



Brockville Municipal Accessibility Advisory Committee

Tuesday, June 23, 2015, 1:00 pm
City Hall, Boardroom

AGENDA

Page

Disclosure of Interest

Approval of the Agenda

THAT the Agenda for the June 23, 2015 meeting of Brockville Municipal Accessibility Advisory Committee be approved.

Chair's Remarks

Adoption of the Minutes

3 - 6 *THAT the minutes of the Brockville Municipal Accessibility Advisory Committee meeting dated May 26, 2015 be adopted as circulated.*

New Business Arising from the Minutes

Delegations/Presentations

Nil.

Correspondence & Communications

Nil.

Staff Reports

Nil.

New Business

- | | | |
|---------|---|---|
| 7 - 15 | 1 | Tactile sidewalks |
| | 2 | Summer meeting schedule |
| 16 - 25 | 3 | Brock Trail Extensions - Accessibility requirements |

Capital Budget - Review

Nil.

Sub-Committee and Member Reports/Project Updates

- | | | |
|---------|---|--|
| 26 - 28 | 1 | Wheelchair By-law |
| 29 - 31 | 2 | Brockville Gymnastics Academy - entrance doors |
| | 3 | Accessible Tables for parks |

Adjournment

THAT the BMAAC meeting be adjourned until its next regular meeting scheduled for July 28, 2015.

**Brockville Municipal
Accessibility Advisory
Committee**

Tuesday, May 26, 2015, 1:00 pm
City Hall, Boardroom

Committee Minutes

Roll Call:

Mr. Ryan Billing, Chair
Ms. Colleen Cooke
Ms. Mary Ann Greenwood
Mr. Doug Hone
Mr. Mel Watson

Absent:

Mr. Joseph Boisvenue
Councillor L. Bursey

Staff:

Ms. Lynn Murray, Deputy City Clerk (Recording Secretary)

DISCLOSURE OF INTEREST

Nil.

APPROVAL OF THE AGENDA

Moved by: Doug
Seconded by: Mary Ann

THAT the Agenda for the May 26, 2015 meeting of Brockville Municipal Accessibility Advisory Committee be approved.

CARRIED

CHAIR'S REMARKS

Ryan informed the Committee that Shannon Stilnovich has retired from the Committee due to the fact that she has taken a new job in Smiths Falls and therefore is not eligible to continue as a member.

ADOPTION OF THE MINUTES

Moved by: Mel
Seconded by: Doug

THAT the minutes of the Brockville Municipal Accessibility Advisory Committee meeting dated April 28, 2015 be adopted as amended.

CARRIED

NEW BUSINESS ARISING FROM THE MINUTES

Regarding the "Bus Stop Accessibility" Ryan said that he has sent the photos and database out via Drop Box this morning, for the Committee's reference. He confirmed that he had reviewed the Blue Route and 90% of the information is still accurate. Ryan would like to set up a meeting with Paul McMunn to review the extremely inaccessible spots. He welcomed any other members to join them and will forward them the date and time once it is set.

Ryan reported that he has passed the Committee's views on the Brock Trail upgrades over to Conal Cosgrove in Operations.

DELEGATIONS/PRESENTATIONS

1. Accessible Brockville - Mel

Mel reviewed his presentation for the group.

Ryan felt it would be a good idea for Mel to show this presentation at a Waterfront Committee meeting. He will contact them to hopefully be included on a future meeting agenda.

CORRESPONDENCE & COMMUNICATIONS

1. AODA 10th Anniversary Toolkit

Ryan stated that this would be a good opportunity to draw attention to what the committee has accomplished over the last ten years. He added that they could also take a report to Council to showcase their achievements.

2. Ontario Medal of Good Citizenship
(Ministry of Citizenship, Immigration and International Trade)

Ryan reviewed the attached request for nominations for the Committee.

STAFF REPORTS

Nil.

NEW BUSINESS

1. Wheelchair By-law - City of Cornwall

Ryan remarked that he had briefly reviewed this by-law. He encouraged the other Committee members to read the by-law over the next few weeks and then they will talk more about it at June meeting.

2. Brockville Gymnastics request

Ryan stated that the gymnastics academy has once again requested funds for the installation of automatic doors at the facility. He reminded the Committee that they had allocated funds probably around 2012 for the installation but the project fell through for various upgrades that were being done at the time. Ryan has asked them to obtain quotes and forward the previous quotes to possibly have the companies re-work the numbers. Ryan said he will bring the re worked quotes to the next meeting for a decision from BMAAC. He added that he made it clear that the Committee would only be interested in funding the installation of the doors.

CAPITAL BUDGET - REVIEW

No change

SUB-COMMITTEE AND MEMBER REPORTS/PROJECT UPDATES

Nil.

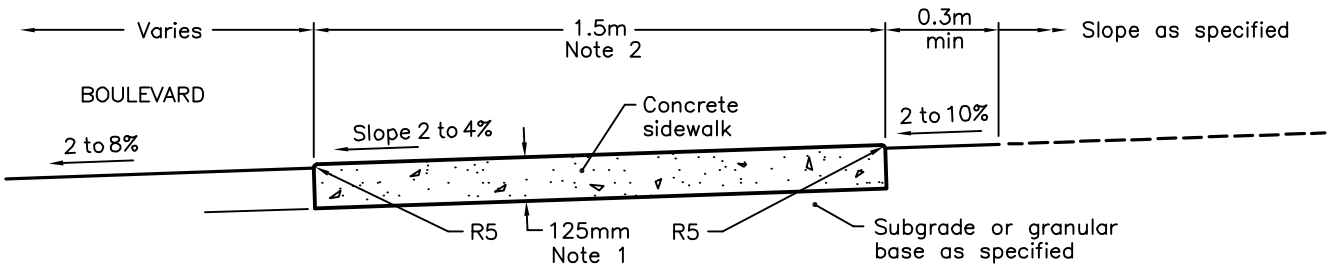
ADJOURNMENT

Moved by: Mary Ann
Seconded by: Colleen

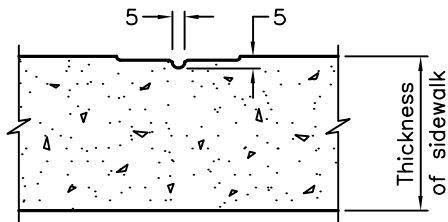
THAT the BMAAC meeting be adjourned until its next regular meeting scheduled for June 23, 2014.

CARRIED

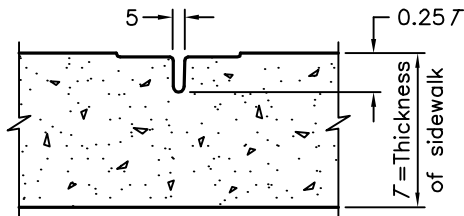
The meeting adjourned at 2:30 pm.



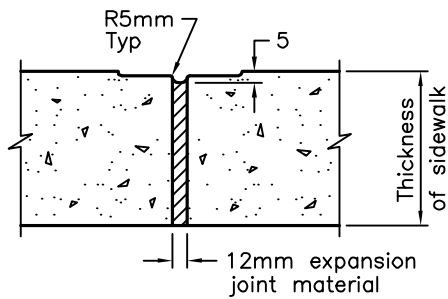
TYPICAL SECTION



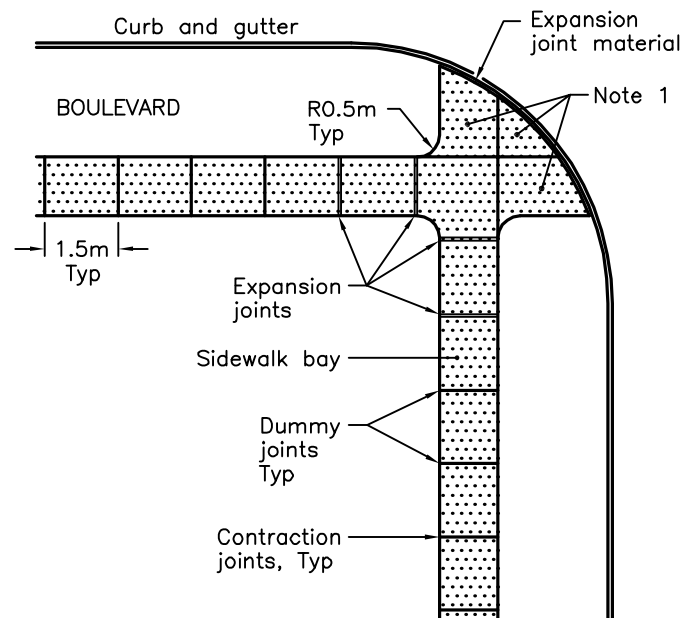
DUMMY JOINT



CONTRACTION JOINT



EXPANSION JOINT



JOINT LAYOUT

NOTES:

- 1 Sidewalk thickness at residential driveways and adjacent to curb shall be 150mm. At commercial and industrial driveways, the thickness shall be 200mm.
- 2 Sidewalk width shall be increased to 2.4m at schools, bus stops, and other high pedestrian areas.

A This OPSD shall be read in conjunction with OPSD 310.030, 310.031, 310.032, 310.033 and 310.039.

B All dimensions are in millimetres unless otherwise shown.

THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

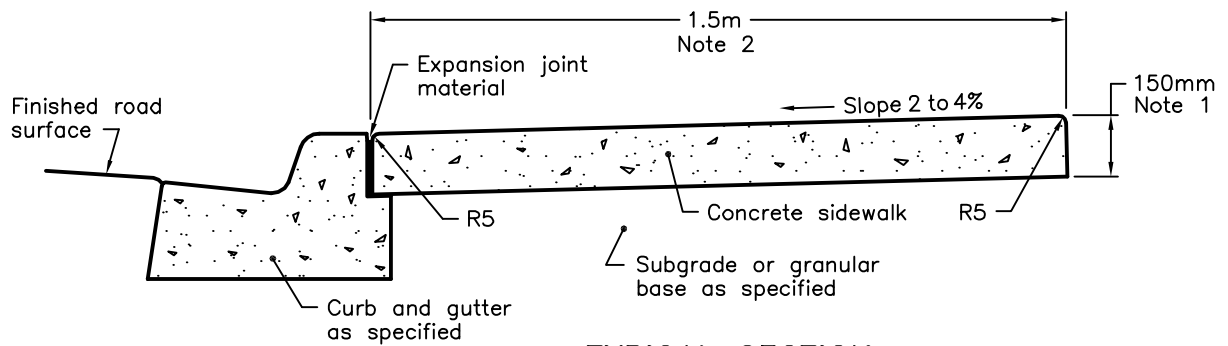
CONCRETE SIDEWALK

DRAFT

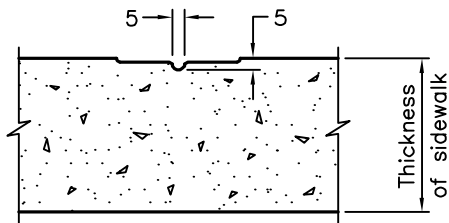
2015 05 01

D 310.010-0

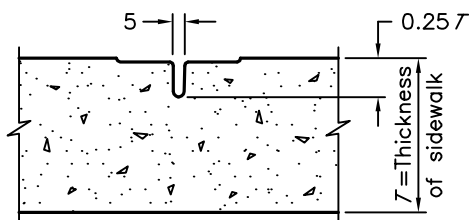
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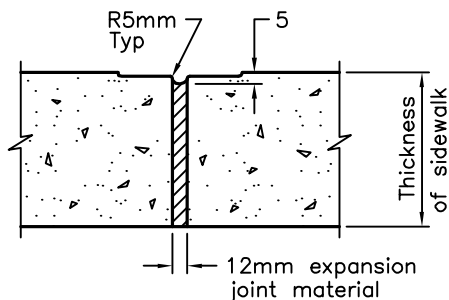
TYPICAL SECTION



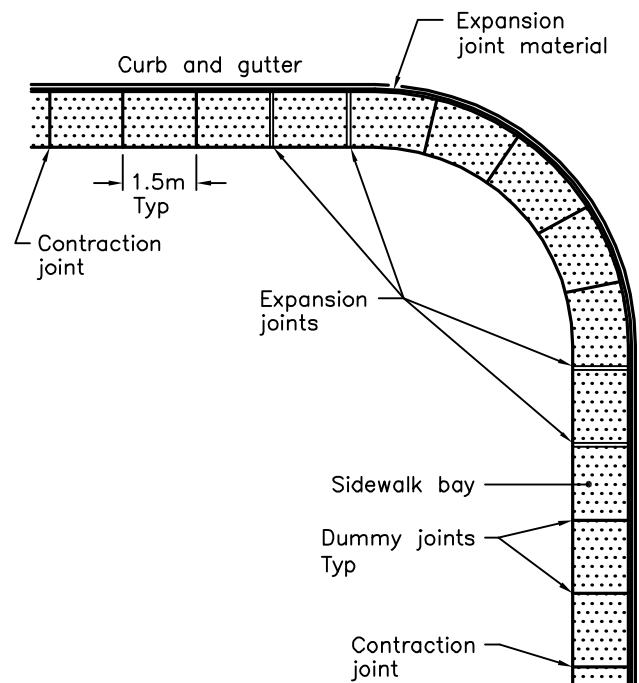
DUMMY JOINT



CONTRACTION JOINT



EXPANSION JOINT



JOINT LAYOUT

NOTES:

- 1 At commercial and industrial driveways, the thickness shall be 200mm.
- 2 Sidewalk width shall be increased to:
 - 1.8m on major roadways
 - 2.4m at schools, bus stops, and other high pedestrian areas.

A This OPSD shall be read in conjunction with OPSD 310.030, 310.031, 310.032, 310.033 and 310.039.

B All dimensions are in millimetres unless otherwise shown.

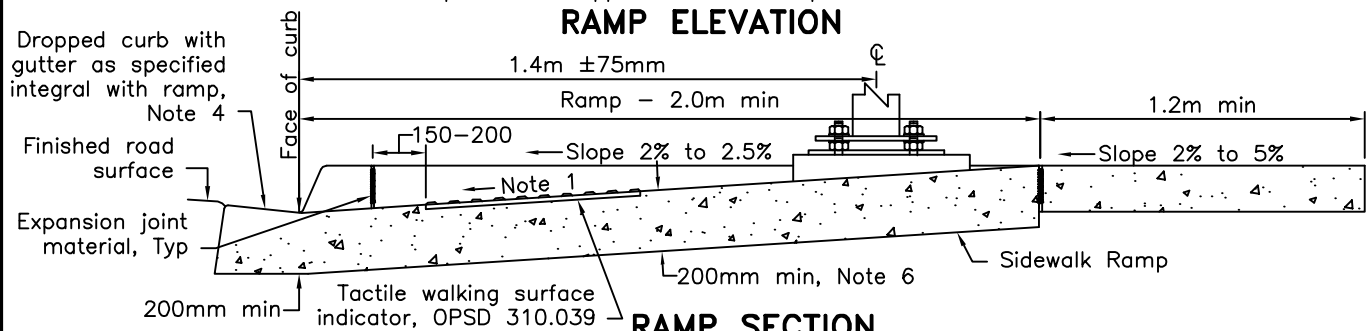
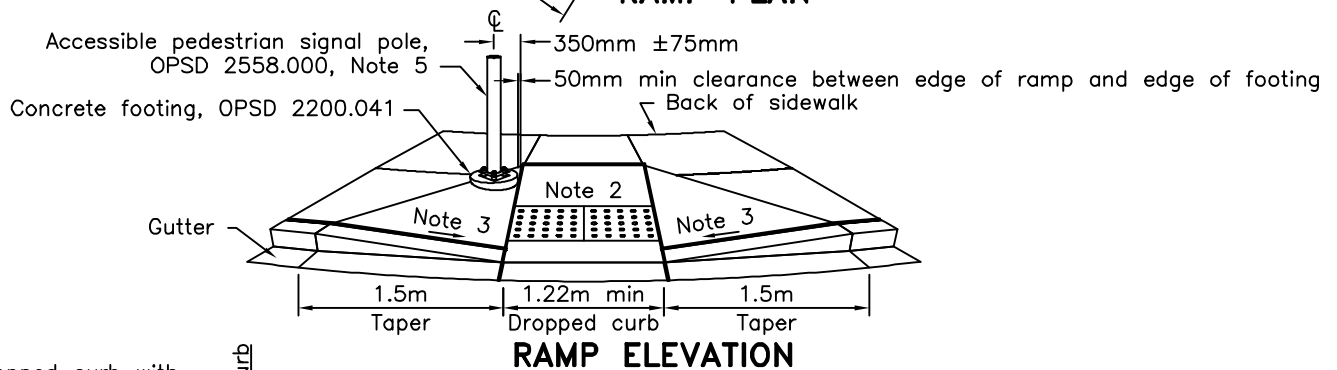
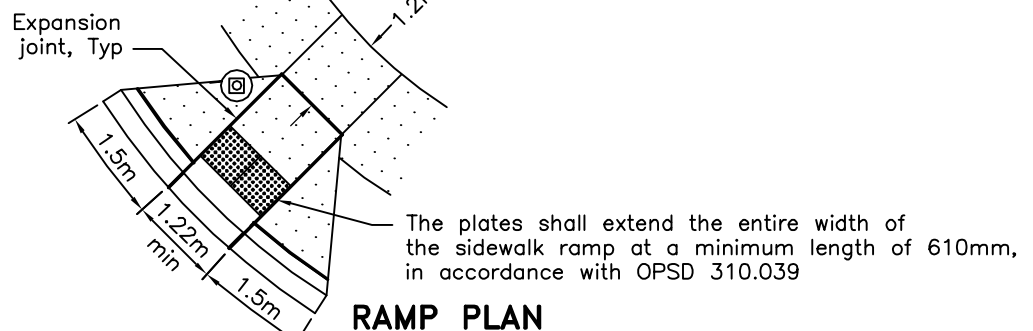
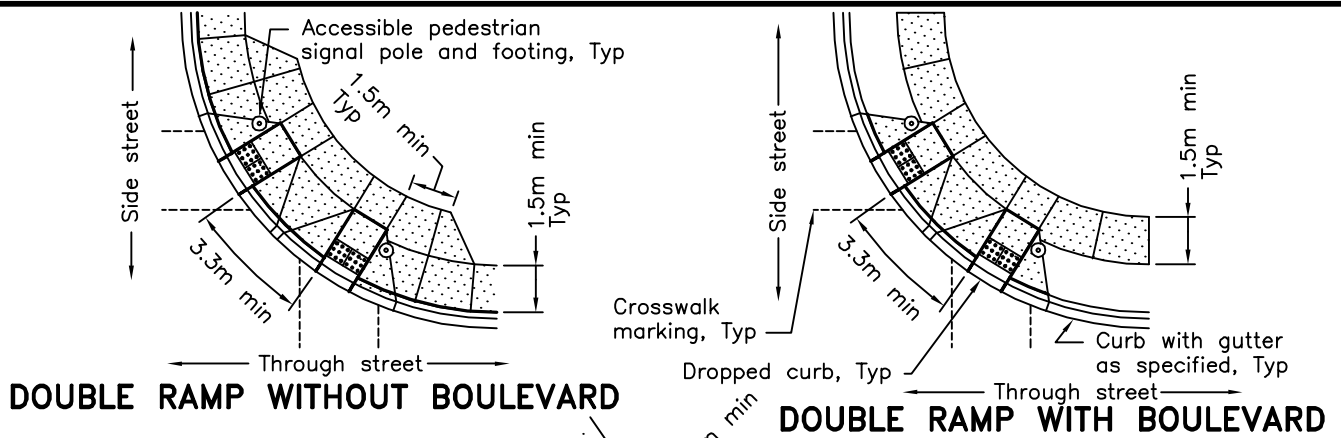
THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

**CONCRETE SIDEWALK
ADJACENT TO CURB AND GUTTER**

**DRAFT
2015 05 01**

D 310.020-0

2



NOTES:

- 1 Slope of ramp shall not exceed 8%.
 - 2 Cross slope of ramp shall not exceed 2% in either direction.
 - 3 Cross slope of flared side of ramp shall not exceed 8%.
 - 4 Dropped curb at ramp shall be modified to eliminate 30 mm step at gutter.
 - 5 Accessible pedestrian signal to have tactile arrow that aligns with direction of crosswalk.
 - 6 Minimum thickness of ramp is 200mm. Minimum thickness of sidewalk and flared sides adjacent to ramp is 150mm.
- A Where only one crosswalk is present at an intersection, only one curb ramp is required.
 B All dimensions are in millimetres unless otherwise shown.

