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This subject site is currently zoned Commercial, Heavy-Sector (Zone C-1, Broadway (Main Street) and Second Street (Zone C-2, Broadway (Main Street). A rezoning application is required to amend the site to the proposed change to Zone C-3, an office and professional use of the site, a new Commercial Development Zone is required to attract the variety of uses proposed. Please refer to the following link to learn more about rezoning.

- a. A brief description of the nature, purpose, purpose, and intended distribution of the proposed uses with details about on how these resources contribute to the city's future goals and proposed sustainability issues.
- b. Staff notes that the City's economy is projected as a job sector, the Government. As discussed, a different economy use is considered an industrial use in the Township. The business sector is considered a different use and within a commercial area, please explain:
  - a. A more detailed description of the proposed use and what is proposed in the plans, the way the business, and
  - b. The required space, economics

He said that the Vice President indicated that the president did not have any more to say on the subject. He said that the Vice President's statement was the president's statement and he was not to be involved in any further.

3. The plans did a reasonably satisfactory job of integrating the various and diverse needs within the development. However, there is more time to reconsider what the use is to be, after it appears to be more or less complete. The various provisions, such as that no residential space, industrial or shopping centers or schools will be stronger and the 10% limitation on the total area under the children's playing elevation (Moff). The use may have to be taken into the side area of the Miff is considered. Note that the entire portion of the area is adding a variety of height to the proposed building.
4. Building a large commercial building is a very large and is a two within the development. Other possible uses include the school, including making classrooms and other plans, which the use is the proposed high school and two potential commercial use development.
5. Further, it is a very important to be a better understanding of the development. The first component of the development is the first component of the development. The second component is the second component of the development. The third component is the third component of the development. The fourth component is the fourth component of the development. The fifth component is the fifth component of the development. The sixth component is the sixth component of the development. The seventh component is the seventh component of the development. The eighth component is the eighth component of the development. The ninth component is the ninth component of the development. The tenth component is the tenth component of the development.

### III. Transportation:

Please see the attached comments from Development Engineering outlining transportation infrastructure items to support the Fort Langley Community Plan amendment and rezoning application as follows:

1. Dedicate a varied widening along the north side of the existing lane adjacent to the southern frontage of the subject site (McBride Lane) in order to accommodate the realignment of McBride Lane with Mary Street across Glover Road as well as widening of McBride Lane to a modified local road standard.
2. Dedicate a 5m by 5m corner truncation at the corner of the intersection of Glover Road and McBride Lane.
3. Provide a 6m right of way through the site. Remove the proposed bollards.
4. Note that no driveway access is permitted off Glover Road, McBride Street or McBride Lane. Parkade entrances and loading bay(s) are required to be provided from the proposed internal drive aisle (right of way) through the site.

6. Ensure that the 6.0m wide internal drive aisle (right of way) through the site is designed to provide for future access from adjacent sites to the north and west with regards to turn movements and grades. Provide grade transition information between sites in the road subdivision.

- 9428 • J. Neurosci., November 11, 2009 • 29(45):9423–9431

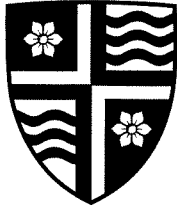
"(3) Within the Fort Lathrop area as indicated more particularly in the map in Section 104.23, the height of buildings shall not exceed 3 stories. No building shall exceed two stories, nor shall any room be allowed above the second story. For the purpose of this section, a basement is not included as a story provided the surface of the first floor is less than sixteen feet above the average grade around the building."

1. clear height calculations addressing the overall height and beam/cant proportions of Section 404 S20 for each building in the proposal;
2. a design rationale to support the request not addressed the Fort Langley Community Plan policies and the Heritage Conservation Area Guidelines.

This Integrated Site Design Concept (ISDC) for the above-named property submitted March 7, 2017 has been reviewed by staff and requires revision to address the following items:

- Please submit a revised ISBC that addresses these comments for review and ensure that the revised ISBC is coordinated with the comments provided herein regarding the overall development.

