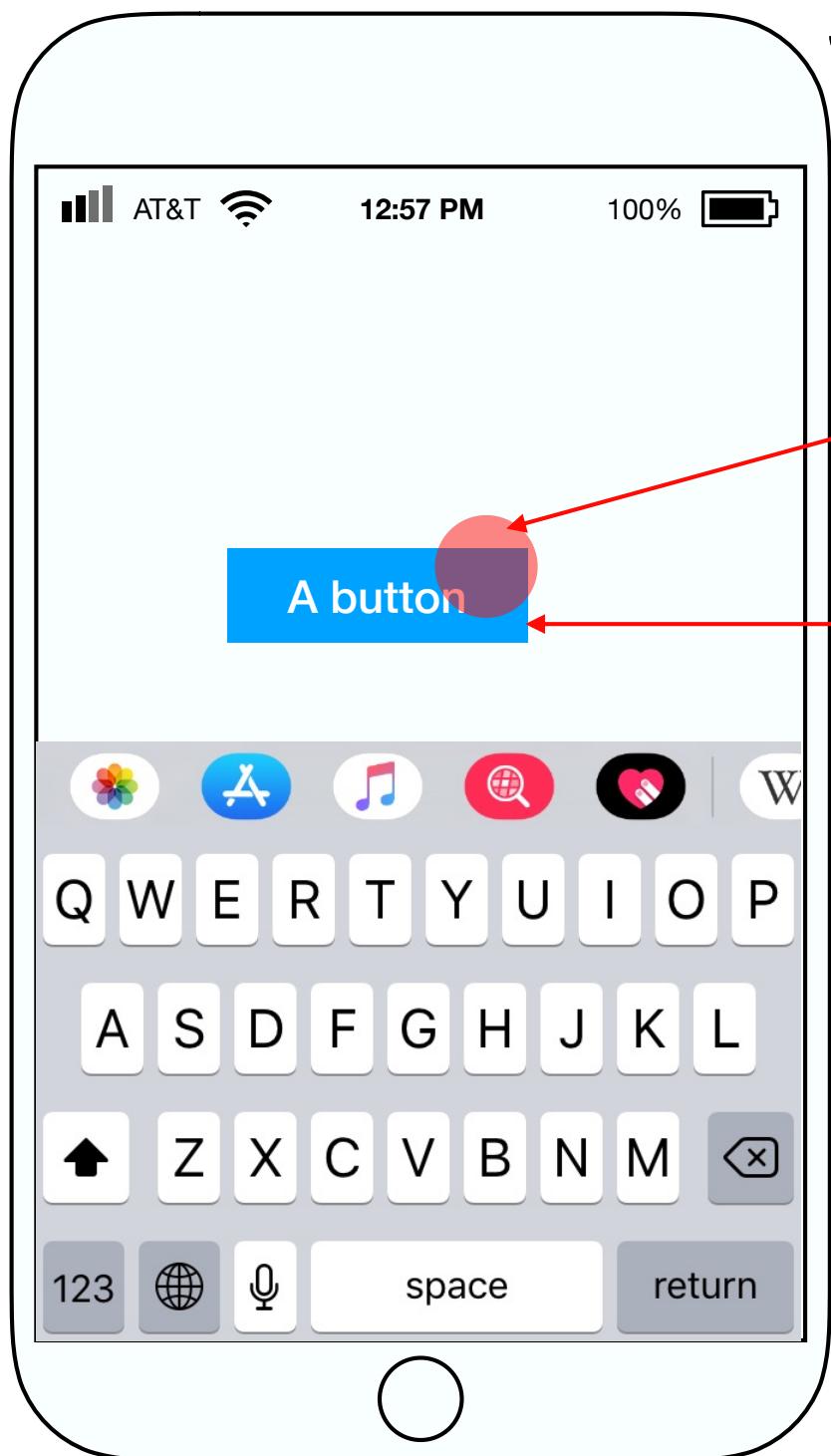


The webtag user interface should have the following features:

1. **No dependencies on any fields.** That is, each field can accept any type of text, including non-English alphabet. None of the fields is dependent on any other field being completed before being accessed. (For example, Time Arrive need not be filled in before Time Depart is filled in, even though the time arrived comes before departure time.)
2. **Easy to read.**
3. **No left-right scrolling.** Almost all users of mobile devices understand the need to scroll up or down, but it's awkward and unintuitive to expect users to scroll left or right.
4. **“Done” buttons to capture data as soon as possible.** Drivers might spend a lot of time between data entry. They want to know that if they're entering data to one field, it is actually captured (either in a cookie to push to the website or is directly sent to the website). They don't want to believe that they entered data, only to find out that it was never stored anywhere.
5. **Ability to write a note at any time.** Drivers often scribble comments on their paper tags, something like, “12:30pm, stuck in traffic on 101-North” or “scale broken, weight is estimated”. There should be a “Note” button that pops up a text field for the driver to fill out a short comment.
6. **Cues to orient the user on tag location.** On a physical paper, the driver looks at the upper left to know that's where the data about the truck number, name of Prime Carrier, etc. They know to look at the middle to see where to fill in the load/unload information. On a mobile device, where the window shows only the small part, it is hard to identify where on they are. We want to provide simple visual cues (much like the vertical scroll bar on a website) to help users know whether to scroll up or down to their desired location.

