60.0% / 780 sq. ft. Proposed Lot Occupancy 70.0% / 910 sq. ft. Requesting Special Exception per §223

Min. Required Rear Yard: 20 ft. Existing Rear Yard: 26 ft. +/-

Proposed Rear Yard at Addition: 20.00 ft.

SR-14-05815(2014) * E-MAIL





A R C H I T E C T U R A L S E R Y
4408 BEECHWOOD ROAD UNIVERSITY PARK, MARX

PROPOSED PLANS CELLAR & FIRST

GALLAS RESIDENCE
ADDITION & RENOVATION
4430 9TH STREET, NW
WASHINGTON, DC 20011

ISSUE DATE 21 JULY 2014

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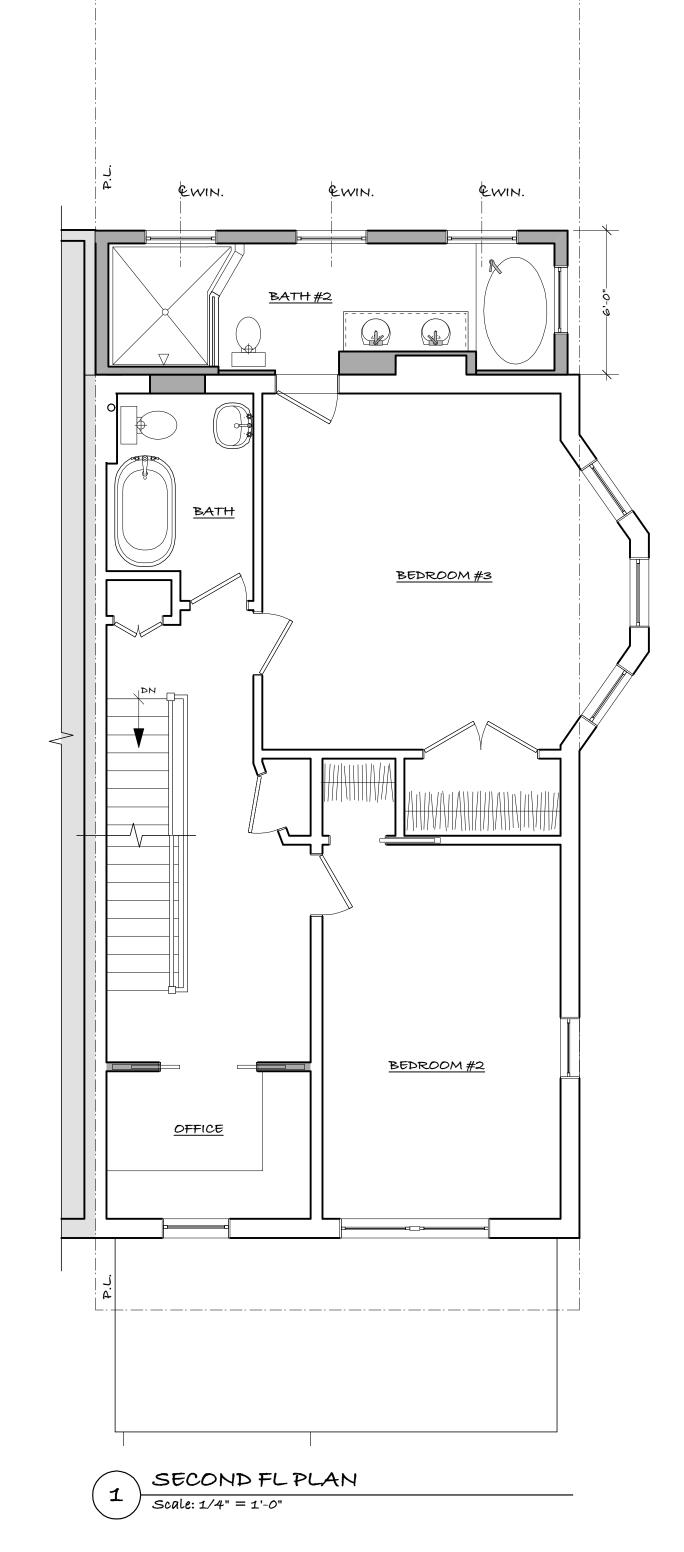
Ahmann LLC © 2014