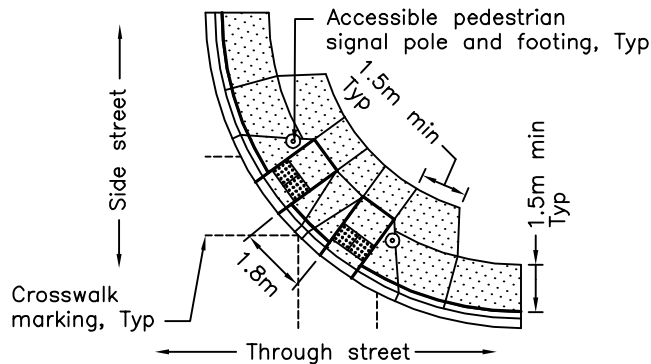
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CONCRETE SIDEWALK RAMPS AT SIGNALIZED INTERSECTIONS

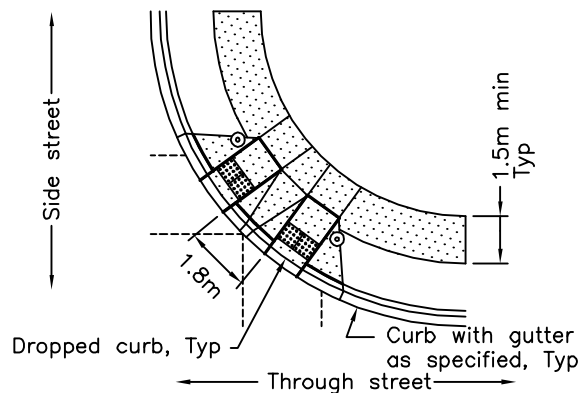
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2015 05 01

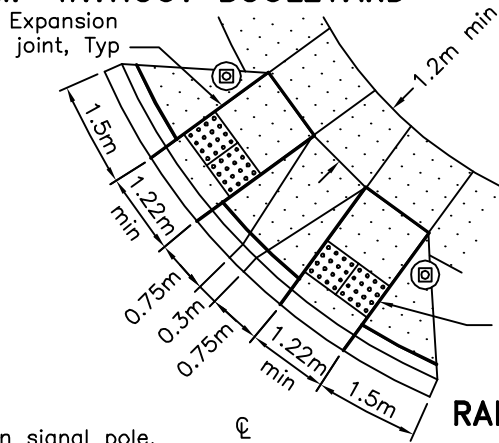
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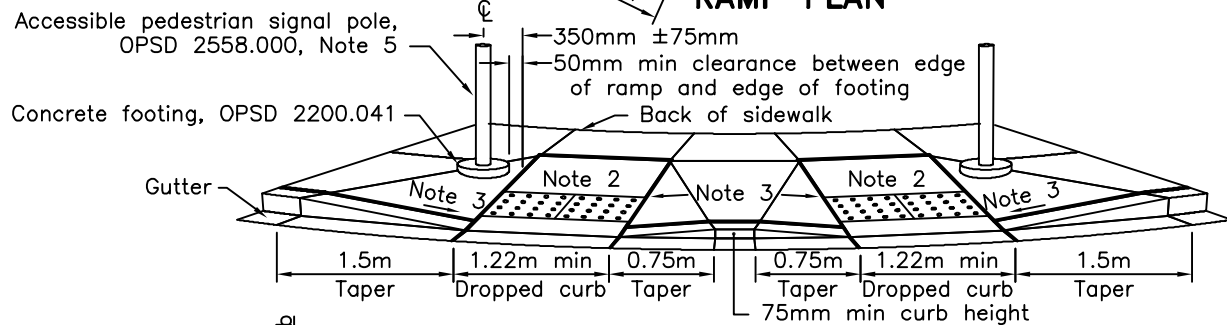
DOUBLE RAMP WITHOUT BOULEVARD



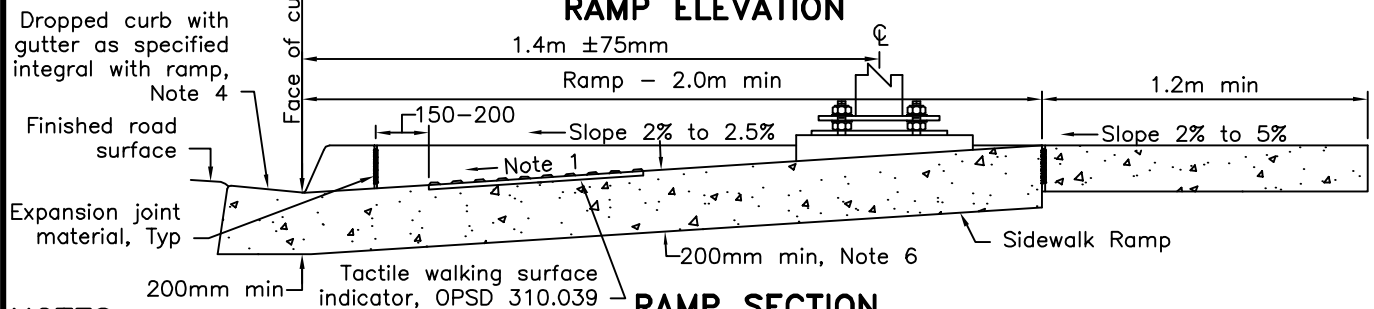
DOUBLE RAMP WITH BOULEVARD



RAMP PLAN



RAMP ELEVATION



RAMP SECTION

NOTES:

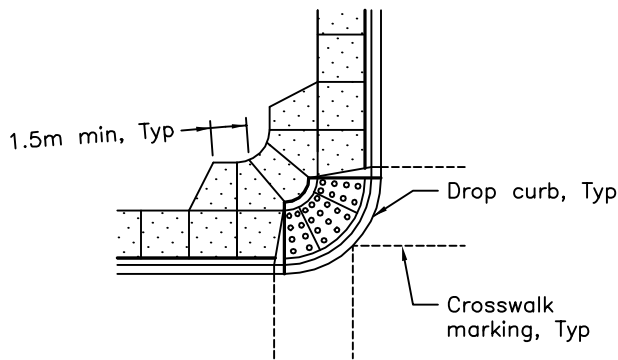
- 1 Slope of ramp shall not exceed 8%.
- 2 Cross slope of ramp shall not exceed 2% in either direction.
- 3 Cross slope of flared side of ramp shall not exceed 8%.
- 4 Dropped curb at ramp shall be modified to eliminate 30 mm step at gutter line.
- 5 Accessible pedestrian signal to have tactile arrow that aligns with direction of crosswalk.
- 6 Minimum thickness of ramp is 200mm. Minimum thickness of sidewalk and flared sides adjacent to ramp is 150mm.
- A All dimensions are in millimetres unless otherwise shown.

THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

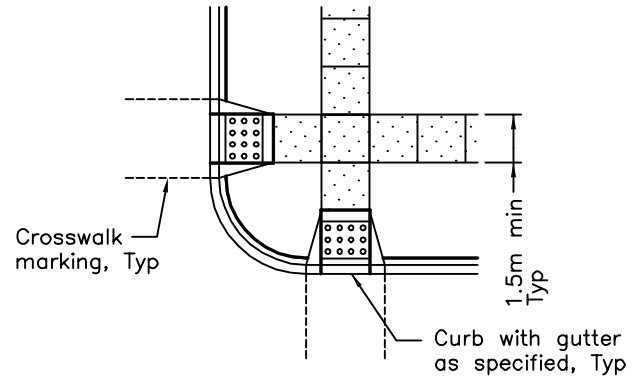
**CONCRETE SIDEWALK RAMPS AT
SIGNALIZED INTERSECTIONS WITH
INTERSECTING CROSSWALKS**