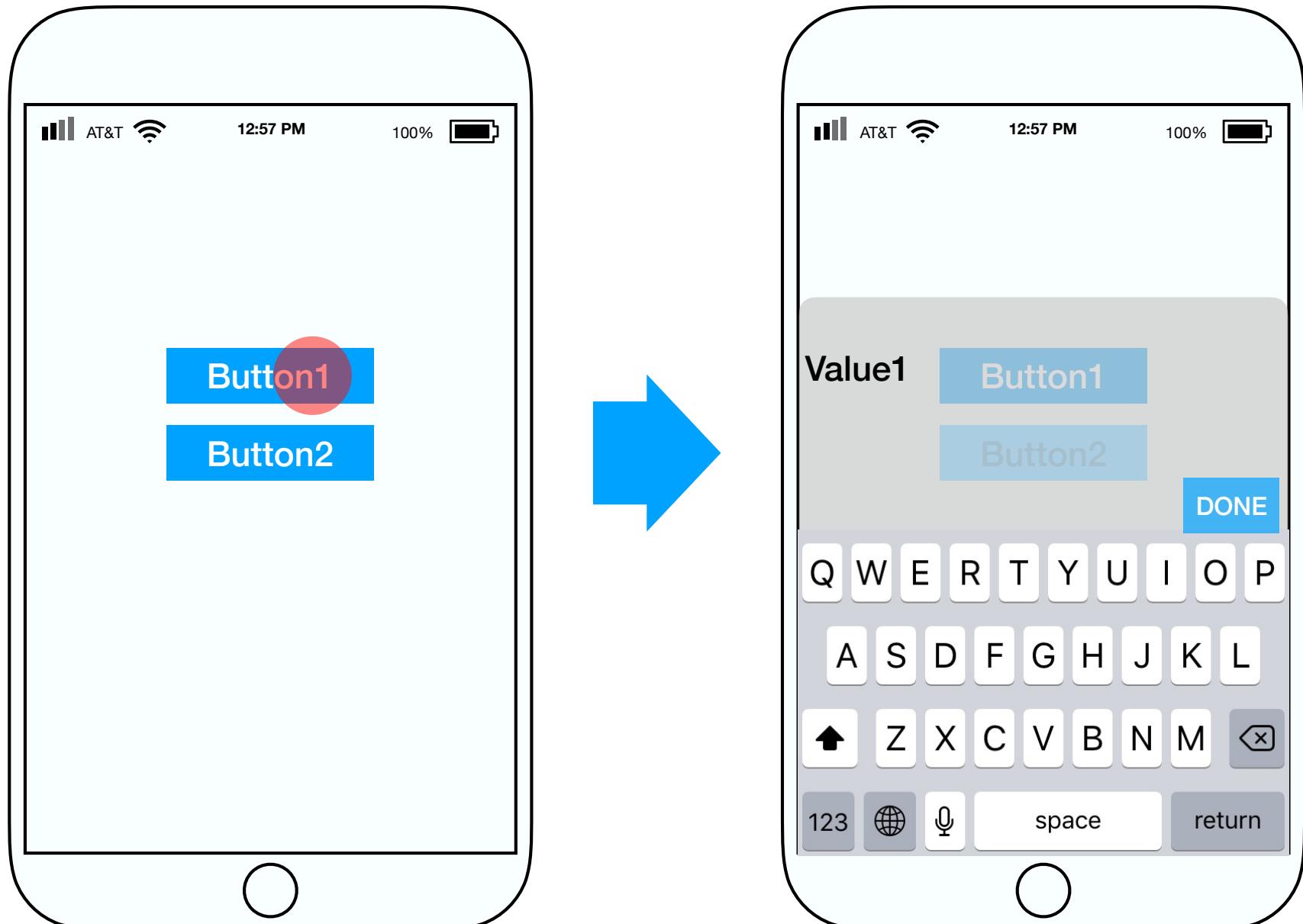
Some notations



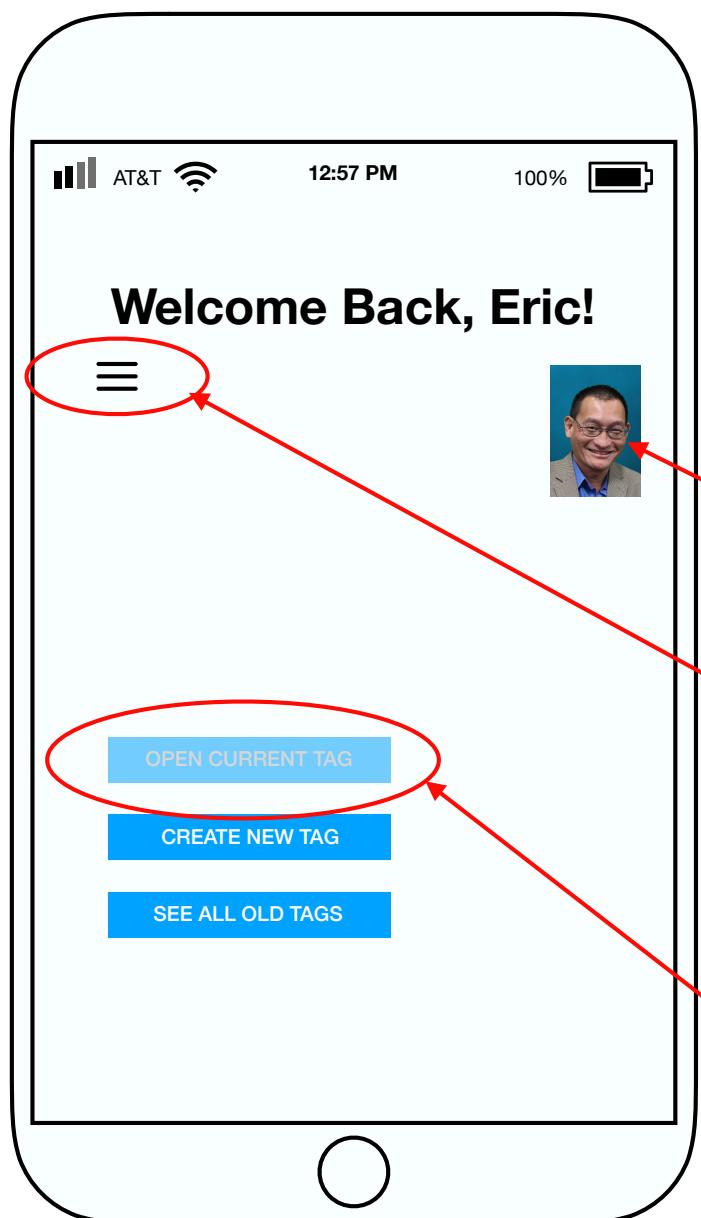
Red semi-transparent circle will represent a touch with a finger.