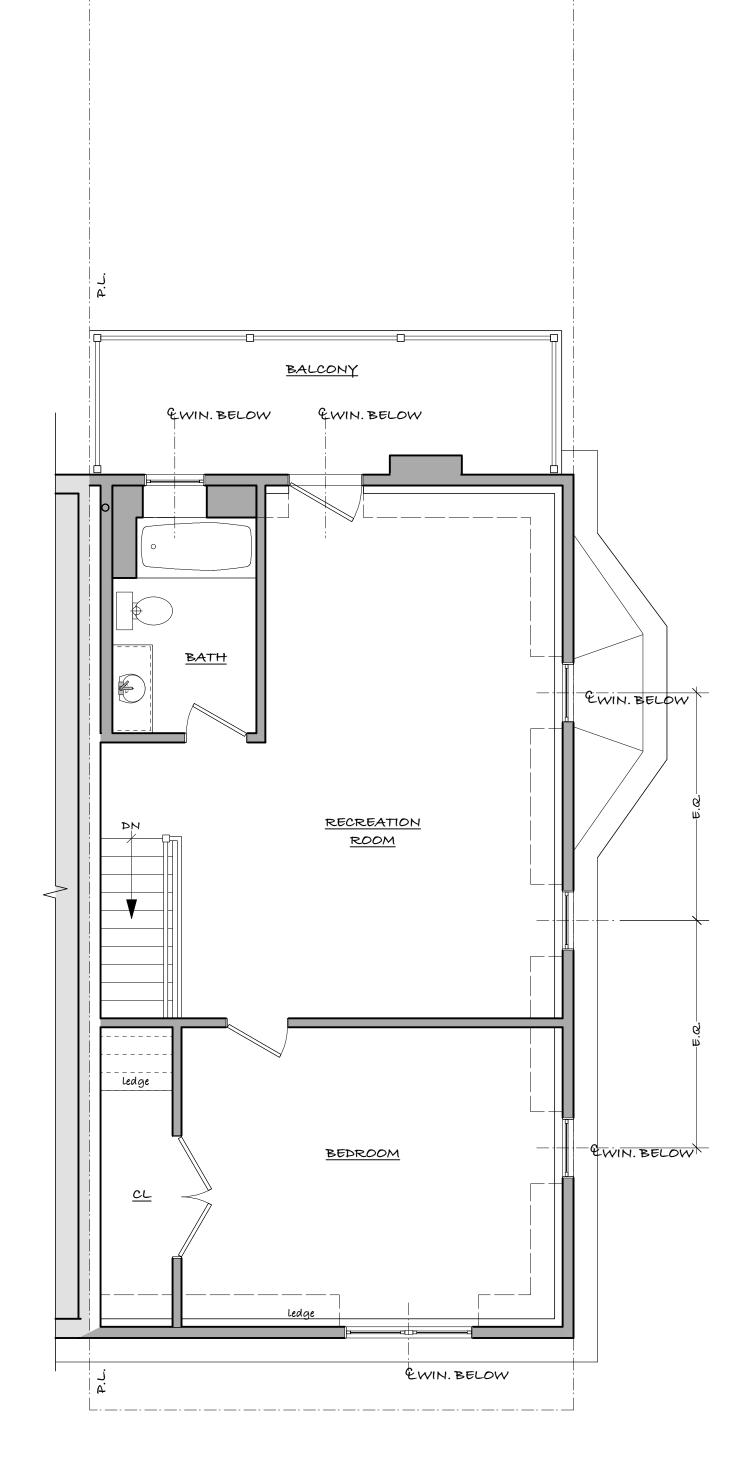
21 JULY 2014

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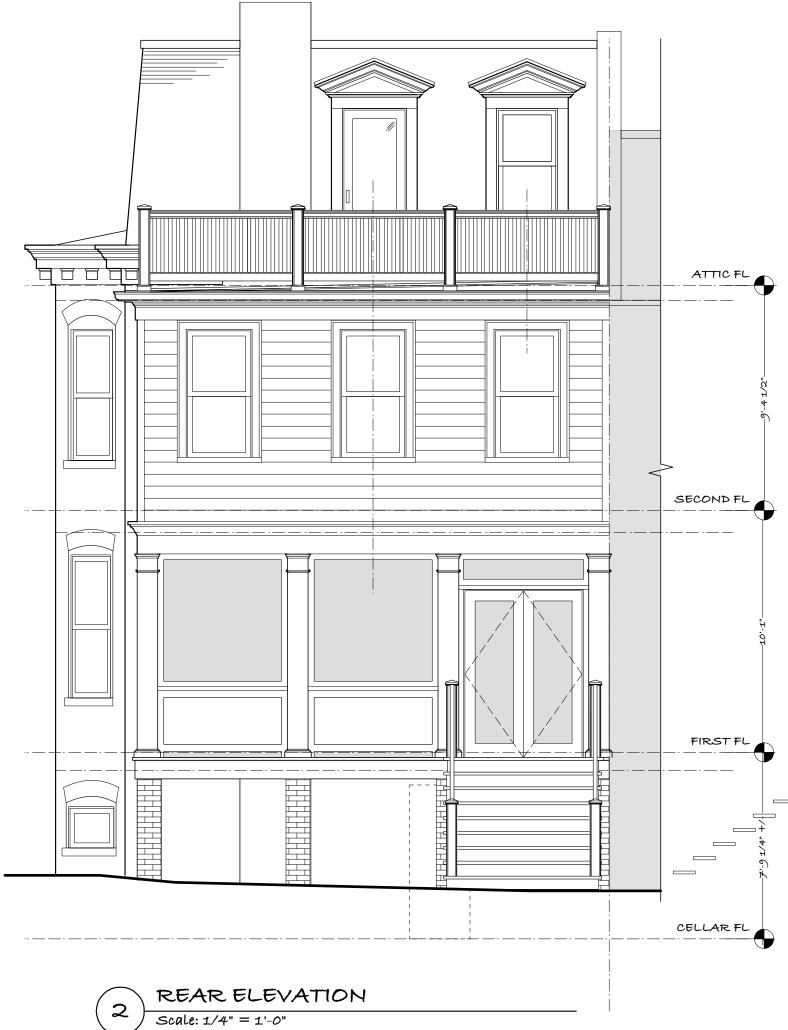


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21 JULY 2014

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PROPOSED ELEVATIONS RIGHT SIDE

IDENCE TOVATION GALLAS RESI ADDITION & REN

4430 9TH STREET, NW WASHINGTON, DC 20011

ISSUE DATE 21 JULY 2014

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