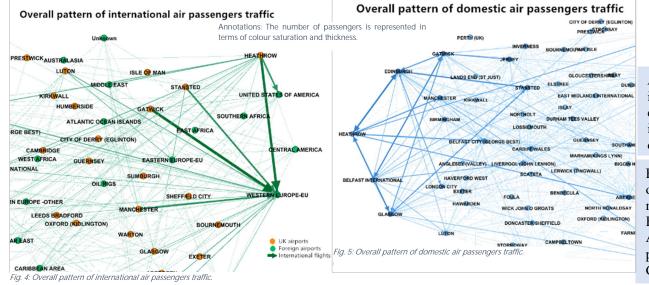


Fig. 1 Total Number of Domestic Passengers in 12 months of 2016

Fig. 1 shows that a minority of routes (as shown in the right) are much more popular than others among UK domestic flights. The whisker is far higher than the boxes, which means the maximum volume of the airports is far larger than the 75th, 50th or 25th percentile of volume of all airports. Similar phenomenon is observed in international flights in Fig. 2 It can be concluded that a few popular routes constitute the majority of UK domestic traffic.

The lines are often in pair and almost overlap each other, the similar number of passenger traffic in both ways show that people take return flights more often and less likely to take open-jaw flights. Although the data of international flights does not distinguish the outbound and inbound flights, it could be deduced that people take international flights also with the same practice.



Overall pattern of air passenger traffic - International&Domestic



As Fig. 3 shows, scheduled passengers are far more than chartered passengers in both London and other airports, because chartered flights are only considered as urgent, time-sensitive and ad hoc air transportation. More importantly, London area airports concentrate on international flights while other reporting airports focus more on domestic flights.

From Fig. 4 respecting international flights, in descending order of total number of passengers, Heathrow, Gatwick, Stansted and Manchester airports have far more flights and passengers than any other UK airports. Furthermore, Western Europe-EU has the most number of passengers and from other UK airports. Among these flights, Heathrow and Gatwick airports contribute the most passengers. For domestic flights in Fig. 5, Edinburgh, Glasgow, Heathrow, Gatwick, and Manchester airports have more passengers than all UK airports.

