

TRAFFIC CONTROL PLAN  
PW VARIOUS LOCATIONS PAVEMENT RENOVATION NO. 54 AND SEWER REPLACEMENT  
SEWER WORK PLAN - SOUTH VAN NESS AVE FROM HOWARD ST TO 12TH ST

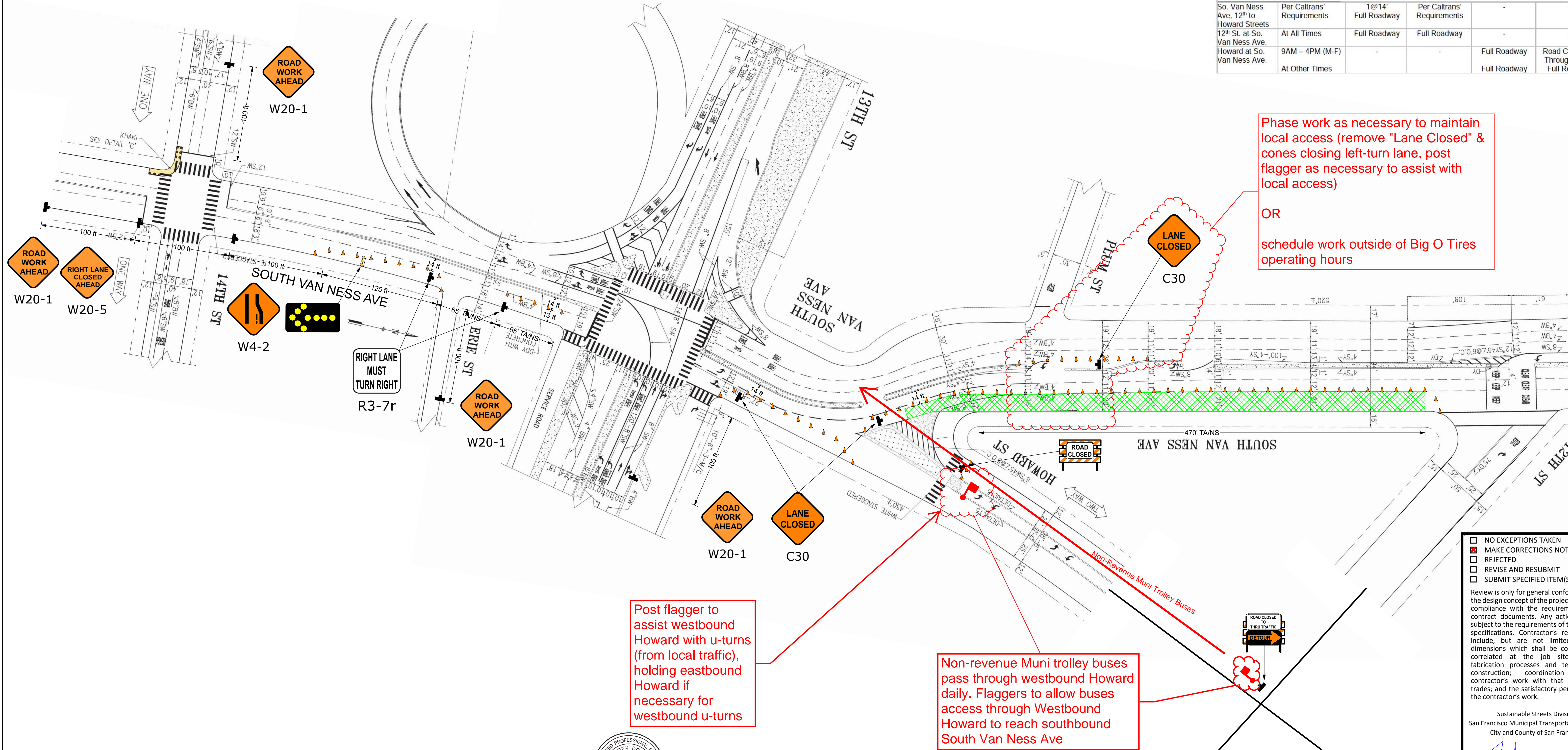
Traffic Lane Requirements  
Number and Width of Lanes

| STREET   | TIME                       | NORTHBOUND         | SOUTHBOUND                 | EASTBOUND    | WESTBOUND                                   |
|--|----------------------------|--------------------|----------------------------|--------------|---|
| <b>DURING SEWER CONSTRUCTION</b>                     |                            |                    |                            |              |   |
| So. Van Ness Ave, 12 <sup>th</sup> to Howard Streets | Per Caltrans' Requirements | 1@14' Full Roadway | Per Caltrans' Requirements | -            | -   |
| 12 <sup>th</sup> St. at So. Van Ness Ave.            | At All Times               | Full Roadway       | Full Roadway               | -            | -   |
| Howard at So. Van Ness Ave.                          | 9AM - 4PM (M-F)            | -                  | -                          | Full Roadway | Road Closed To Through Traffic Full Roadway |
|  | At Other Times             |                    |                            | Full Roadway |   |