Buttons will be blue with white lettering in it.
Note: final button design can be of any color.

When Button1 is pressed, a text field pops up with half-alpha (semi-transparent). Button2 can be seen, but is even more muted to acknowledge that Button1 is the button that is being worked on. Upon finishing entering data for Button1, the user will hit the DONE button on the lower right corner and that should either enter the data assigned to Button1, or state that it is not possible to enter data at the moment (due to lack of wireless coverage or some other reason).



The user home page



When the user logs in, or after he has finished a tag, he should access his personal home page. The home page should offer three main items:

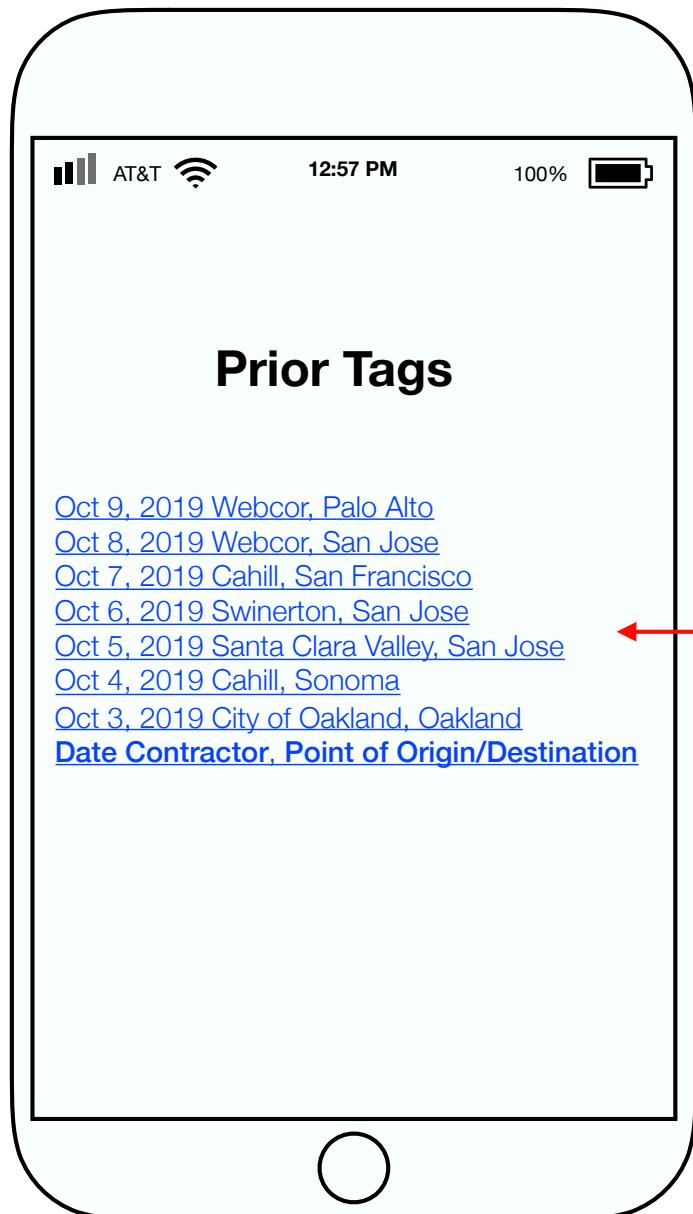
1. Return to a currently incomplete tag
2. Creation of a new tag
3. A list of prior tags in reverse chronological order (later: grouped monthly)

Menu icon opens up a menu offering the following:

1. Profile settings like photo, truck number, truck type and license plate
2. Change or exit credit card billing
3. Location/region of service
4. Any other settings

Grey out the “OPEN CURRENT TAG” button if there is no open tag available to open.

