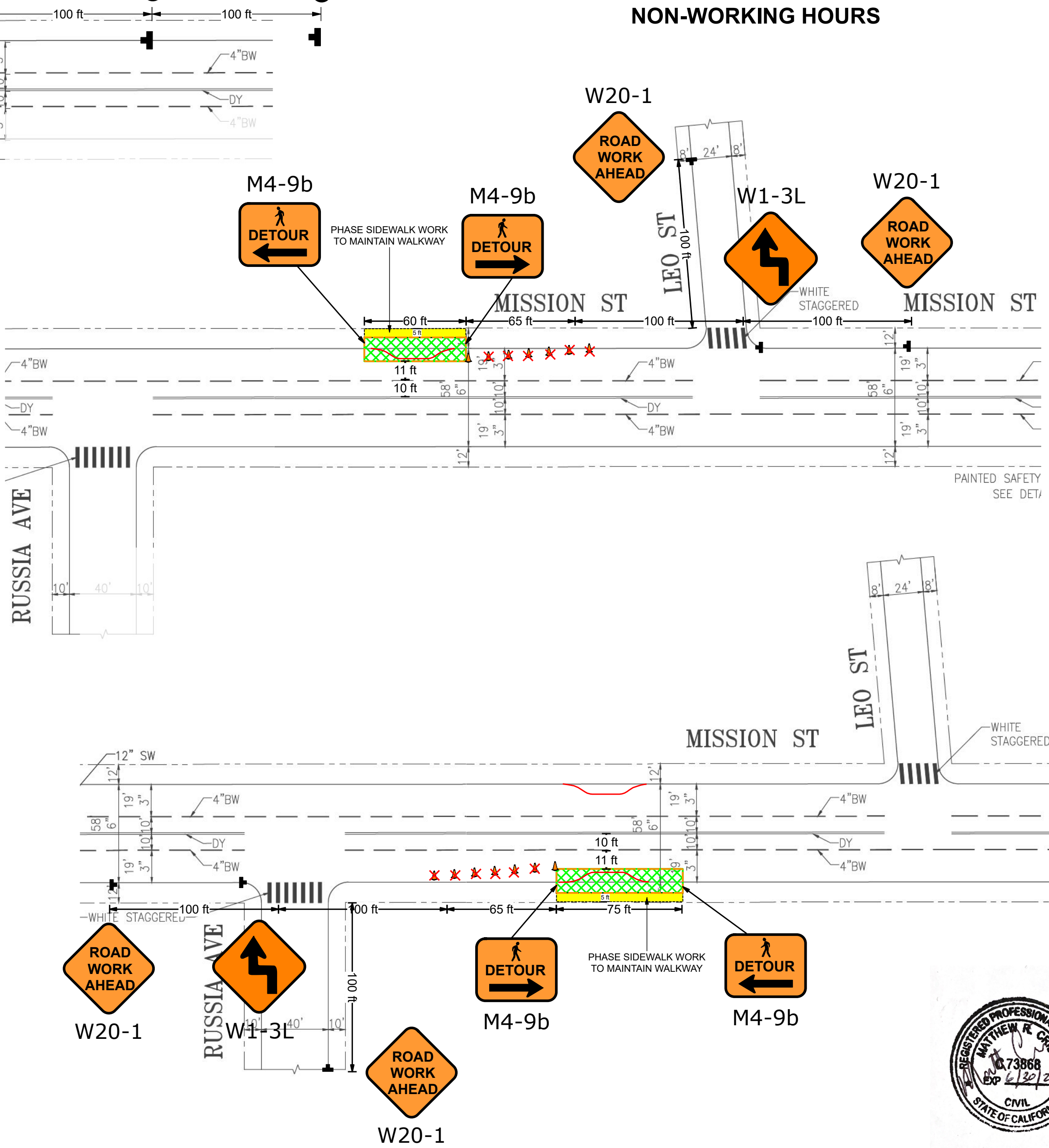
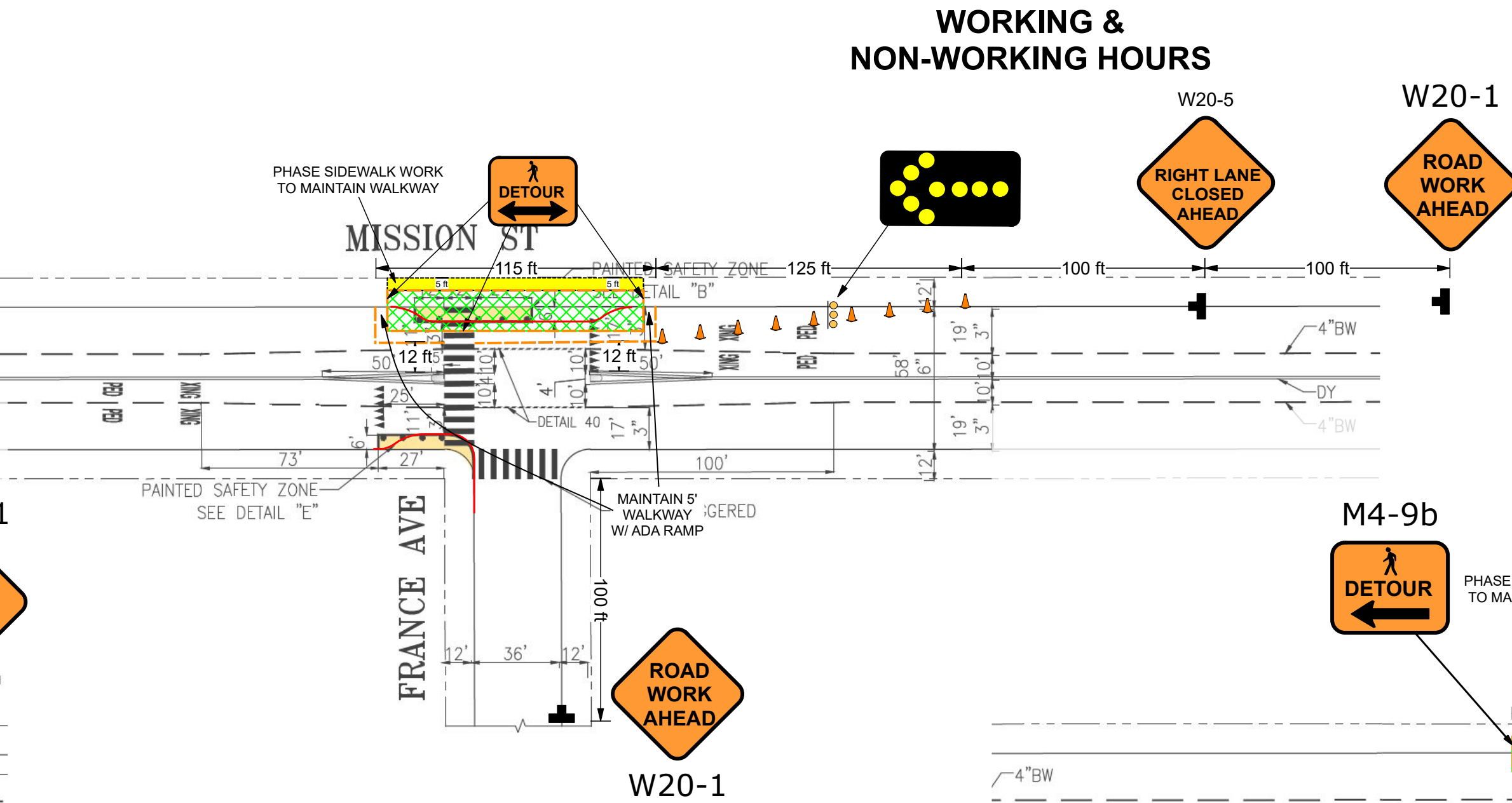
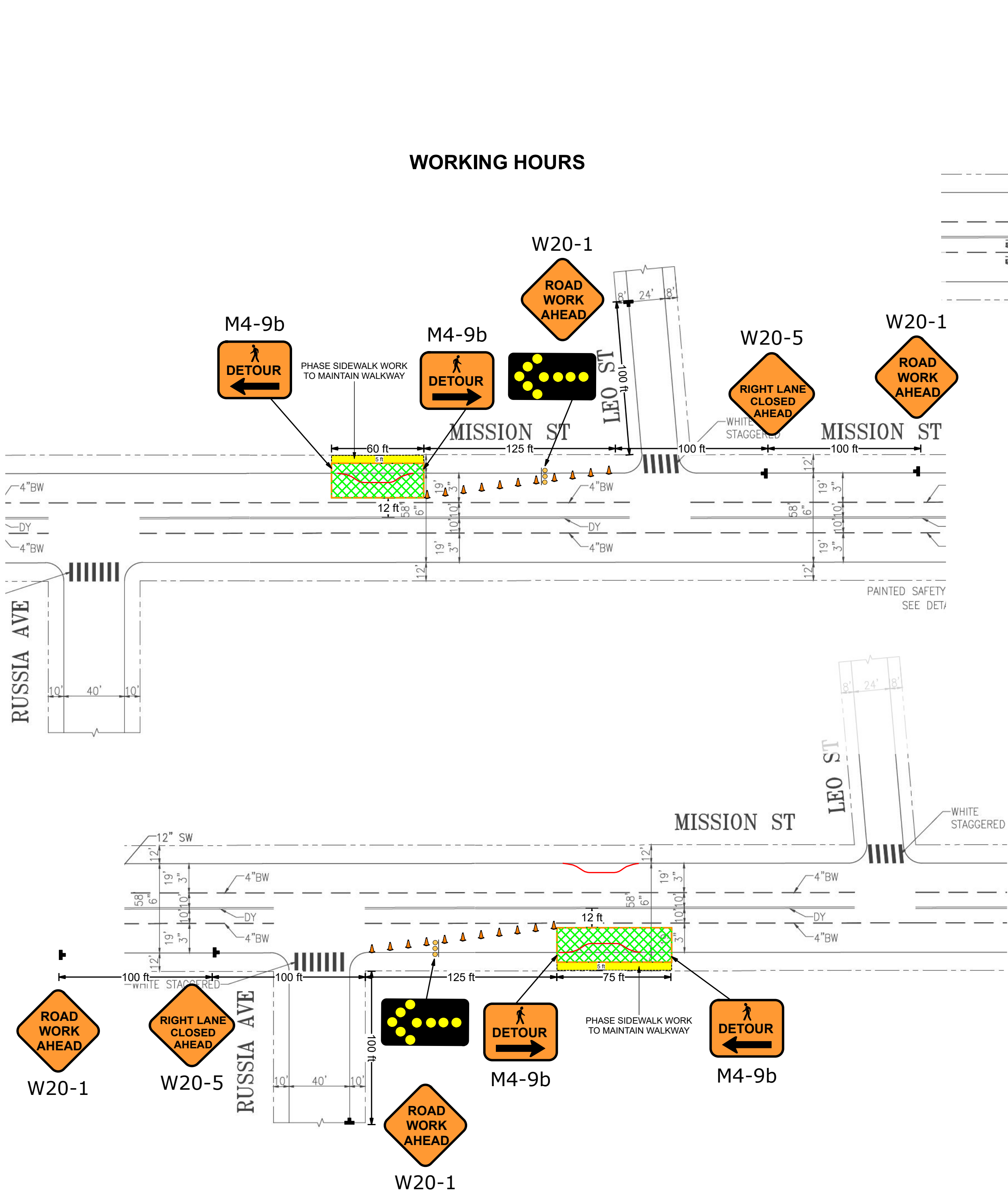


TRAFFIC CONTROL PLAN
MISSION ST & GENEVA AVE IMPROVEMENT PROJECT - 0000005626
SIDEWALK & CURB RAMP WORK - MISSION ST

Traffic Lane Requirements Number and Width of Lanes				
STREET	TIME	NORTHBOUND	SOUTHBOUND	EASTBOUND WESTBOUND
CURB & GUTTER (EXCEPT ALONG BUS PAD LOCATIONS), SIDEWALK, CURB RAMP, RAISED CROSSWALK AND PARKING STRIP WORK				
MISSION STREET				
Geneva Ave to Noy St	7AM - 3AM (M-F) 3AM - 3PM (M-F) Non-Working Hrs During Curing All Other Times	1@11'1@10' 1@12' 1@11'1@10' 1@11'1@10' Full Rdy	1@11' 1@12' 1@11'1@10' 1@11'1@10' Full Rdy	- - - - -



☐ NO EXCEPTIONS TAKEN
☒ MAKE CORRECTIONS NOTED
☐ REJECTED
☐ REVISE AND RESUBMIT
☐ SUBMIT SPECIFIED ITEM(S)

Review is only for general conformance with the design concept of the project and general compliance with the requirements of the contract documents. Any action shown is subject to the requirements of the plans and specifications. Contractor's responsibilities include, but are not limited to actual dimensions which shall be confirmed and correlated at the job site; preferred fabrication processes and techniques of construction; coordination of the contractor's work with that of all other trades; and the satisfactory performance of the contractor's work.