Township of  
**Langley**



Est. 1873

**MEMORANDUM**

TO: Teresa Hanson  
Development Planning

FROM: Dave Lind  
Development Engineering

DATE: AUGUST 1, 2017

PROJECT: 11-33-0126

SUBJECT: ENGINEERING COMMENTS FOR APPLICATION **RO100145** AT  
9181, 9199, 9205, 9213, & 9217 Glover Road and 9226 & 9232 McBride Street

In response to the above referenced Rezoning Application, the following information details the works and services required to support the proposed development at:

**9181, 9199, 9205, 9213, & 9217 Glover Road and 9226 & 9232 McBride Street**

**DEDICATIONS/RIGHTS-OF-WAY**

The Owner is to:

- Dedicate a varied widening along the north side of the existing lane adjacent the southern frontage of the subject site (McBride Lane) in order to accommodate its realignment with Mary Street opposite Glover Road.
- Dedicate to the Municipality a 5.0 m by 5.0 m corner truncation at the corner(s) of the proposed intersection of Glover Rd. and McBride Lane.
- Provide a 6.0m right-of-way/easement for public access through the site.

- Grant to the Municipality all necessary road dedications, statutory rights-of-way and easements over the said lands to accommodate the said works and, where the said works are located upon or under privately owned lands other than the said lands, to obtain at the Owner's expense, all necessary road dedications, statutory rights-of-way and easements over such lands in favour of the Municipality, to accommodate the said works.

Rights-of-way/easements for services, if required, are to be determined by the Development Engineering Department in conjunction with the Owner's Consulting Engineers.

**SERVICE EXTENSIONS (Non DCC Services)**

In addition to those services that now exist, the following works and services will be required to support the proposed development:

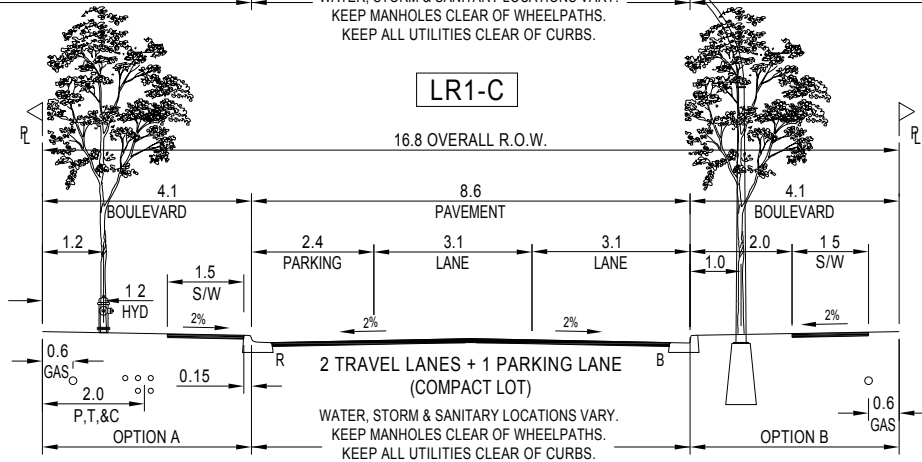
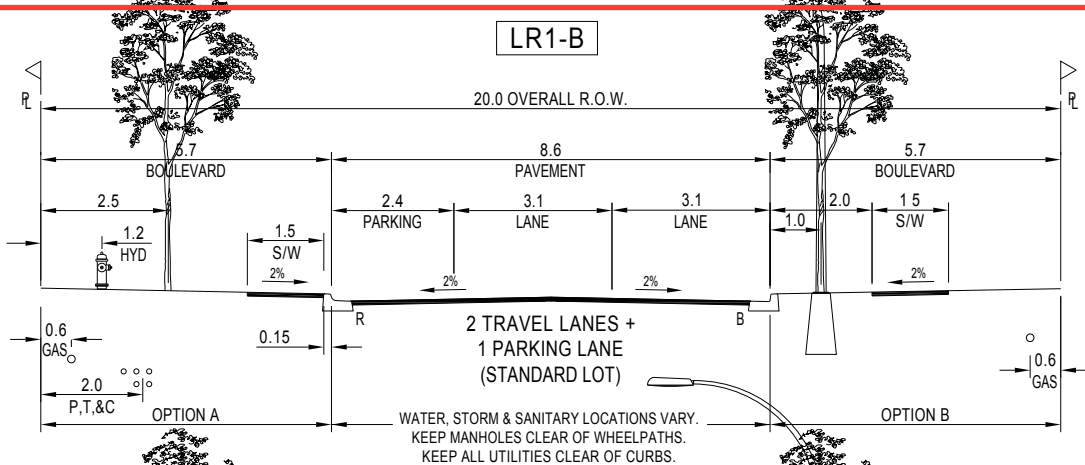
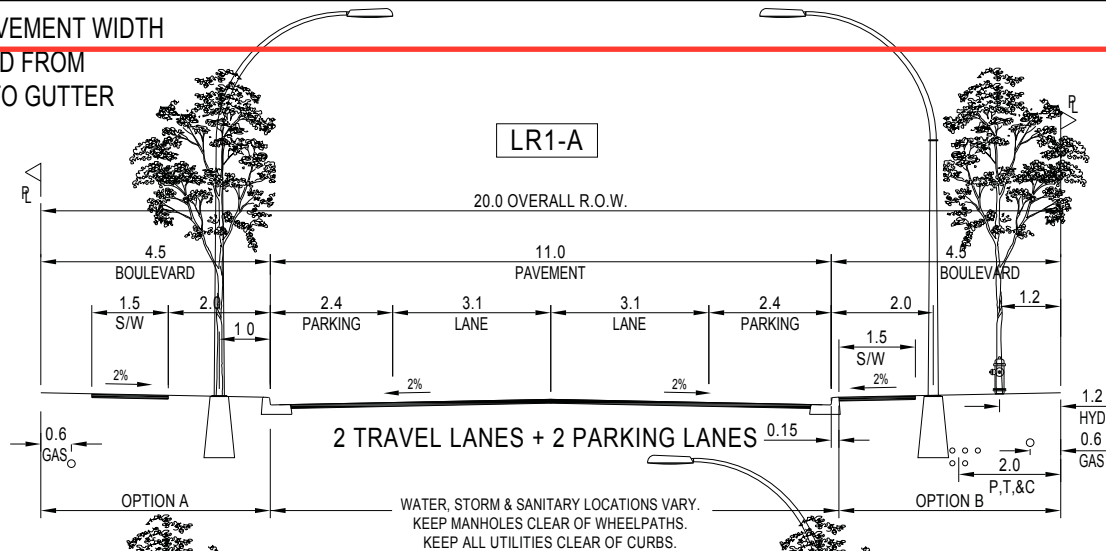
- The construction of the east half of McBride St. along the frontage of the property to Municipal Standard TLR8 / LR1-A (20.0 m ROW, 11.0 m asphalt).
- The construction of McBride Lane along the frontage of the property to Municipal Standard TLR8 / LR1-A (18.5 m ROW, 10.8 m asphalt).

**INFORMATION REQUIRED IN SUPPORT OF THE PROPOSED USE**

The Owner is to:

## TLR 8 – LOCAL RESIDENTIAL ROADS

NOTE: PAVEMENT WIDTH  
MEASURED FROM  
GUTTER TO GUTTER



ROAD CLASSIFICATION & TYPICAL ZONING	ROAD TYPE	No. OF S/W	ROW	PVMT	BLVD	CURB TYPE	OPERATIONAL CHARACTERISTICS
Local - Multifamily	LR1-A	2	20	11	4.5	B	parking both sides
Local - Standard lot Residential	LR1-B	2	20	8.6	5.7	R/B	parking one side
Local and Limited Local - Compact Lot	LR1-C	2	16.8	8.6	4.1	R/B	parking one side

TITLE

## LOCAL RESIDENTIAL ROADS

THE CORPORATION OF THE  
TOWNSHIP OF LANGLEY

DATE REV. AUG./09

APPROVED

S.D.S.B. No.



TLR 8



MCBRIDE  
STATION