**DRAFT
2015 05 01**

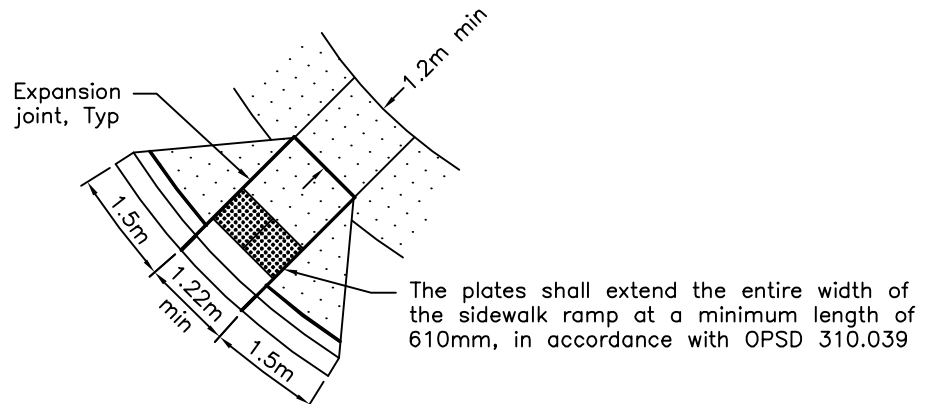
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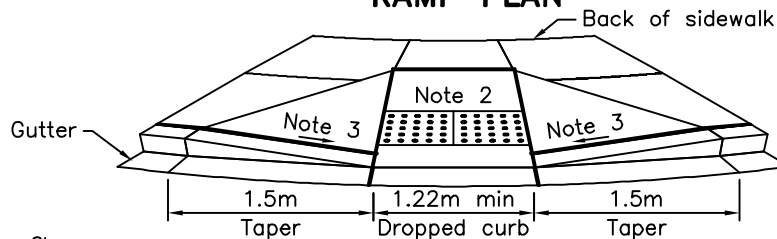
RAMPS WITHOUT BOULEVARD



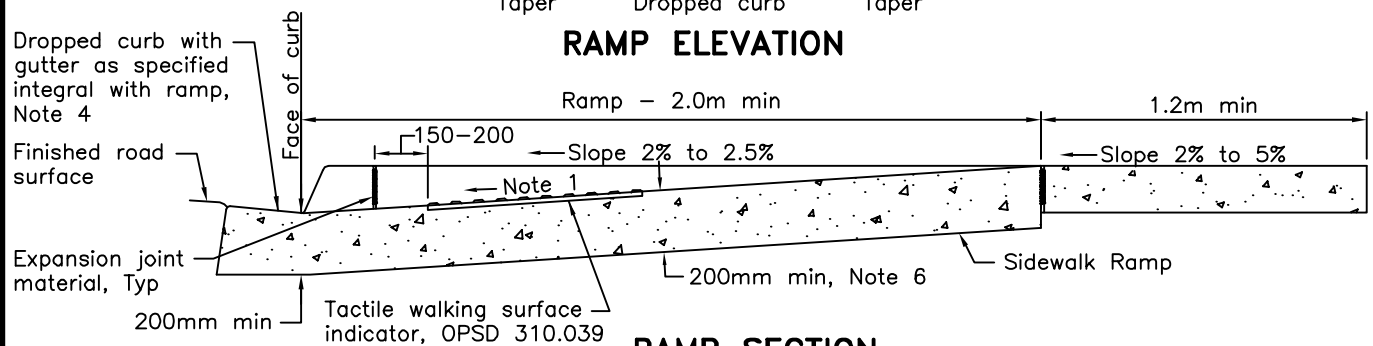
RAMPS WITH BOULEVARD



RAMP PLAN



RAMP ELEVATION



RAMP SECTION

NOTES:

- 1 Slope of ramp shall not exceed 8%.
- 2 Cross slope of ramp shall not exceed 2% in either direction.
- 3 Cross slope of flared side of ramp shall not exceed 10%.
- 4 Dropped curb at ramp shall be modified to eliminate 30 mm step at gutter line.
- 5 Minimum thickness of ramp is 200mm. Minimum thickness of sidewalk and flared sides adjacent to ramp is 150mm.
- A All dimensions are in millimetres unless otherwise shown.

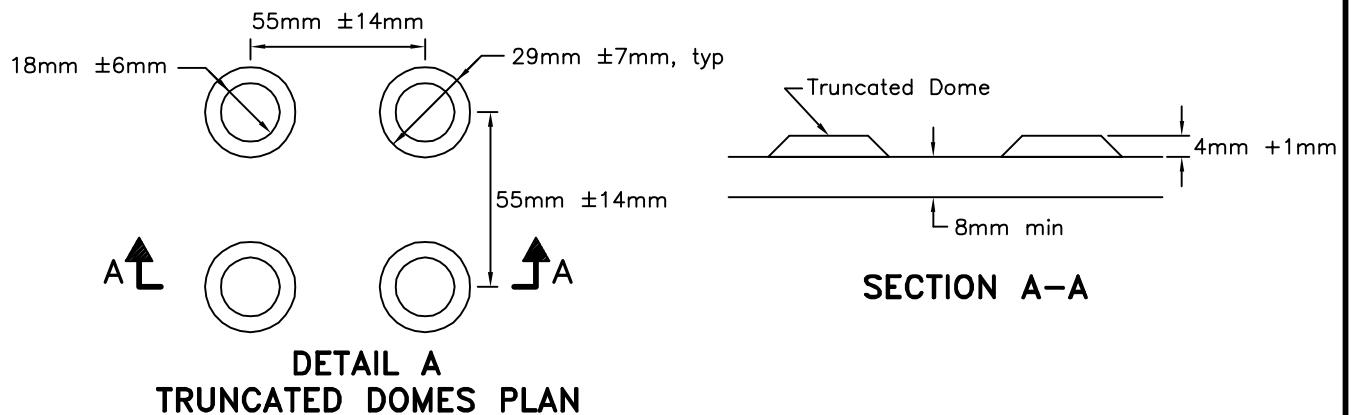
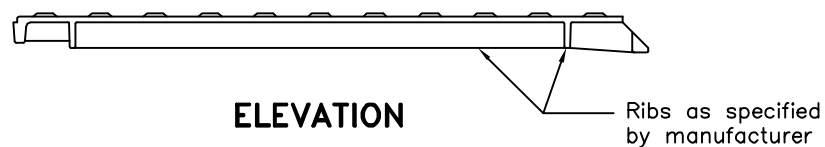
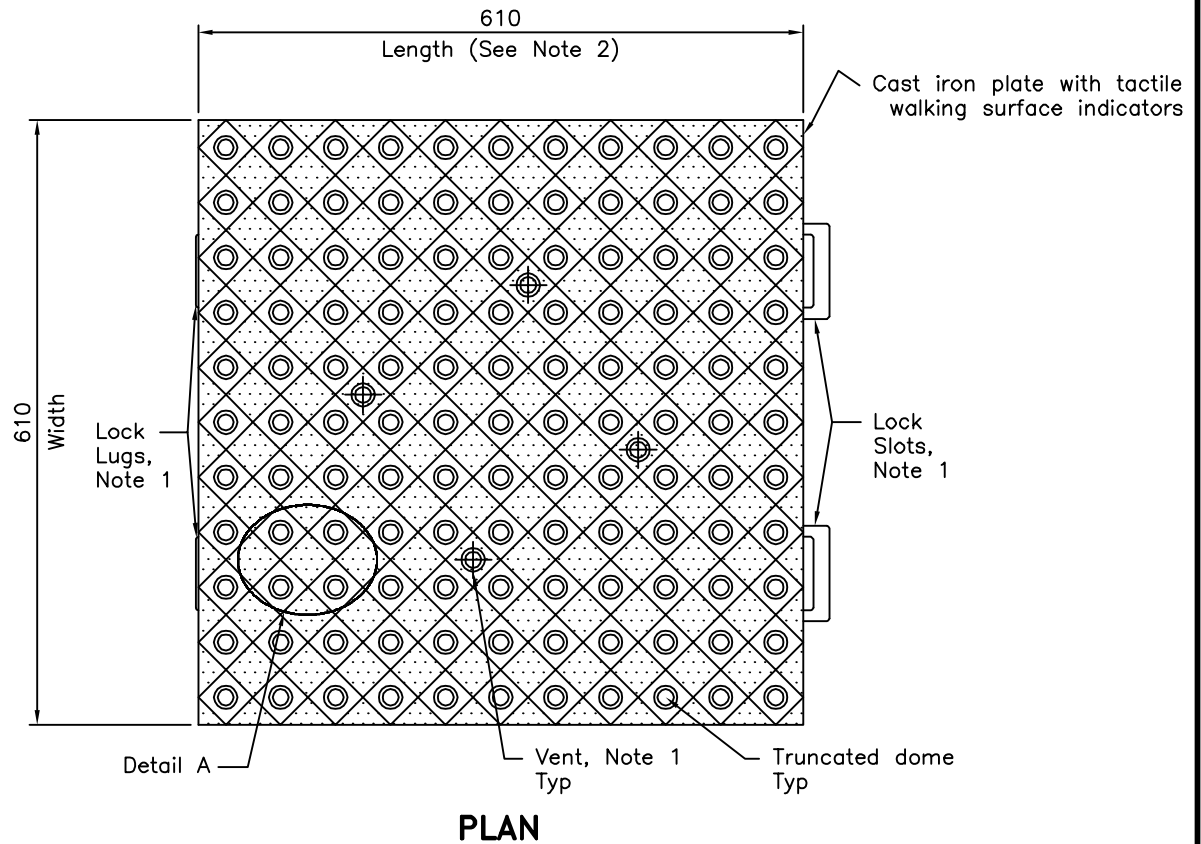
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**CONCRETE SIDEWALK RAMPS AT
UNSIGNALIZED INTERSECTIONS**

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2015 05 29

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NOTES:

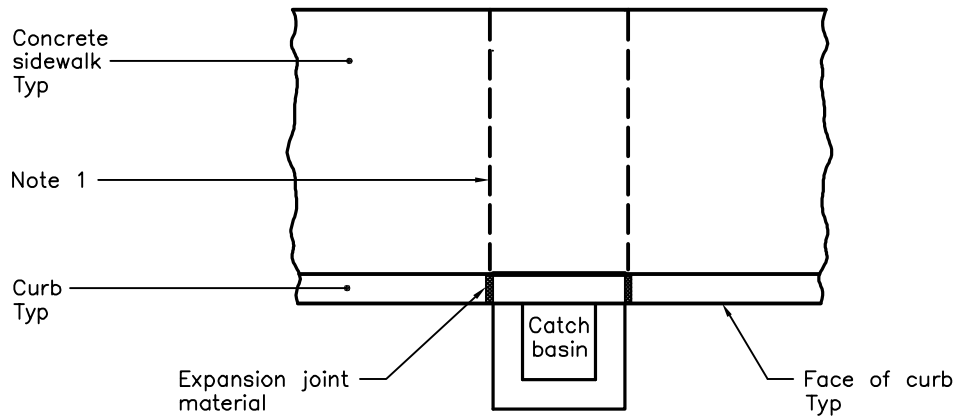
- 1 Lock lug and slots to interconnect adjacent cast iron plates, and vents, are proprietary to manufacturer
- 2 Length of plate may be increased to suit the curb depression width.
- A The coefficient of friction of the cast iron plate shall not be less than 0.8.
- B All dimensions are in millimetres unless otherwise shown.

THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

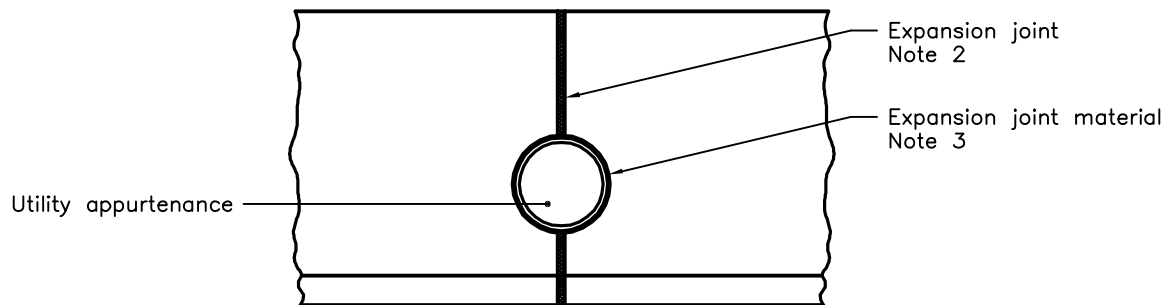
**CONCRETE SIDEWALK RAMPS
TACTILE WALKING SURFACE
INDICATORS COMPONENT**

**DRAFT
2015 05 01**

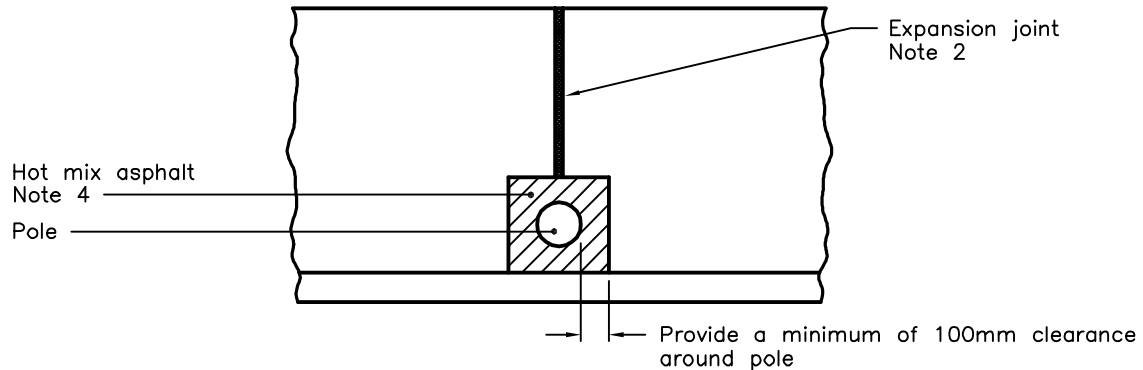
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CATCH BASIN



UTILITY APPURTENANCE



UTILITY POLE

NOTES:

- | | |
|--|---|
| <p>1 Expansion joint through sidewalk is required when curb and gutter is poured integral with sidewalk.</p> <p>2 Adjust joints to coincide with centre of utility, with minimum slab length of 1m.</p> <p>3 Expansion joint material shall be placed around Utility appurtenance flush with concrete surface.</p> | <p>4 For concrete alternative use expansion joint material around boxout.</p> <p>A For expansion joint detail, see OPSD-310.010.</p> <p>B All dimensions are in millimetres unless otherwise shown.</p> |
|--|---|

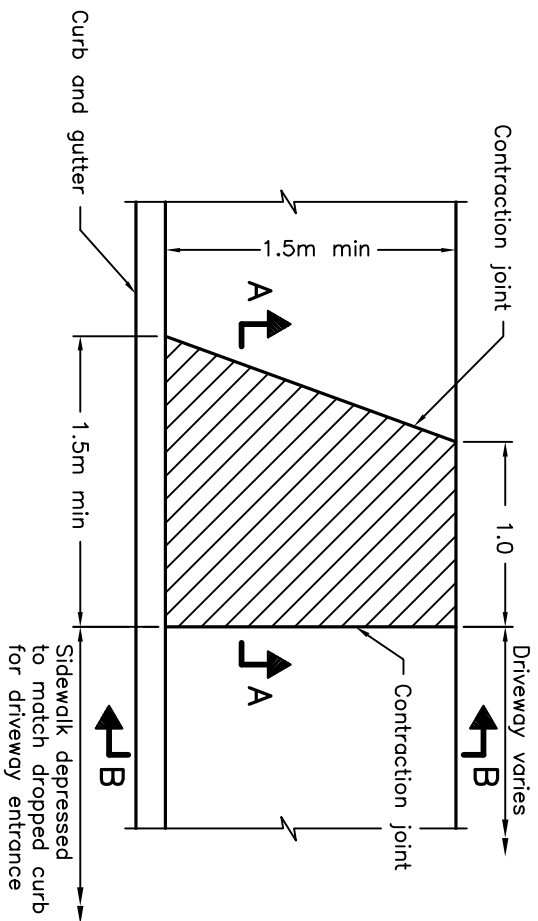
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**UTILITY ISOLATION
IN CONCRETE SIDEWALKS**

**DRAFT
2015 05 01**

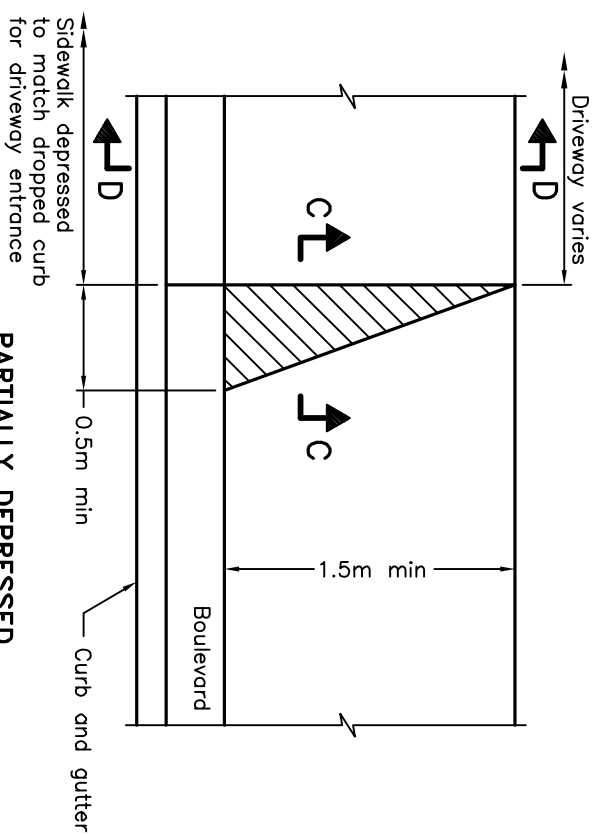
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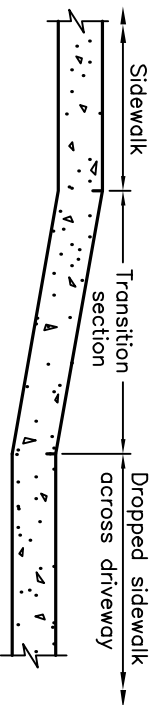


FULLY DEPRESSED

SIDEWALK AT DRIVEWAY ENTRANCE—PLAN

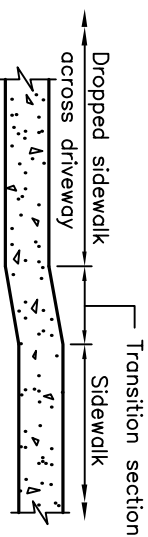
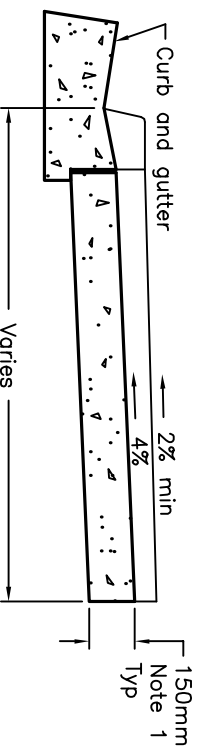


PARTIALLY DEPRESSED



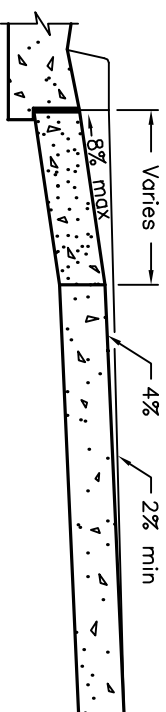
SIDEWALK WITHOUT BOULEVARD

SECTION A-A



SIDEWALK WITH BOULEVARD

SECTION C-C



SECTION B-B

SECTION D-D

NOTES:

1 At commercial and industrial driveways, the thickness shall be 200mm.

A For contraction joint detail, see OPD-310.010.