Clicking On The “See All Old Tags” Button

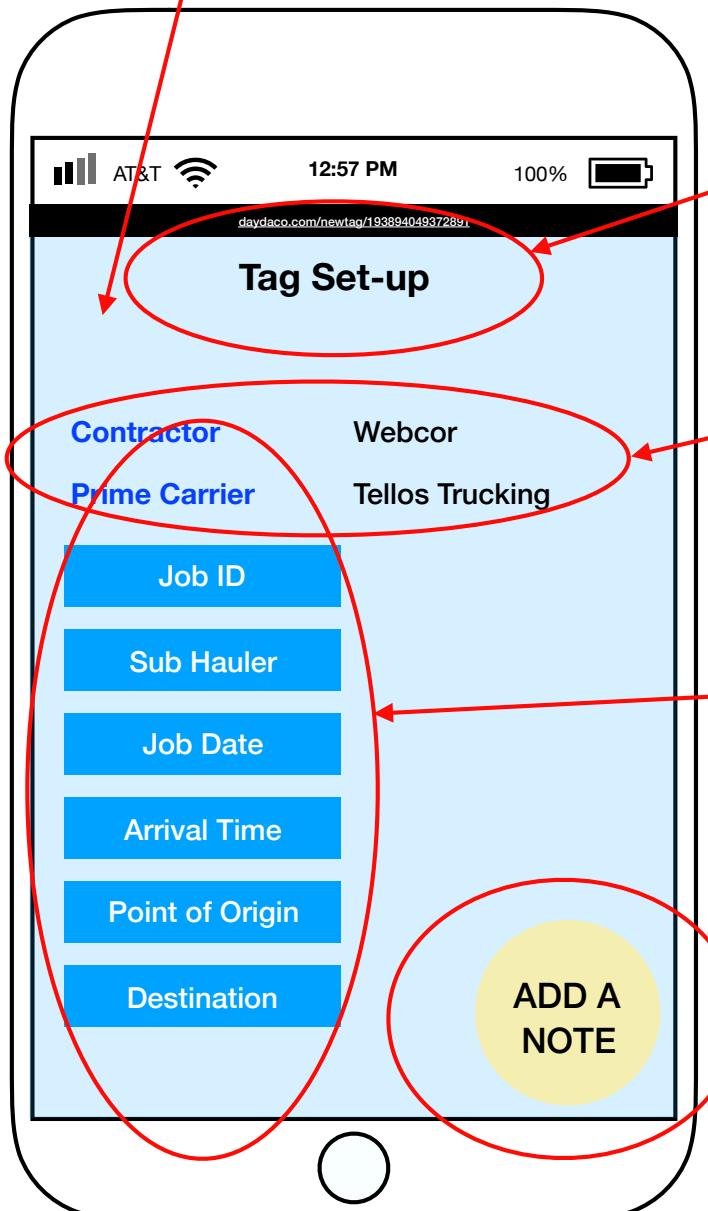


All prior tags that the driver has completed will be available for review. The list will be links to the formatted html or PDF page of the tag for that date, contractor, and location. It is possible that a driver might have more than one job (and hence, more than one tag) per day.

At the start, it will just be in reverse chronological order. Once we see usage and understand how the drivers want to use them, we can reconfigure the ordering to be more beneficial for the driver.

A New Webtag Being Used: The Set-up Stage

I'm using the blue semi-opaque background to orient the user to realizing that they're in the set-up stage of the tag. It may be redundant if we use the section label that hovers at the top.



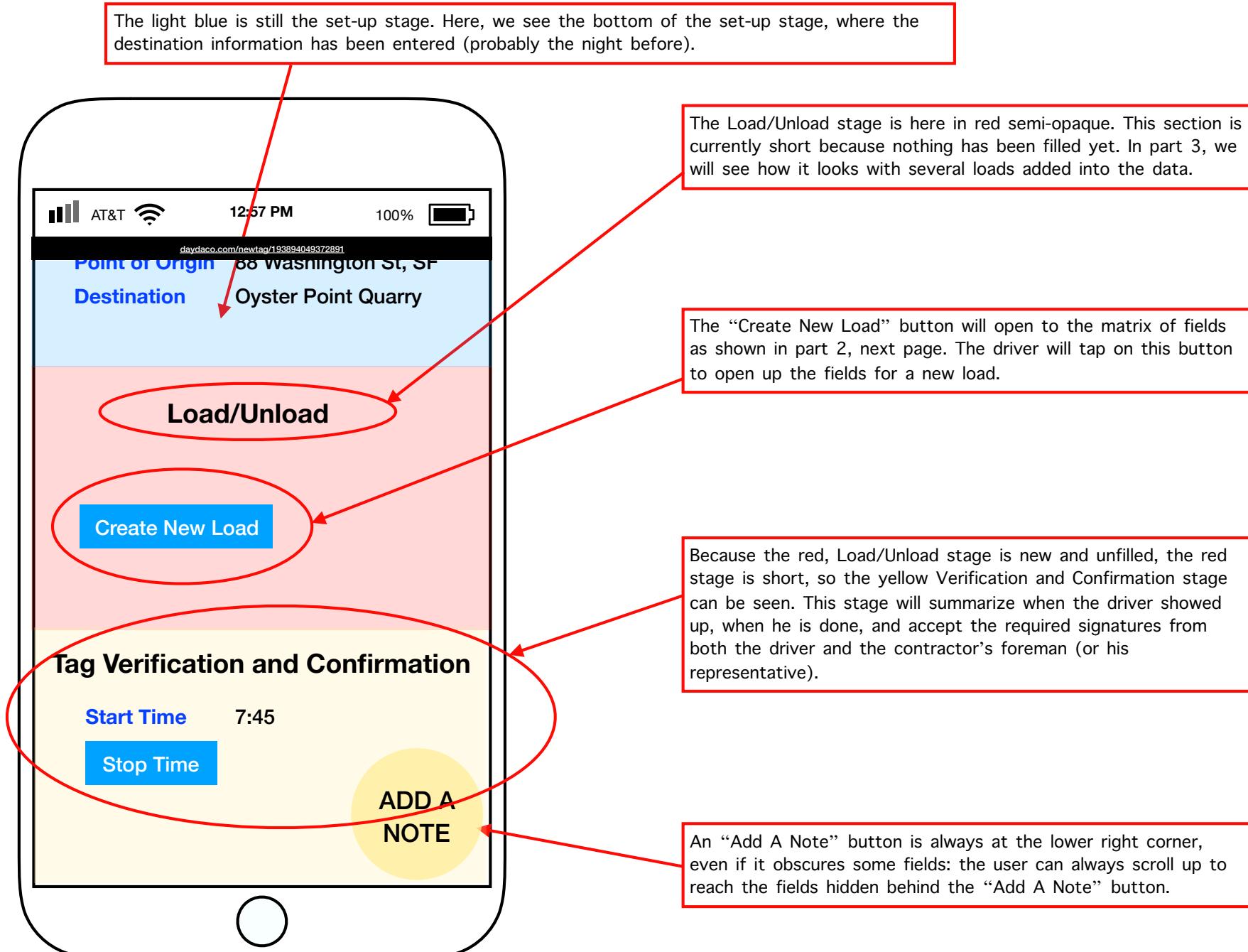
The name of the section should hover over the section. The beginning of the tag is the set up (initialization) where the driver inputs all the relevant information about the job: date, time, location, contractor, etc. The second section is Loading/Unloading and the third section is Verification/Confirmation