Phase work as necessary to maintain local access (remove "Lane Closed" & cones closing left-turn lane, post flagger as necessary to assist with local access)

OR

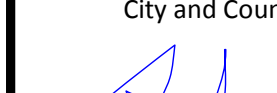
schedule work outside of Big O Tires operating hours



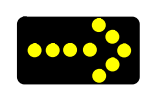

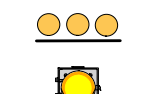
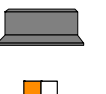




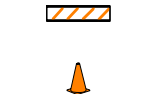


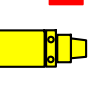
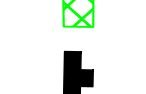
☐ NO EXCEPTIONS TAKEN  
☒ MAKE CORRECTIONS NOTED  
☐ REJECTED  
☐ REVISE AND RESUBMIT  
☐ SUBMIT SPECIFIED ITEM(S)

Review is only for general conformance with the design concept of the project and general compliance with the requirements of the contract documents. Any action shown is subject to the requirements of the plans and specifications. Contractor's responsibilities include, but are not limited to actual dimensions which shall be confirmed and correlated at the job site; preferred fabrication processes and techniques of construction; coordination of the contractor's work with that of all other trades; and the satisfactory performance of the contractor's work.

Sustainable Streets Division  
San Francisco Municipal Transportation Agency  
City and County of San Francisco

By:  Date: **05/09/2022**

LEGEND

- |   |                                  |   |                                  |
|---|----------------------------------|---|----------------------------------|
|  | Flashing Arrow Board             |  | Bus Stop                         |
|  | Flashing Arrow Board (Plan View) |  | Concrete K-Rail Barrier          |
|  | Portable Flashing Beacon         |  | Water Filled Barrier             |
|  | Type I Barricade                 |  | Not To Scale                     |
|  | Type II Barricade                |  | Tow-Away/No Stopping             |
|  | Type III Barricade (Plan View)   |  | Parking Control / Police Officer |
|  | 28\"/>                           |   |                                  |

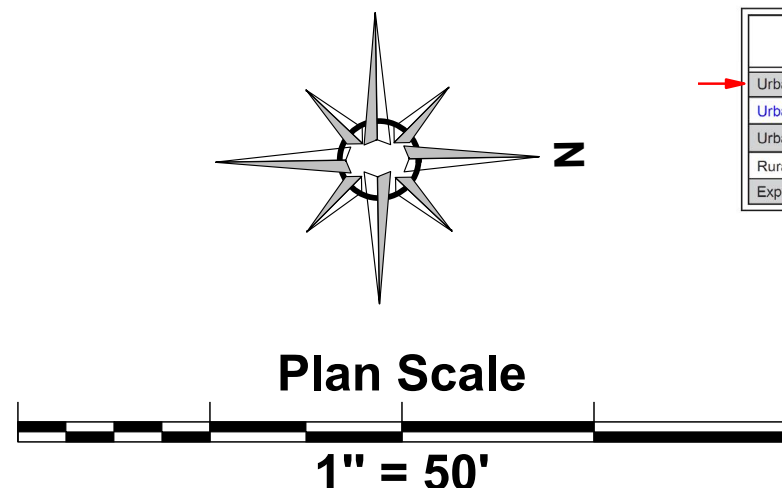


Table 6C-1. Recommended Advance Warning Sign Minimum Spacing

| Road Type                             | Distance Between Signs** |            |            |
|---------------------------------------|--------------------------|------------|------------|
|                                       | A                        | B          | C          |
| Urban - more than 25 mph to 40 mph*** | 100 feet                 | 100 feet   | 100 feet   |
| Urban - more than 25 mph to 40 mph*** | 250 feet                 | 250 feet   | 250 feet   |
| Urban - more than 40 mph***           | 350 feet                 | 350 feet   | 350 feet   |
| Rural                                 | 500 feet                 | 500 feet   | 500 feet   |
| Expressway / Freeway                  | 1,000 feet               | 1,000 feet | 2,000 feet |

\* Approximate spacing to be determined by the highway agency.  
\*\* The column headings A, B, and C are the dimensions shown in Figures 6C-1 through 6C-4. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.)  
\*\*\* Posted speed limit, off-peak 85th-percentile speed prior to work starting, or other anticipated operating speed in mph.