B All dimensions are in millimetres unless otherwise shown.

THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

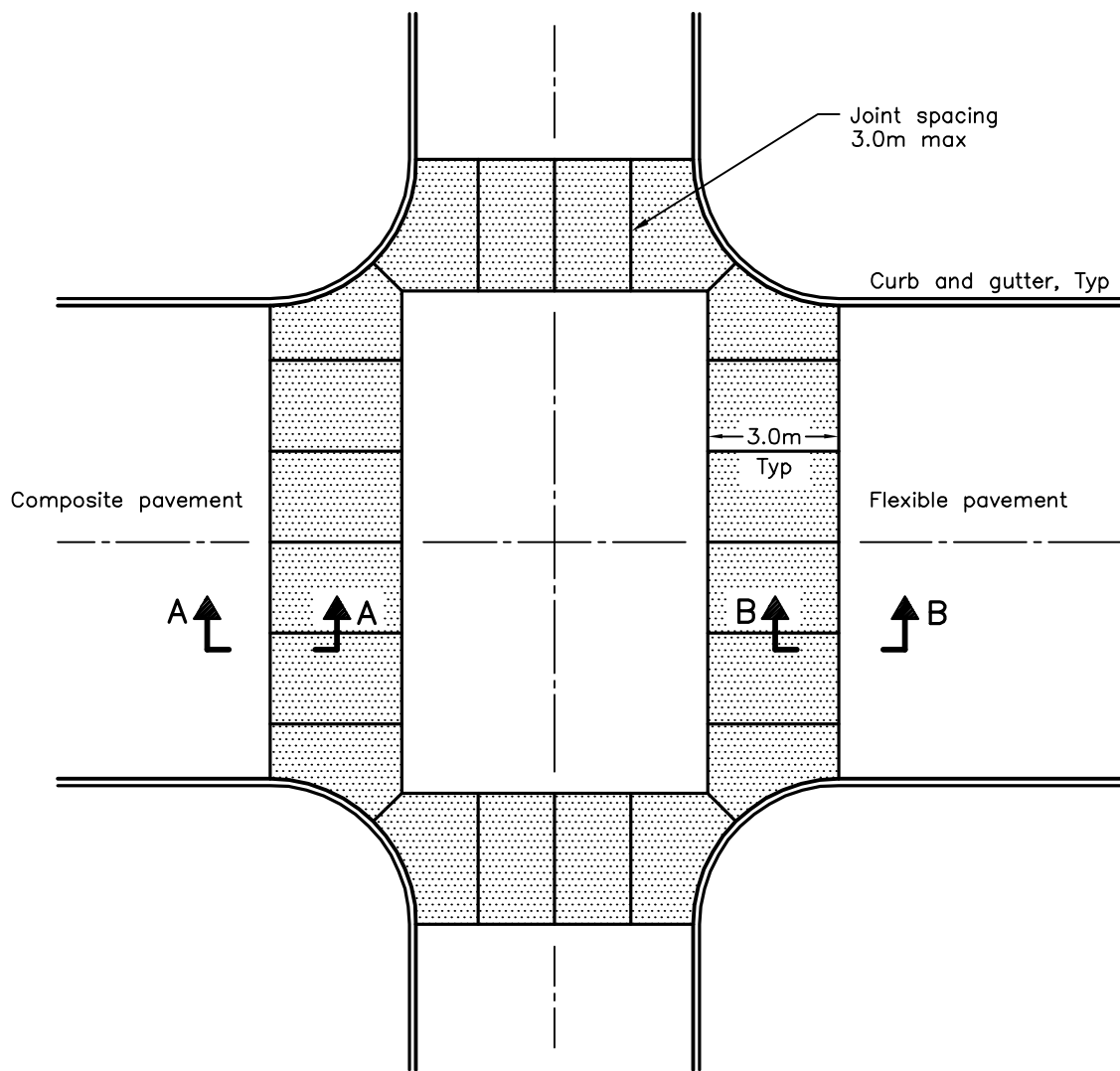
CONCRETE SIDEWALK DRIVEWAY ENTRANCE DETAILS

DRAFT

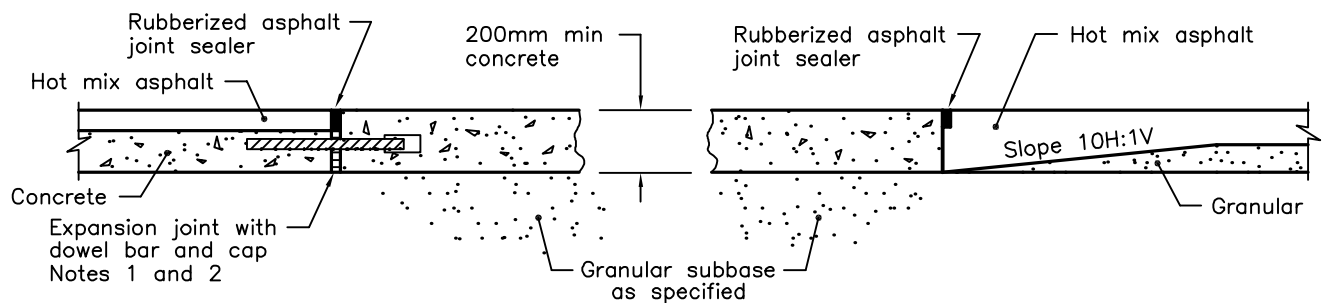
2015 05 01

D 310.050 - 0

2



PLAN



**COMPOSITE PAVEMENT
SECTION A-A**

**FLEXIBLE PAVEMENT
SECTION B-B**

NOTES:

- 1 For expansion joint detail, see OPSD-552.020.
- 2 Dowel bar with cap shall be installed at 300mm centres.
- A All dimensions are in millimetres unless otherwise shown.

THIS DRAFT STANDARD IS NOT FOR USE IN CONTRACTS

**CONCRETE PEDESTRIAN
CROSSOVER AT
SIGNALIZED INTERSECTIONS**

**DRAFT
2015 05 01**

D 310.060-0

2



Recreational Trails and Beach Access Routes

Overview

Accessible recreational trails and beach access routes provide an inclusive experience that allows people of all abilities to enjoy natural spaces in our communities.

A recreational trail is a public pedestrian trail intended for recreational and leisure purposes. The standard applies to newly constructed or redeveloped recreational trails that an organization intends to maintain. The standard does not apply to the certain types of recreational trails, such as wilderness trails, backcountry trails and portage routes. These types of trails are difficult to access because of their location, and are built in a way that reduces their impact on the natural environment.

Multi-use trails are used for different purposes at different times. For example, pedestrians may use a trail in the summer, but in the winter, the trail becomes a snowmobile trail. Another example is a pedestrian trail that is also a biking trail. These types of trails are not “solely intended” for either snowmobiling or biking, and are required to comply with the Standard.

Beach access routes are routes constructed for public pedestrian use that provide access to public beaches from off-street parking facilities, recreational trails, exterior paths of travel and amenities. The standard applies to beach access routes that can be either permanent or temporary, but does not apply to beach access routes that are created through repetitive use and without formal authorization.

Organizations must meet the requirements of the standard when they construct a recreational trail or beach access route that they intend to maintain or redevelop an existing one.

Trails not constructed or redeveloped by an obligated organization but rather created in an unplanned way are not required to meet these requirements. For example, decommissioned railway lines that now serve as trails, but were never constructed or redeveloped by an obligated organization with formal authorization to serve as a trail are exempt. If an organization replaces a decommissioned railway line or other unplanned trail, such as a path created in a



wooded area by frequent travel, with a new pedestrian trail, that new trail must meet the requirements.

Requirements for recreational trails and beach access routes apply on a go-forward basis to new construction and redeveloped trails and beach access routes. Organizations are not required to retrofit or change existing trails or beach access routes unless they choose to redevelop them.

This section is divided into the following five areas:

1. 80.6 – 80.8 Recreational trails and beach access routes, general
2. 80.9 Technical requirements for recreational trails
3. 80.10 Technical requirements for beach access routes
4. 80.11 – 80.13 Technical requirements common to recreational trails and beach access routes
5. 80.14 – 80.15 Exceptions to the requirements for recreational trails and beach access routes

Recreational Trails and Beach Access Routes, General

Requirements as Stated in the Regulation

Trails

80.6 This Part applies to newly constructed and redeveloped recreational trails that an obligated organization intends to maintain, but does not apply to the following types of recreational trails:

1. Trails solely intended for cross-country skiing, mountain biking or the use of motorized snow vehicles or off-road vehicles.
2. Wilderness trails, backcountry trails and portage routes.

Beach access routes

80.7 This Part applies to newly constructed and redeveloped beach access routes that an obligated organization intends to maintain, including permanent and temporary routes, and temporary routes that are established through the use of manufactured goods, which can be removed for the winter months.

Consultation, recreational trails

80.8 (1) Obligated organizations shall consult on the following before they construct new or redevelop existing recreational trails:

1. The slope of the trail.

2. The need for, and location of, ramps on the trail.
 3. The need for, location and design of,
 - i. rest areas,
 - ii. passing areas,
 - iii. viewing areas,
 - iv. amenities on the trail, and
 - v. any other pertinent feature.
- (2) Obligated organizations shall consult on the matters referred to in subsection (1) in the following manner:
1. Obligated organizations must consult with the public and persons with disabilities.
 2. Municipalities must also consult with their municipal accessibility advisory committees, where one has been established in accordance with subsection 29 (1) or (2) of the Act.