The contractor and Prime Carrier fields have already been entered, so the button disappears and turns into a label, and the value of the field is shown. It follows the standard **Label Value** format. Both the label and value can be tapped to allow editing of the value.

These buttons are not yet completed. Buttons can remain untouched and thus will have an empty value in the field. That should be acceptable. A user can also tap on a button and not fill it out. In that case, the button turns into a label and the value is empty.

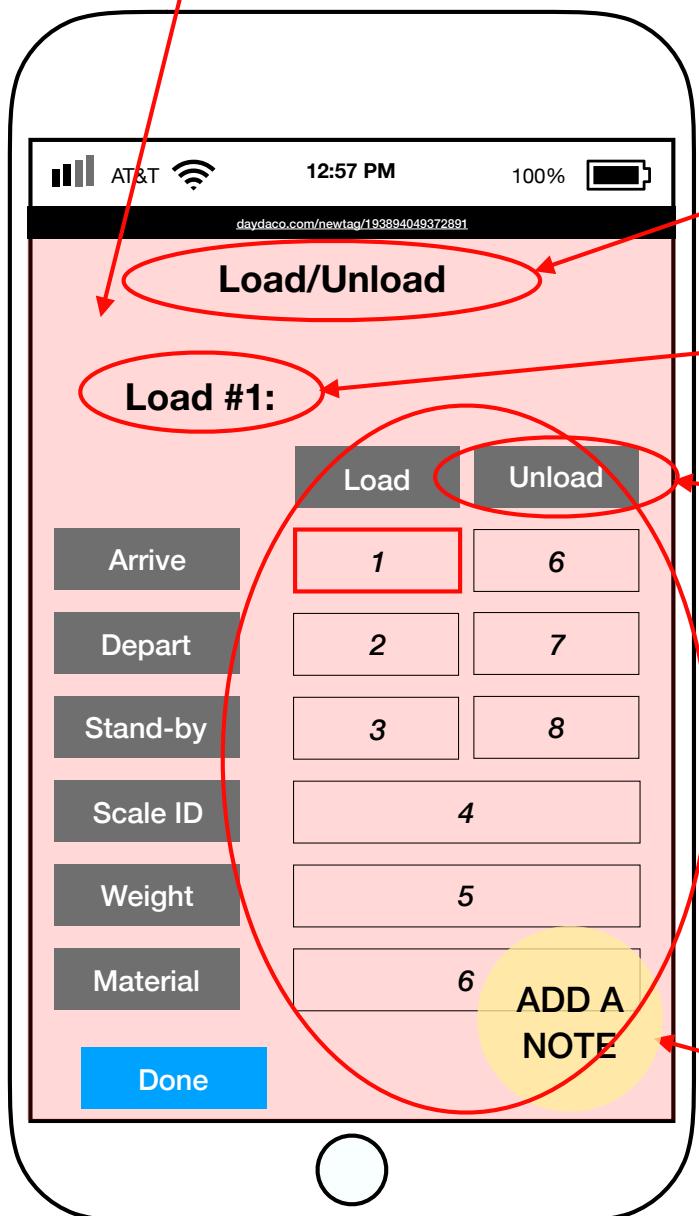
An “Add A Note” button is critical. It should be always present and allows the user to add any comment or note at any time. We will keep track of the notes and add them to the final document. Internally, we will time stamp the notes, but not print out the time stamp information.

The Load/Unload Stage - part 1



The Load/Unload Stage - part 2

I'm using the red semi-opaque background to orient the user to knowing that they're in the load/unload stage of the tag. It may be redundant if we use the section label that hovers at the top.



The name of the section should hover over the top of the section. The beginning of the tag is the set up (initialization) where the driver inputs all the relevant information about the job: date, time, location, contractor, etc. The second section is Loading/Unloading and the third section is Verification/Confirmation