Sustainable Streets Division
San Francisco Municipal Transportation Agency
City and County of San Francisco

By *David P. Biddle* Date **8/29/23**

Legend	Table 6C-3(CA). Taper Length Criteria for Temporary Traffic Control Zones (for 12 foot Offset Width)	Table 6F-101(CA). Maximum Spacing of Channelizing Devices	Table 6C-1. Recommended Advance Warning Sign Spacing			<p>3450 3RD ST #3G SAN FRANCISCO, CA 94124 415-206-1700 PHONE 415-206-1711 FAX INFO@CMCTRAFFIC.COM</p> <p>LICENSE NO 792059 CLASS A, 31, C21 WBE/SBE/LBE/DBE CERTIFIED WWW.CMCTRAFFIC.COM</p>	<p>Date: 8/22/2023 Author: WY/MC Project: MISSION ST AND GENEVA AVE Client: BAUMAN LANDSCAPE Location: SAN FRANCISCO TCP: 176 Job #: 3405 Rev: 2</p> <p>Comments:</p> <ol style="list-style-type: none">1) WORK HOURS: SEE TRAFFIC LANE REQUIREMENTS2) CONTRACTOR TO VERIFY EXISTING STRIPING IS ACCURATE PRIOR TO START OF WORK.3) ALL TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF CA MUTCD.4) ALL TRAFFIC CONTROL DEVICES SHALL BE RETROREFLECTIVE IF SETUP DURING HOURS OF DARKNESS.5) THE CONTRACTOR SHALL BE ALLOWED TO WORK ON TWO (2) BLOCKS AND/OR TWO (2) INTERSECTIONS ON ANY ONE DAY NOT TO EXCEED 1,200 LINEAR FEET FOR CONCRETE BASE REPAIR WORK.6) THE CONTRACTOR SHALL NOTIFY SFMTA AT LEAST (10) WORKING DAYS IN ADVANCE OF DOING ANY WORK IN EXISTING PASSENGER LOADING AND UNLOADING ZONE. THE SFMTA MAY TEMPORARILY AUTHORIZE THE RELOCATION OF THESE ZONES.7) THE CONTRACTOR SHALL NOT PREVENT OR DELAY THE OPERATION OF MASS TRANSIT VEHICLES AT ANY TIME.																																																																																																																																																			
<p>28" Traffic Cone</p> <p>Delineator</p> <p>Pedestrian Barricade</p> <p>Work Area</p> <p>Sign and Stand</p> <p>Type I Barricade</p> <p>Type III Barricade</p> <p>Flagger</p> <p>Parking Control Officer</p> <p>NTS Not To Scale</p> <p>TOWS Towaway/No Stopping</p> <p>Curing Concrete</p>	<p>Speed* S (mph)</p> <table><thead><tr><th rowspan="2">Speed* S (mph)</th><th colspan="4">Minimum Taper Length** for Width of Offset 12 feet (W)</th></tr><tr><th>Merging L (feet)</th><th>Shifting L/2 (feet)</th><th>Shoulder L/3 (feet)</th><th>Down Stream (feet)***</th></tr></thead><tbody><tr><td>20</td><td>80</td><td>40</td><td>27</td><td>50</td></tr><tr><td>25</td><td>125</td><td>63</td><td>42</td><td>50</td></tr><tr><td>30</td><td>180</td><td>90</td><td>60</td><td>50</td></tr><tr><td>35</td><td>245</td><td>123</td><td>82</td><td>50</td></tr><tr><td>40</td><td>320</td><td>160</td><td>107</td><td>50</td></tr><tr><td>45</td><td>540</td><td>270</td><td>180</td><td>50</td></tr><tr><td>50</td><td>600</td><td>300</td><td>200</td><td>50</td></tr><tr><td>55</td><td>660</td><td>330</td><td>220</td><td>50</td></tr><tr><td>60</td><td>720</td><td>360</td><td>240</td><td>50</td></tr><tr><td>65</td><td>780</td><td>390</td><td>260</td><td>50</td></tr><tr><td>70</td><td>840</td><td>420</td><td>280</td><td>50</td></tr><tr><td>75</td><td>900</td><td>450</td><td>300</td><td>50</td></tr></tbody></table> <p>* - Posted speed limit, off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph. ** - For other offsets use the following merging taper length formula for L: For speeds of 40 mph or less, L = WS/160 For speeds of 45 mph or more, L = WS Where: L = taper length in feet W = width of offset in feet S = posted speed limit, off-peak 85th-percentile speed prior to work, or the anticipated operating speed in mph *** - Maximum downstream taper length is 100 feet. See Section 6C.08.