Intent of these Requirements

These sections clarify the types of recreational trails and beach access routes that must comply with the regulation and those that do not. These requirements also outline the features of recreational trails that are subject to consultation as required before an obligated organization constructs new, or redevelops existing, recreational trails.

Table 2 - When do Organizations have to Comply

Affected Organizations	Compliance Dates
Ontario Government and Legislative Assembly	January 1, 2015
Designated public sector organizations with 50+ employees	January 1, 2016
Designated public sector organizations with 1-49 employees	January 1, 2016
Private and not-for-profit organizations with 50+ employees	January 1, 2017
Private and not-for-profit organizations with 1-49 employees	January 1, 2018

Implementing the Requirements

Consultations for recreational trails

Before constructing a recreational trail or redeveloping an existing trail, obligated organizations must consult with the public, including people with disabilities. Municipalities with an Accessibility Advisory Committee, established in accordance with subsection 29 (1) or (2) of the Accessibility for Ontarians with Disabilities Act, must also consult with the committee.

Consultations must address the following design elements that may be part of the trail:

- The slope of the trail (e.g. the appropriate cross slope, running slope or both)
- Need for, and location of, ramps on the trail
- Need for, location and design of:
 - rest areas
 - passing areas
 - viewing areas
 - amenities on the trail
 - any other accessibility feature.

It is important to note that consultation on beach access routes **is not required**.

The intent of consultation on these specific elements is to give people with disabilities the opportunity to provide input as part of the planning and development of accessible trails. Consultation is important because it prevents organizations from assuming what accessibility features the people accessing the trail will need.

There is a common misconception that an accessible trail that is useable by people with disabilities must be flat. However, not every person with a disability wants to use a flat trail, as they may appreciate a challenging experience. Some people with disabilities will be able to use a trail regardless of the design specifications, even if it has a steep running slope. Understanding this can help designers and decision-makers when developing design solutions that meet the needs of the local population.

Consultations should be held as early as possible in the planning and design process to add value. That way, organizations can weigh all considerations before making decisions and finalizing design plans.

The consultation requirement does not set out a particular process or way to consult. The requirements recognize that consultations can be conducted in a wide variety of ways, depending on the organization.



This requirement also allows organizations to use consultation processes they may already have in place, or to combine consultations (e.g., consultations on play spaces and recreational trails may be conducted at the same time, based on an organization's need to do both). For more information about accessible consultation processes, organizations may wish to consult the Ontario Municipal Social Services Association's [Guides for Accessible Community Engagement](#). These guides were developed through the Accessibility Directorate of Ontario's EnAbling Change Program.

Technical Requirements for Recreational Trails

Requirements as Stated in the Regulation

80.9 (1) Obligated organizations shall ensure that any recreational trails that they construct or redevelop, and that they intend to maintain, meet the following technical requirements:

1. A recreational trail must have a minimum clear width of 1,000 mm.
2. A recreational trail must have a clear height that provides a minimum head room clearance of 2,100 mm above the trail.
3. The surface of a recreational trail must be firm and stable.
4. Where a recreational trail has openings in its surface,
 - i. the openings must not allow passage of an object that has a diameter of more than 20 mm, and
 - ii. any elongated openings must be orientated approximately perpendicular to the direction of travel.
5. Where a recreational trail is constructed adjacent to water or a drop-off, the trail must have edge protection that meets the following requirements:
 - i. The edge protection must constitute an elevated barrier that runs along the edge of the recreational trail in order to prevent users of the trail from slipping over the edge.
 - ii. The top of the edge protection must be at least 50 mm above the trail surface.
 - iii. The edge protection must be designed so as not to impede the drainage of the trail surface.

6. Despite paragraph 5, where there is a protective barrier that runs along the edge of a recreational trail that is adjacent to water or a drop-off, edge protection does not have to be provided.
7. The entrance to a recreational trail must provide a clear opening of between 850 mm and 1,000 mm, whether the entrance includes a gate, bollard or other entrance design.
8. A recreational trail must have at each trail head signage that provides the following information:
 - i. The length of the trail.
 - ii. The type of surface of which the trail is constructed.
 - iii. The average and the minimum trail width.
 - iv. The average and maximum running slope and cross slope.
 - v. The location of amenities, where provided.
- (2) The signage referred to in paragraph 8 of subsection (1) must have text that,
 - (a) has high tonal contrast with its background in order to assist with visual recognition; and
 - (b) includes characters that use a sans serif font.
- (3) Where other media, such as park websites or brochures, are used by the obligated organization to provide information about the recreational trail, beyond advertising, notice or promotion, the media must provide the same information as listed in paragraph 8 of subsection (1).

Intent of these Requirements

These requirements provide organizations with minimum accessibility requirements that must be met when constructing new or redeveloping existing recreational trails that the organization intends to maintain.

Table 3 - When do Organizations Have to Comply

Affected Organizations	Compliance Dates
Ontario Government and Legislative Assembly	January 1, 2015
Designated public sector organizations with 50+ employees	January 1, 2016
Designated public sector organizations with 1-49 employees	January 1, 2016



Affected Organizations	Compliance Dates
Private and not-for-profit organizations with 50+ employees	January 1, 2017
Private and not-for-profit organizations with 1-49 employees	January 1, 2018

Implementing the Requirements

Minimum clear width

The minimum width of the surface of a new or redeveloped trail must be 1,000 mm. This is wide enough to accommodate a person using a mobility device, a cane or a service animal.

Minimum head room clearance

Head room clearance refers to the area above the surface of the trail. This area must be clear of any obstacle that a person may have to duck under, such as tree branches or signs. Obstacles in the overhead area above the trail are safety hazards for people with low or no vision.

The minimum head room clearance on a trail is 2,100 mm above the trail surface. Obstacles must not be located lower than this height.

Trail surface

A firm and stable surface resists indentations. For example, when a person walks or wheels across it, the surface should return to its original condition once this pressure is removed. The trail surface must be firm and stable so that the wheels of a mobility device or the tips of canes, crutches or walkers will not sink into the surface.

Organizations can choose from a variety of materials that will achieve a firm and stable surface. Organizations have the flexibility to choose the most appropriate surface material, while still considering maintenance requirements or budget. The requirement for a firm and stable surface does not mean that organizations must use concrete or asphalt. When determining if a surface is firm and stable, organizations should consider whether the wheels of a mobility device could sink into the surface or cause the surface to move.

Openings in the surface

Openings in the surface can come from grates or other objects designed and placed in the ground to provide drainage or ventilation. This does not include openings in the trail's surface caused by naturally occurring erosion.

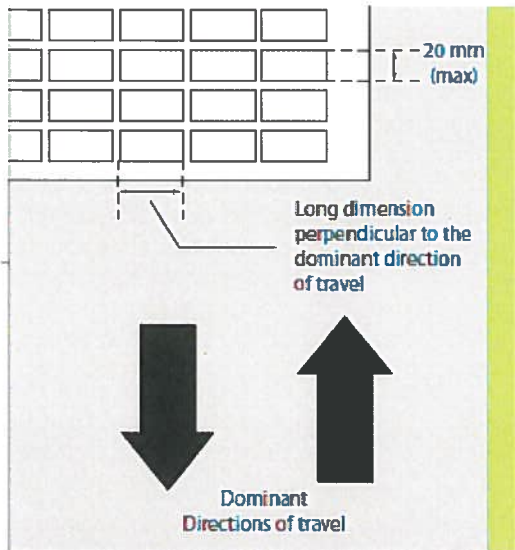


Figure 2 – Elongated openings

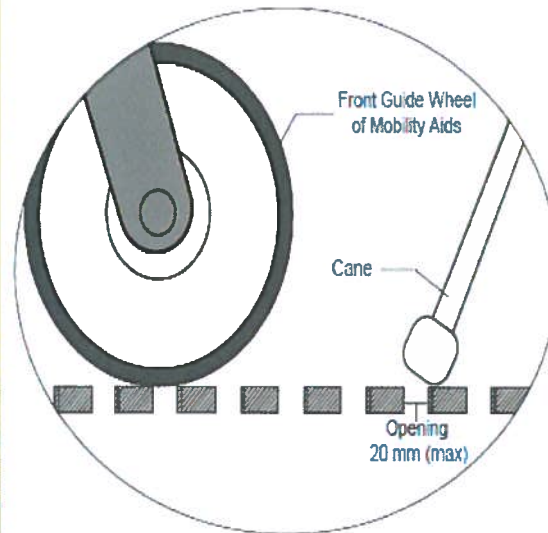


Figure 3 - Openings in the surface

Openings on the surface of a recreational trail must not allow for the passage of an object that is greater than 20 mm in diameter. This is so that mobility device casters (small front wheels) or cane tips cannot pass through them.

For elongated openings (i.e., those that are not square), such as those on certain grates, length should be placed at a right angle to the direction of travel to prevent slipping. If openings, such as those on a grate, have a longer length than width, the length should be placed at a right angle to the direction of travel to prevent slipping.

Edge protection

When a recreational trail is located directly beside water or a drop-off, edge protection is required. A drop-off may exist, for example, where a trail is located beside a cliff edge or other sudden, significant change in level. Edge protection is a small curb built at the side of the trail that would stop, for example, a mobility device from rolling off the edge of the trail and allow a person with low or no vision to detect the edge of the trail. The top of the edge protection must be a minimum of 50 mm in height above the boardwalk surface.

In addition, the edge protection must be designed so it does not prevent water from draining away from the trail surface, which could cause erosion of the trail surface, water pooling and unsafe pathways. However, in cases where a protective barrier is already in place, such as a wall or a railing, edge protection is not required.