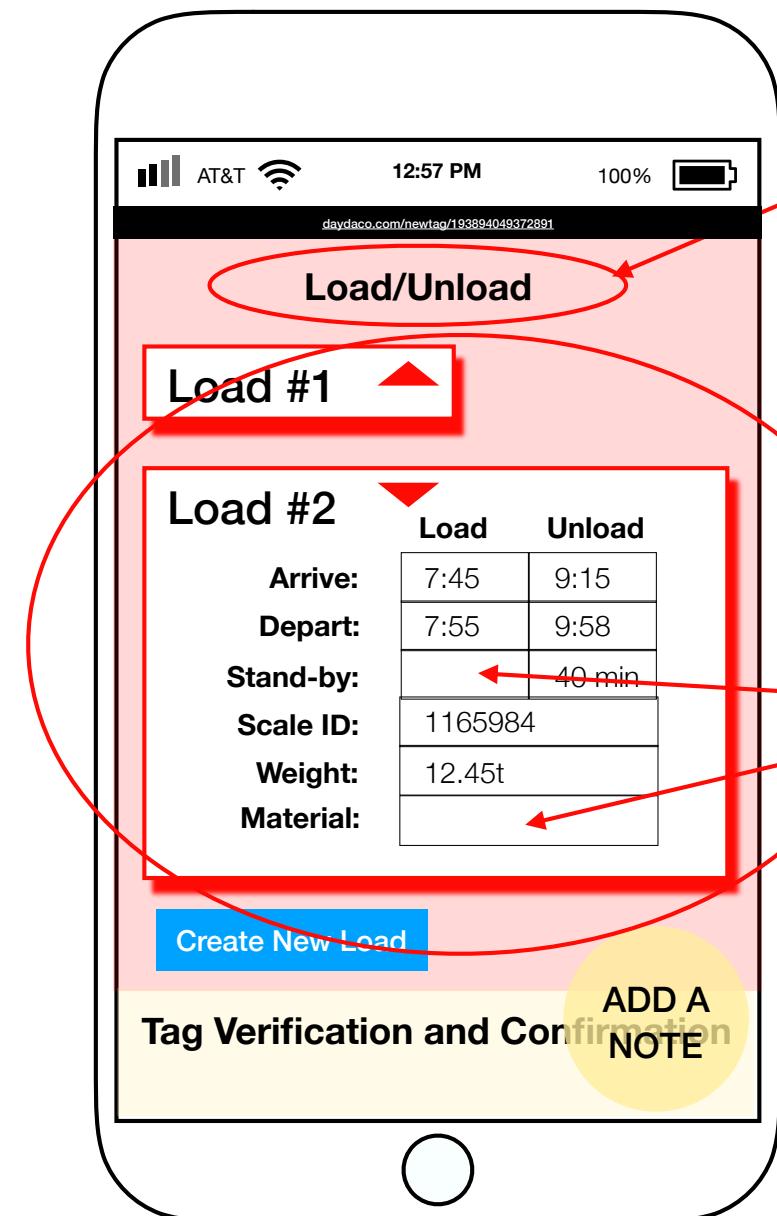
Identifies which load is being completed. The number ("1" in this example) will be automatically incremented whenever the user creates a new load.

The boxes for the labels are in grey to indicate they're not buttons, but just text highlighted using white text font color on top of darker background. The blue "Done" button on the lower left is in blue to indicate it's a button. When the "Done" button is pressed, a new load can be initiated.

The load/unload matrix of fields should be independent of each other, but to help guide the user, the most pertinent one should be highlighted. Here, it's highlighted in darker red with thicker line (2pt line versus 0.75pt black line for the other boxes). The user is not obligated to fill in the box that is highlighted, but the highlighting should help orient the user to know which field to fill in. Of course, any and all fields can be left blank. The driver might not get paid for an incomplete load record, but that's not for us to specify. The numbers in the boxes indicate the order they should be filled. That is, the order of the movement of the red box.

An "Add A Note" button is always at the lower right corner, even if it obscures some fields: the user can always scroll up to reach the fields hidden behind the "Add A Note" button.

The Load/Unload Stage - part 3



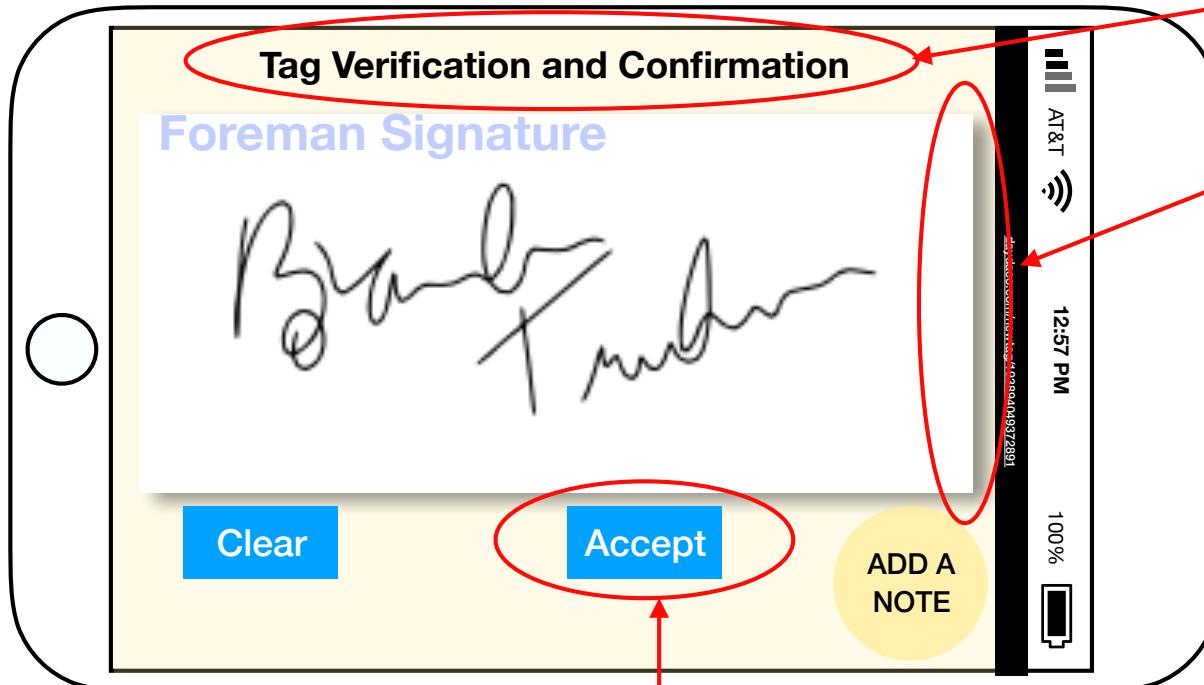
The title of the stage, “Load/Unload” in this case, should always float on top to tell the user where they are. We can revisit whether this is redundant, given that we have the color scheme as well.