</p>	Speed* S (mph)	Minimum Taper Length** for Width of Offset 12 feet (W)					Merging L (feet)	Shifting L/2 (feet)	Shoulder L/3 (feet)	Down Stream (feet)***	20	80	40	27	50	25	125	63	42	50	30	180	90	60	50	35	245	123	82	50	40	320	160	107	50	45	540	270	180	50	50	600	300	200	50	55	660	330	220	50	60	720	360	240	50	65	780	390	260	50	70	840	420	280	50	75	900	450	300	50	<p>Speed (mph)</p> <table><thead><tr><th rowspan="2">Speed (mph)</th><th colspan="3">Maximum Channelizing Devices Spacing</th></tr><tr><th>Taper* (feet)</th><th>Tangent (feet)</th><th>Conflict** (feet)</th></tr></thead><tbody><tr><td>20</td><td>20</td><td>40</td><td>10</td></tr><tr><td>25</td><td>25</td><td>50</td><td>12</td></tr><tr><td>30</td><td>30</td><td>60</td><td>15</td></tr><tr><td>35</td><td>35</td><td>70</td><td>17</td></tr><tr><td>40</td><td>40</td><td>80</td><td>20</td></tr><tr><td>45</td><td>45</td><td>90</td><td>22</td></tr><tr><td>50</td><td>50</td><td>100</td><td>25</td></tr><tr><td>55</td><td>50</td><td>100</td><td>25</td></tr><tr><td>60</td><td>50</td><td>100</td><td>25</td></tr><tr><td>65</td><td>50</td><td>100</td><td>25</td></tr><tr><td>70</td><td>50</td><td>100</td><td>25</td></tr><tr><td>75</td><td>50</td><td>100</td><td>25</td></tr></tbody></table> <p>* Maximum channelizing device spacing for all speeds on one-lane/two-way tapers is 20 feet. Maximum channelizing device spacing for all speeds on downstream tapers is 20 feet. All other tapers are as shown.</p> <p>** Use on intermediate and short-term projects for taper and tangent sections where there are no pavement markings or where there is a conflict between existing pavement markings and channelizing devices.</p>	Speed (mph)	Maximum Channelizing Devices Spacing			Taper* (feet)	Tangent (feet)	Conflict** (feet)	20	20	40	10	25	25	50	12	30	30	60	15	35	35	70	17	40	40	80	20	45	45	90	22	50	50	100	25	55	50	100	25	60	50	100	25	65	50	100	25	70	50	100	25	75	50	100	25	<p>Road Type</p> <table><thead><tr><th rowspan="2">Road Type</th><th colspan="3">Distance Between Signs**</th></tr><tr><th>A</th><th>B</th><th>C</th></tr></thead><tbody><tr><td>Urban - 25 mph or less***</td><td>100 feet</td><td>100 feet</td><td>100 feet</td></tr><tr><td>Urban - more than 25 mph***</td><td>250 feet</td><td>250 feet</td><td>250 feet</td></tr><tr><td>Urban - more than 40 mph***</td><td>350 feet</td><td>350 feet</td><td>350 feet</td></tr><tr><td>Rural</td><td>500 feet</td><td>500 feet</td><td>500 feet</td></tr><tr><td>Expressway/Freeway</td><td>1,000 feet</td><td>1,500 feet</td><td>2,640 feet</td></tr></tbody></table> <p>** The column headings A, B, and C are the dimensions shown in Figures 6H-1 through 6H-46. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.) *** Posted speed limit, off-peak 85th-percentile speed prior to work starting, or other anticipated operating speed in mph.</p>	Road Type	Distance Between Signs**			A	B	C	Urban - 25 mph or less***	100 feet	100 feet	100 feet	Urban - more than 25 mph***	250 feet	250 feet	250 feet	Urban - more than 40 mph***	350 feet	350 feet	350 feet	Rural	500 feet	500 feet	500 feet	Expressway/Freeway	1,000 feet	1,500 feet
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Plan Scale
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