Trail entrance

The entrance to a recreational trail must provide a clear opening of between 850 mm and 1,000 mm. This applies whether the entrance uses a gate, bollard or any other entrance design that restricts access to the trail.

The minimum range allows for the passage of people who use mobility devices or service animals. At the same time, the maximum range will prevent the passage of devices not desirable on a pedestrian trail, such as snowmobiles, all-terrain vehicles or small cars. A maximum range for entrances only applies to recreational trail entrances and does not apply to entrances elsewhere in the standard (where only a minimum is set out).

The entrance opening should be clear of any obstructions like handles, locks or hinges that could reduce the width to less than 850 mm.

Signage

For new or redeveloped recreational trails, signage must be placed at each trail head and provide the following information to all trail users:

- Trail length
- Surface type on the trail
- Average and minimum trail width
- Average and maximum running slope
- Average and maximum cross slope
- Location of amenities, where provided

A trail head refers to a point of access to a trail. They are commonly located close to a sidewalk or parking area. Organizations must identify which entrance/exit points to a recreational trail are trail heads to determine where the signage requirements will apply.

To help people with low or no vision and make the signage easier to read, the text on the signage must:

- Have high tonal contrast with its background
- Use sans serif font for its characters



Figure 4 - Trail signage

These requirements provide minimum standards for accessibility. Organizations have the flexibility to decide on the size of their signage, as well as what other information to provide, based on their specific needs.

Some organizations use other media to provide information about the recreational trail, such as park websites or brochures. Those media must also provide the same information listed on the signage as noted above, except when the media is used specifically for advertising, notices or promoting special events. For example, if an organization advertises the opening of a new trail in a newspaper, the accessibility information does not need to be included in the advertisement.

Technical Requirements for Beach Access Routes

Requirements as Stated in the Regulation

80.10 Obligated organizations shall ensure that beach access routes that they construct or redevelop, and that they intend to maintain, meet the following technical requirements:

THE CORPORATION OF THE CITY OF CORNWALL

By-law #037-2004

A By-law to to a further amend the Traffic and Parking By-law 069-1989
by adding the Definitions and Interpretations of "Pedestrian" and "Wheelchair"

WHEREAS The Transportation Section has received direction from Council to address concerns from close to 1000 residents regarding safe operation of Motorized Wheelchairs, also commonly known as "medical scooters" or "disable scooters/wheelchairs", upon city streets and sidewalks; and

WHEREAS in order to improve the safe operation of motorized wheelchairs, as well as motorists and pedestrian's safety on City streets and sidewalks, elemental rules for wheelchair's operation upon City highways and sidewalks are necessary, in order to achieve consistency throughout the City. The set of rules will also serve as a toll for the disable community to advise their members with respect to how to drive in a more consistent and safer manner on City streets and sidewalks in accordance to the proposed by-law amendment; and

WHEREAS to achieve this, it is desirous to implement the proposed by-law amendment to the Traffic and Parking by-law, as per the motion carried by City Council at its regular meeting of February 9, 2004.

NOW THEREFORE THE COUNCIL OF THE CORPORATION OF THE CITY OF CORNWALL ENACTS AS FOLLOWS:

Pursuant to Sections 8, 9 and 11 of the Municipal Act and Section 185(2) of the Highway Traffic Act. and amendments, the Traffic and Parking by-law 069-1989 is amended by **adding** thereto the following:

Definitions and Interpretations:

- (32) **"pedestrian"** means:
- (a) a person on foot, or
 - (b) a child, or other person in a wheeled chair, baby carriage, or toy vehicle being carried and propelled by another person on foot.
- (48) **"wheelchair"** means a chair mounted on wheels driven by muscular or any other kind of power and used for the carriage of a person who has a physical defect or disability.

Part B - TRAFFIC

WHEELCHAIRS

- (50.A) (1) A person driving a wheelchair upon a roadway shall:

- (a) drive as near the right-hand side of the roadway as practical, and
 - (b) exercise due care when passing a parked, stopped or standing vehicle or one proceeding in the same direction.
 - (c) bear the slow moving vehicle sign required by the Highway Traffic Act. Regulation 616 and comply with any other provisions required by any applicable legislation, including Regulation 625 of the Highway Traffic Act.
 - (d) be equipped with mirrors attached to the wheelchair or to the operator and placed in such a position as to afford the operator a clearly reflected view of the roadway in the rear, or of any vehicle approaching from the rear.
- (2) A person driving a wheelchair upon a roadway designated for one-way traffic shall:
- 1. obey and follow the designated one way street direction.
 - (b) drive as near the right-hand side of the roadway as practicable, and
 - (c) exercise due care when passing a standing vehicle or one proceeding in the same direction.
- (3) Persons driving wheelchairs upon a roadway shall do so in single file.
- (4) When on the roadway at any time from one-half hour before sunset to one-half hour after sunrise and at any other time when, due to insufficient light or unfavourable atmospheric conditions, persons and vehicles on the highway are not clearly discernible at a distance of 150 meters or less, a wheelchair shall carry on the front thereof a lighted lamp displaying a white or amber light and on the rear thereof a lighted lamp displaying a red light.
- (5) No person driving a wheelchair shall carry any package, bundle or article which prevents the operator from keeping both hands on the handle bars or joystick control.
- (6) No wheelchair towing, hauling, pulling or pushing any kind of attachment, except those provided by the manufacturer as integral part of the equipment for the wheelchair, shall be operated upon a public highway or sidewalk.
- (7) No operator of a wheelchair shall permit any person riding upon a bicycle, coaster, roller skates, skis, wagon, toboggan, sled, or toy vehicle to attach the same, himself or herself to the wheelchair.
- (8) No person shall park a wheelchair on a highway except in such a manner as to cause the least possible obstruction to pedestrian or vehicular traffic.

- (9) No wheelchair shall be operated upon a public highway or sidewalk except for the purpose of the carriage of a person who has a physical defect or disability.
- (10) No wheelchair carrying more than one person shall be operated upon a public highway or sidewalk.
- (11) A person driving a wheelchair upon the sidewalk shall:
 - (a) drive as near the right-hand side of the sidewalk as practical, and
 - (b) exercise due care, slowing down, even coming to a full stop as the case may require, when encountering oncoming pedestrians or while overtaking pedestrians proceeding in the same direction, always yielding the right-of-way to pedestrians.
 - (c) be equipped with mirrors attached to the wheelchair or to the operator and placed in such a position as to afford the operator a clearly reflected view of the sidewalk in the rear, or of any pedestrian approaching from the rear.

Also, the Traffic and Parking by-law 069-1989 is amended by repealing thereto the following:

- (32) “**pedestrian**” means:
 - (a) a person on foot, or
 - (b) an invalid, child, or other person in a wheeled chair, baby carriage, or play vehicle;

Read a first and second time, in open Council, this 23rd day of February, 2004.

DENISE LABELLE-GÉLINAS, CLERK

MAYOR PHIL POIRIER

Read a third and final time, signed and sealed in open Council this 23rd day of February, 2004.

DENISE LABELLE-GÉLINAS, CLERK

MAYOR PHIL POIRIER

620 McKay Street
Kingston, Ontario K7M 8V8
Phone 613-384-2535 Fax 613-384-6089
email : robw@kos.net

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	To supply labour ,equipment and materials to install the following	
	into opening prepared by others:	
1	7100 series by Horton barrier free operater c/w 2 x cm60/4 stainless	
	steel push buttons.	
1	per sketch and info provided,1 x 3400 series frame,1 x 3070 100a stock	
	door package,continuos hinge,threshold and weatherstripping.	
	Exclusions:preparation of wall if required,110v 60 cycle power supply,	
	relays,timers,conduits,wire mold if required,low voltage line to switch	
	locations,connections to outside equipment,wall finishings,permits,	
	boxes or fees if required,wall repair if required.Flashings or trims.	
	Thank-you for the opportunity to quote your project and if you wish	
	to proceed please sign and return with po# if required.Any additional	
	information to qualify this quote is available upon request.	
	TERMS: C.O.D. or NET 30 ON APPROVED CREDIT	
	Sub. Ttl.	\$4,128.00
	H.S.T.	536.64
	TOTAL \$	\$4,664.64

THIS QUOTE IS VALID FOR
THIRTY
DAYS FROM DATE BELOW

May 24 /2012


R. WELSH
PRESIDENT

ACCEPTED

SIGNATURE

DATE

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www.wiltse.com, wiltse@wiltse.com

BROCKVILLE GYMNASTICS
613-342-1100
613-498-1637

18-May-12

ATT: LESLIE McKELLAR

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- STANDARD COMMERCIAL DOOR AND HARDWARE
- INSIDE PADDLE HANDLE
- ELECTRIC STRIKE LATCH
- HORTON SERIES 7100 AUTOMATIC DOOR OPERATOR
- CLEAR THERMAL TEMPERED GLAZING

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- 1 - 52 X 54 X 6mm CLEAR TEMPERED GLASS
- 1 - 102 X 54 X 6mm CLEAR TEMPERED GLASS
- 1 - 96 X 36 X 6mm CLEAR TEMPERED GLASS

4912

912

797

944

SUB-TOTAL \$ 912.00

SPLIT OFFICE DOOR

- 1 - SPLIT OFFICE DOOR
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- HINGES
- SURFACE BOLT
- KEYED LEVER

SUB-TOTAL \$ 797.00

WASHROOM DOOR

- 1 - STEEL FRAME
- HARDBOARD DOOR
- HINGES
- PRIVACY FUNCTION LEVER

SUB-TOTAL \$ 944.00



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TO: Brockville Gymnastics Academy **Attn:** Lesley Mackellar

Date: May 29, 2012 **# of Pages (incl. cover):** 1

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