The arrow closes up the details. The arrow opens up the load details. The user can open up all loads to see the contents of each load. At the moment, the times and other entered data are just plain text. We don't help the user set the time, but that would be a next step feature to add.

Note also that each field can be tapped and edited.

Note that empty fields are acceptable. It is never required that a field must be filled.

The Verification And Confirmation Stage



Title of the section, to help user know where they are. Also, the background color indicates they're in the verification stage.

Border around the signature field gives guidance to user where to sign.

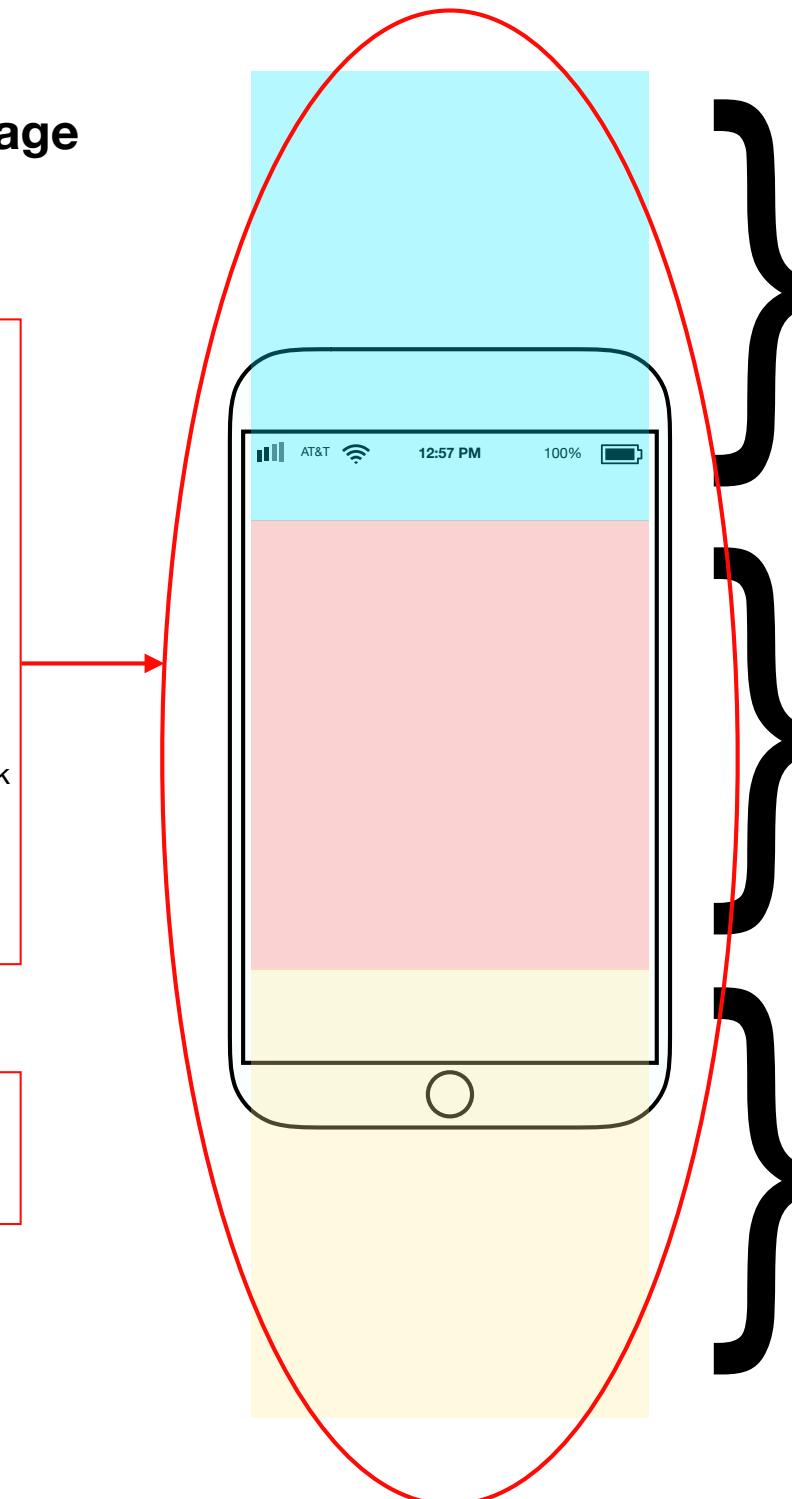
Once the foreman signs off and presses the Accept button, he will receive a URL link to the tag (either the html version or the PDF version) AS IS. Think of this as tearing off a copy of the paper tag to hand over to the foreman. After the Accept button is pressed, all fields are still editable, but any changes will be done in RED text color to differentiate them from any entries done prior to Accept button pressed. Any deletions after the Accept button is pressed will be replaced with a red square ■ to indicate that there was text/data, but has been deleted.

