


- ☒ NO EXCEPTIONS TAKEN
☐ MAKE CORRECTIONS NOTED
☐ REJECTED
☒ REVISE AND RESUBMIT
☐ SUBMIT SPECIFIED ITEM(S)

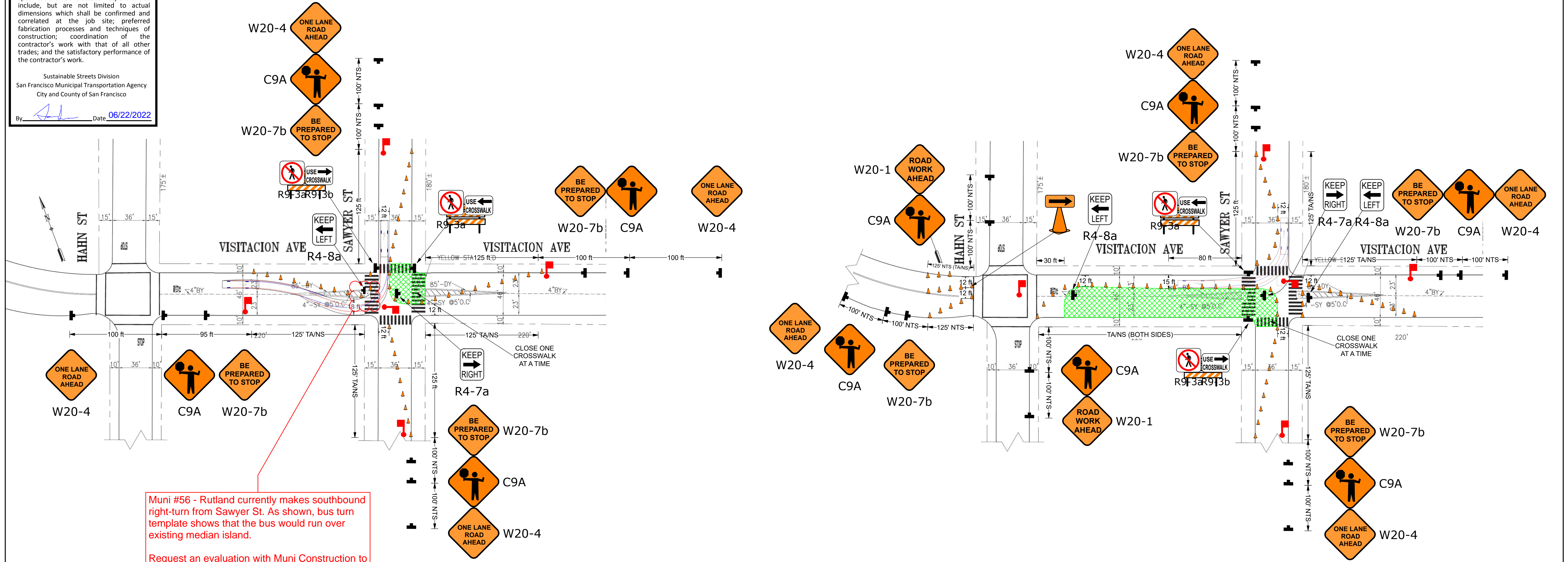
Review is only for general conformance with the design concept of the project and general compliance with the requirements of the contract documents. Any action shown is subject to the requirements of the plans and specifications. Contractor's responsibilities include, but are not limited to actual dimensions which shall be confirmed and correlated at the job site; preferred fabrication processes and techniques of construction; coordination of the contractor's work with that of all other trades; and the satisfactory performance of the contractor's work.

Sustainable Streets Division
San Francisco Municipal Transportation Agency
City and County of San Francisco

By:  Date: 06/22/2022

Approved 2022-08-12 following receipt of Muni bus reroute request. 

TRAFFIC CONTROL PLAN PW VARIOUS LOCATIONS PAVEMENT RENOVATION NO. 54 AND SEWER REPLACEMENT SEWER LINING WORK PLAN - VISITACION AVE FROM HAHN ST TO SAWYER ST







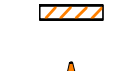

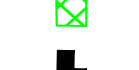








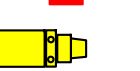




Muni #56 - Rutland currently makes southbound right-turn from Sawyer St. As shown, bus turn template shows that the bus would run over existing median island.

Request an evaluation with Muni Construction to determine whether to a) revise TCP to accommodate bus turns, or b) request bus reroute during construction activity. Revisions to TCP should make clear how Muni is accommodated



STREET	TIME	NORTHBOUND	SOUTHBOUND	EASTBOUND	WESTBOUND
DURING SEWER CONSTRUCTION					
Visitacion Ave. *	9AM - 4PM At Other Times	1@12' ↑ Full Roadway	1@12' ↑ Full Roadway	-	-
Sawyer St. at Visitacion Ave	9AM - 5PM At Other Times	-	-	1@10' ↑ Full Roadway	1@10' ↑ Full Roadway

LEGEND

-  Flashing Arrow Board
-  Flashing Arrow Board (Plan View)
-  Portable Flashing Beacon
-  Type I Barricade
-  Type III Barricade
-  Type III Barricade (Plan View)
-  28" Traffic Cone
-  Pedestrian Barricade
-  Work Area
-  Sign and Stand
-  Bus Stop
-  Concrete K-Rail Barrier
-  Water Filled Barrier
-  NTS
-  Tow-Away/No Stopping
-  Parking Control / Police Officer
-  Flagger
-  Equipment
-  Pipe to be installed
-  Fixed Mounted Channelizers