Table 6F-10(CA). Maximum Spacing of Channelizing Devices

| Speed (mph) | Maximum Channelizing Devices Spacing |                |                   |
|-------------|--------------------------------------|----------------|-------------------|
|             | Taper* (feet)                        | Tangent (feet) | Conflict** (feet) |
| 20          | 20                                   | 40             | 10                |
| 25          | 25                                   | 50             | 12                |
| 30          | 30                                   | 60             | 15                |
| 35          | 35                                   | 70             | 17                |
| 40          | 40                                   | 80             | 20                |
| 45          | 45                                   | 90             | 22                |
| 50          | 50                                   | 100            | 25                |
| 55          | 50                                   | 100            | 25                |
| 60          | 50                                   | 100            | 25                |
| 65          | 50                                   | 100            | 25                |
| 70          | 50                                   | 100            | 25                |
| 75          | 50                                   | 100            | 25                |

\* Maximum channelizing device spacing for all speeds on one-lane/two-way tapers is 20 feet. Maximum channelizing device spacing for all speeds on downstream tapers is 20 feet. All other tapers are as shown.  
\*\* Use on intermediate and short-term projects for taper and tangent sections where there are no pavement markings or where there is a conflict between existing pavement markings and channelizing devices.

Table 6C-3(CA). Taper Length Criteria for Temporary Traffic Control Zones (for 12 feet Offset Width)

| Speed* S (mph) | Minimum Taper Length** for Width of Offset 12 feet (W) |                     |            |                       |
|----------------|--|---------------------|------------|-----------------------|
|                | Merging L (feet)                                       | Shifting L/2 (feet) | L/3 (feet) | Down Stream (feet)*** |
| 20             | 60   | 40                  | 27         | 50                    |
| 25             | 125  | 63                  | 42         | 50                    |
| 30             | 180  | 90                  | 60         | 50                    |
| 35             | 245  | 123                 | 82         | 50                    |
| 40             | 320  | 160                 | 107        | 50                    |
| 45             | 340  | 210                 | 140        | 50                    |
| 50             | 600  | 300                 | 200        | 50                    |
| 55             | 600  | 300                 | 220        | 50                    |
| 60             | 720  | 360                 | 240        | 50                    |
| 65             | 780  | 380                 | 260        | 50                    |
| 70             | 840  | 420                 | 280        | 50                    |
| 75             | 900  | 450                 | 300        | 50                    |

\* Posted speed limit, off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph.

\*\* For other offsets use the following merging taper length formula for L:  
For speeds of 40 mph or less, L=WS/50  
For speeds of 45 mph or more, L=WS

Where:  
L = taper length in feet  
W = width of offset in feet  
S = posted speed limit, off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph

\*\*\* Maximum downstream taper length is 100 feet. See Section 6C.08.



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Date: 11/16/2021 Author: WY/DD Project: VARIOUS LOCATIONS NO. 54  
Client: PRECISION Location: SAN FRANCISCO TCP: 006  
Job #: 3128 Rev: 0

- Comments:
- 1) WORK HOURS: SEE TRAFFIC LANE REQUIREMENTS
  - 2) CONTRACTOR TO VERIFY EXISTING STRIPING IS ACCURATE PRIOR TO START OF WORK.
  - 3) ALL TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE CA MUTCD.
  - 4) ALL TRAFFIC CONTROL DEVICES SHALL BE RETROREFLECTIVE IF SETUP DURING HOURS OF DARKNESS.
  - 5) MAINTAIN LOCAL ACCESS TO BUSINESSES AND RESIDENTS AT ALL TIME.
  - 6) THE CONTRACTOR SHALL PERFORM THE APPROPRIATE MEASURES TO ENSURE THE SAFETY OF BICYCLISTS ON ALL STREETS ON WHICH THERE IS CONSTRUCTION.
  - 7) THE CONTRACTOR SHALL NOT PREVENT OR DELAY THE OPERATION OF MASS TRANSIT VEHICLES AT ANY TIME.