The “Create New Tag” Page

Consider the tag as a very long piece of paper (like the paper receipts for credit card purchasing). This long piece of paper is broken up into three sections, which I have color coded in blue, pink, and yellow. The sections are explained on the right.

Whereas the paper version of the tag arranges the fields in a standard way that works on a piece of paper, the form factor of a mobile device requires us to arrange the fields slightly differently. My suggestion for color coding the different sections is to help the user know where they are. Blue means they're at the initialization stage, where they should be entering data such as address and truck IDs. Pink means they're in the execution stage where they should be entering weight, Scale ID #, arrival and departure times. Yellow is for verification, where the contractor representative (possibly the foreman) will sign off, together with the driver.

NOTE: The color scheme concept is just a suggestion. The goal is to help the driver orient themselves on the device so they know whether to scroll up or down to find what they want to get to.



The set-up section: the driver's name, arrival location, contact person, Job ID, date, time, etc.

The load/unload section: loading, unloading, weight, material, time loading and unloading, any delays and tolls during the shift.

This section is where we want drivers to put in as many loads as he actually takes on. It may be one load, or it could be 10 loads. Or more.

The confirmation section: summary of the day's work, signature fields for driver and for foreman/site contact

Comparing To Two Tags With The Color Scheme For WebTag

S	M	T	W	T	F	S
20						
TRUCK #						
TRAILER TYPE						
SLB HAULER	PO. NO. 0084					
PRIME CARRIER	CONSIGNEE					
SHIPPER/CONTRACTOR	DESTINATION					
POINT OF ORIGIN	CITY					
CITY	MANIFEST NO.					
MATERIALS		LOADING		UNLOADING		
NO.	SCALE TAG NO.	YARD OR WEIGHT	TYPE OF MATERIAL	TIME ARRIVE	TIME LEAVE	
1						
2						
3						
4						
5						
6						
7						
8						
9						
10						
11						
12						
13						
14						
15						
START	STOP	DEDUCT TIME	NET TIME	BRIDGEFARE		
DRIVER		RECEIVED BY <input checked="" type="checkbox"/>				
Customer responsible for (A) checking Type & quality of materials before driver dumps load (B) Correct location of dumping (C) Overweight loads & court fines (D) Spills losses & cost for truck (E) Payment ground for dumping (F) Collection charges.						

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TRK LIC. NO. CIRCLE MON. TUES. WED. THUR. FRI. SAT. SUN.

TRUCK NO. ED HS SD ST TF IO LB BD FB DATE

UNDERLYING CARRIER BRIDGE TOLLS

POINT OF ORIGIN POINT OF DESTINATION

ADDRESS ADDRESS

CITY CITY

BILL TO JOB NO.

INITIAL HERE FOR EARLY START TAG NO. WEIGHT LOADING UNLOADING

1 ARRIVE DEPART ARRIVE DEPART

2

3

4

5

6

7

8

9

10

11

12

13 NUMBER OF AXLES TOTAL TONS:

COMMODITY TRANSPORTED

TIME DRIVER REPORTED FOR WORK TIME TRUCK COMPLETED WORK

DISPATCH TIME SIGN OUT TIME INITIAL HERE FOR NO LUNCH OVERALL TIME

TRAVEL TIME TARE DEDUCTIBLE TIME FOR MEALS OR FAILURE OF CARRIER EQUIPMENT

DRIVER'S SIGNATURE NET CHARGEABLE TIME

Driver has no liability for damage and cost resulting from his/her driving.

DEBTOR AGREED TO PAY ANY LEGAL FEES, COURT COSTS FOR COLLECTION OF DELINQUENT ACCOUNTS; LEGAL RATE OF INTEREST WILL BE CHARGED FOR ALL PAST DUE ACCOUNTS.

X CUSTOMER SIGNATURE

"Ownership of material transfers to buyer before loading on truck." "These charges include taxes paid to California cities instead of excise or business license taxes they could otherwise impose."

TOTAL

GREG'S Trucking Service, Inc. is NOT Responsible for ANY HAZARDOUS MATERIAL

T426530 (10/18)

FM GRAPHICS, INC. (916) 361-2183

The most common part is the red-shaded area, and that is also the most important part. The blue parts have some fields labeled with different names, but mean the same thing: Consignee = Bill to = Contractor. Prime Carrier = Broker = Underlying Carrier, and so on.

How Our Webtags Will Be Formatted

The webtag as presented must fit into a single 8.5 x 11 page when printed. Keep font sizes small enough to make sure everything fits.

Will need to determine which fields are really needed to be included, as some are never filled out.

Trucker, date/time, broker, contractor, sub hauler, Job ID

Leave space for broker logo (jpg/png/gif). Contractors sort their payments by the logo situated here.

Truck type, truck No., License plate, maybe photo image of driver

Point of Origin

Destination

All data entered after foreman signature (see page 10) and confirmation button pressed will be in red to highlight post-sign off entry. All other fields already modified are locked against further modification. Only untouched fields may be edited.

Material				Loading			Unloading		
No.	Scale Tag ID	Material	Weight	Arrive	Depart	Std By	Arrive	Depart	Std By
1	114983	gravel	14.5t	7:45	7:55		8:30	8:35	
2	116983	gravel	15.53t	9:13	9:18		10:03	10:55	50min
3	No Tag	dirt	1 load	11:10	11:15		11:53	11:58	
4	117745	gravel	14.53t	12:15	12:22		1:10	1:15	
5	No tag	dirt	1 load	1:33	1:40		2:22	2:30	

All Notes and time summaries, include bridge tolls, net time, deduct hours, etc.

Driver signature, Contractor/customer signature, waiver statements