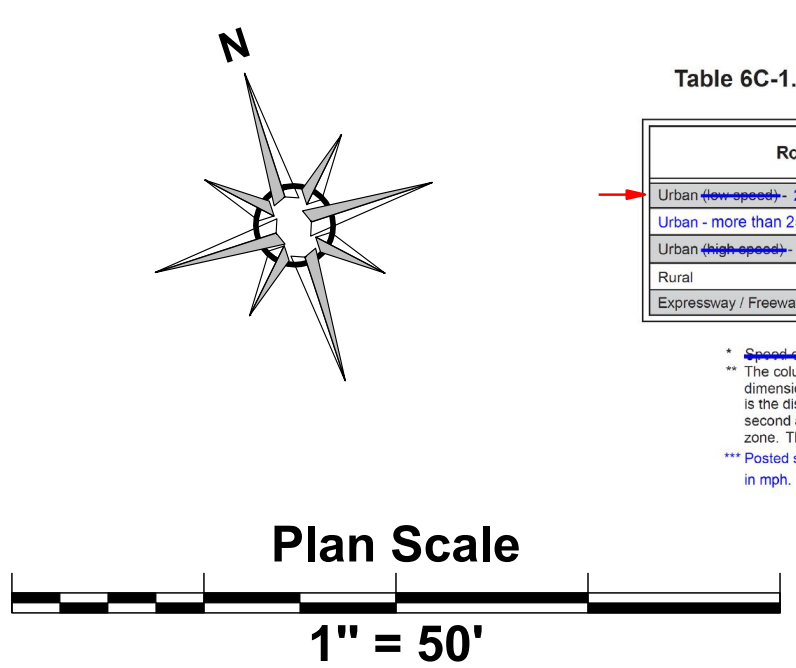


Table 6C-1. Recommended Advance Warning Sign Minimum Spacing

Road Type	Distance Between Signs**		
	A	B	C
Urban downstream - 25 mph or less***	100 feet	100 feet	100 feet
Urban - more than 25 mph to 40 mph***	250 feet	250 feet	250 feet
Urban upstream - more than 40 mph***	500 feet	500 feet	500 feet
Rural	500 feet	500 feet	500 feet
Expressway / Freeway	1,000 feet	1,500 feet	2,640 feet

* Speed ~~downstream~~ to be determined by the highway engineer
** The column headings A, B, and C are the dimensions shown in Figures 6C-1 through 6C-4. The A dimension is the distance from the location or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the T/C zone. The "third sign" is the sign that is furthest upstream from the T/C zone.)
*** Posted speed limit, off-peak 85th percentile speed prior to work starting, or other anticipated operating speed in mph.

Table 6F-16(CA). Maximum Spacing of Channelizing Devices

Speed (mph)	Taper* (feet)	Tangent (feet)	Conflict** (feet)
20	20	40	10
25	25	50	12
30	30	60	15
35	35	70	17
40	40	80	20
45	45	90	22
50	50	100	25
55	50	100	25
60	50	100	25
65	50	100	25
70	50	100	25
75	50	100	25

* Maximum channelizing device spacing for all speeds on one-lane/two-way tapers is 20 feet.
Maximum channelizing device spacing for all speeds on downstream tapers is 20 feet.
All other tapers are as shown.
** Use on intermediate and short-term projects for taper and tangent sections where there are no pavement markings or where there is a conflict between existing pavement markings and channelizing devices.

Table 6C-3(CA). Taper Length Criteria for Temporary Traffic Control Zones (for 12 foot Offset Width)

Speed* S (mph)	Minimum Taper Length** for Width of Offset 12 feet (W)			
	Merging L (feet)	Shifting L2 (feet)	Shoulder L3 (feet)	Down Stream (feet)***
20	80	40	27	50
25	125	63	42	50
30	180	90	60	50
35	245	123	83	50
40	320	160	107	50
45	340	270	180	50
50	600	300	200	50
55	860	330	220	50
60	720	360	240	50
65	780	390	260	50
70	840	420	280	50
75	900	450	300	50

* - Posted speed limit, off-peak 85th percentile speed prior to work starting, or the anticipated operating speed in mph.
** - For other offsets use the following merging taper length formula for L:
For speeds of 40 mph or less, L=WS/50
For speeds of 45 mph or more, L=WS
*** - Maximum downstream taper length is 100 feet. See Section 6C.08.



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Date: 03/15/2022 Author: WY/DD Project: VARIOUS LOCATIONS NO. 54
Client: PRECISION Location: SAN FRANCISCO TCP: 018
Job #: 3128 Rev: 1

Comments:

- 1) WORK HOURS: SEE TRAFFIC LANE REQUIREMENTS
- 2) CONTRACTOR TO VERIFY EXISTING STRIPING IS ACCURATE PRIOR TO START OF WORK.
- 3) ALL TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE CA MUTCD.
- 4) ALL TRAFFIC CONTROL DEVICES SHALL BE RETROREFLECTIVE IF SETUP DURING HOURS OF DARKNESS.
- 5) MAINTAIN LOCAL ACCESS TO BUSINESSES AND RESIDENTS AT ALL TIME.
- 6) THE CONTRACTOR SHALL PERFORM THE APPROPRIATE MEASURES TO ENSURE THE SAFETY OF BICYCLISTS ON ALL STREETS ON WHICH THERE IS CONSTRUCTION.
- 7) THE CONTRACTOR SHALL NOT PREVENT OR DELAY THE OPERATION OF MASS TRANSIT VEHICLES AT ANY TIME.