

OCCUPANCY GROUP: CONSTRUCTION TYPE FUEL CANOPY AREA: **FUEL CANOPY HEIGHT:** 

II B (NOT RATED) 4,576 SQ FT 17'-6" TO TOP OF CANOPY

ADJ	ADJUSTABLE, ADJACENT
ALUM	ALUMINUM
APPROX	APPROXIMATE
BD	BOARD
BLDG	BUILDING
BLK	BLOCK
BLK'G	BLOCKING
BM	BEAM
BTWN	BETWEEN
CB	CATCH BASIN
CI	CAST IRON
CIP	CAST IN PLACE
CJ	CONSTRUCTION JOINT
CL	CENTER LINE
CLR	CLEAR
CMU	CONCRETE MASONRY UNIT
COL	COLUMN
CONC	CONCRETE
CONT	CONTINUOUS
CONSTR	CONSTRUCTION
CORP	CORPORATION

DOUBLE

DETAIL

EACH

DWG

ELEC

ELEV

DIAMETER

DIMENSION

DOWNSPOUT

DRAWING

ELECTRICAL

ELEVATION

**ENCLOSURE** ET CETERA **EXHAUST EXPANSION** FLOOR DRAIN FINISHED FLOOR FINISH FEET FOOTING GAUGE GENERAL CONTRACTOR HANDICAPPED HORIZONTAL HT HEIGHT HTR HEATER INTERNATIONAL BLDG CODE INSULATION INTERIOR JOINT, JOINTS LIQUID CRYSTAL DISPLAY

LIQ

LIQUID

MASONRY

MAXIMUM

MECHANICAL

METAL BUILDING SUPPLIER

MANUFACTURING MANUFACTURER MANHOLE MISCELLANEOUS MOISTURE RESISTANT MOUNTING NOT IN CONTRACT NTS NOT TO SCALE OVERALL ON CENTER OVERHEAD OPERATION OPENING OPPOSITE POLY VINYL CHLORIDE PLATE, PROPERTY LINE PROJECT PRESSURE REDUCING VALVE QUARTER RADIUS LINEAR FOOT, LINEAL FOOT REFLECTED CEILING PLAN

ROOF DRAIN

RAIN LEADER

REINFORCING

RECEIVED

ROLLED FORMED

STD STL STEEL TOS TYPICAL UTIL UTILITY UNSCHD UNSCHEDULED WIDTH WITH W/O WITHOUT WINDOW

**SCHEDULE** SUPPLIED & INSTALLED BY OWNER SUPPLIED & INSTALLED BY VENDOR SAW-CUT JOINT SUPPLIED BY OWNER & INSTALLED BY CONTRACTOR SPECIFIED SQUARE SANITARY SEWER STANDARD STRUCT STRUCTURE, STRUCTURAL T & E TANK & ELECTRICAL

TOP OF MASONRY TOP OF STEEL TUBE STEEL UNITED LABORATORIES UNLESS NOTED OTHERWISE VERIFY IN FIELD EXTERIOR ELEVATION WAREHOUSE AG-2 /- SHEET WHERE DRAWN WATERPROOF INTERIOR ELEVATION WATER RESISTANT 5 DETAIL IDENTIFICATION

# SYMBOLS NORTH ARROW -PROJECT NORTH INDICATOR TRUE NORTH INDICATOR

— DETAIL IDENTIFICATION CUT LINE - DEFINES THE DETAILED ELEMENT SHEET WHERE DRAWN DETAIL IDENTIFICATION CUT LINE - DEFINES THE DETAILED ELEMENT SHEET WHERE DRAWN DETAIL IDENTIFICATION

**FENCE** <del>\_\_\_\_x\_\_\_x\_\_\_x\_\_\_x\_\_\_x\_\_\_x\_\_\_x\_\_\_x\_\_\_</del> BREAKLINE **COLUMN GRIDS** SHEET WHERE DRAWN

**EQUIPMENT MARK** 

EQUIPMENT NUMBER -REFER TO SCHEDI II F

REVISION CLOUD AND DELTA

SLOPE DOWN 1/8"
PER FOOT

PROPERTY LINE

WORK, CONTROL OR DATUM POINT

SLOPE DIRECTION WITH DATUM MARK

# ARCHITECTURAL SYMBOLS **MATERIALS**

CMU OR BRICK

THE FOLLOWING DESCRIBED REAL PROPERTY IS SITUATED IN THE CITY OF S.E. MEMPHIS, CONCRETE SHELBY COUNTY, STATE OF TENNESSEE. EARTH GRAVEL

# SITE & BLDG STATISTICS

683 STALLS (15 ACCESSIBLE STALLS)

LEGAL DESCRIPTION

1. ZONING: PLANNED DEVELOPMENT 2. SETBACKS: **REQUIRED** FRONT: 24 FT NONE REAR: SIDE: 20 FT 3. SITE AREA: 14.24 ACRES (620,460 S.F.) 4. LOT COVERAGE ALLOWED: 5. LANDSCAPING REQUIRED: YES, SEE CIVIL 6. EXISTING BUILDING AREA: BUILDING FOOTPRINT AREA: 149,448 SQ FT

7. PARKING REQUIRED:

PARKING RATIO:

1/300 SQ FT x 149,448 SQ FT TOTAL = 498 PARKING SPACES REQ'D 8. PARKING PROVIDED: EXISTING PARKING: 709 STALLS DELETED PARKING: 36 STALLS TOTAL: 683 STALLS

# SCOPE OF WORK

**FUELING FACILITY EXPANSION:** 

THE SCOPE OF WORK INCLUDES ADDING THREE (3) NEW ISLANDS WITH ONE DOUBLE SIDED FUEL DISPENSER EACH; ALLOWING FOR SIX (6) ADDITIONAL AUTOMOBILE FUELING POSITIONS, REMOVING AND REPLACING THE EXISTING (6) DISPENSERS, ENLARGING THE EXISTING CANOPY BY 1,696 SQUARE FEET, AND CURBS, CONCRETE, ASPHALT AND LANDSCAPE REVISIONS. THE EXISTING CONTROLLER ENCLOSURE WILL BE DEMOLISHED

THE NINE (9) NEW DISPENSERS WILL BE INTEGRATED INTO THE COMPUTERIZED CONTROLLER, WHICH RUNS THE PAYMENT, PUMPING AND MONITORING SYSTEMS. FUEL IS STORED IN THREE (3) EXISTING DOUBLE WALLED, FIBERGLASS UNDERGROUND STORAGE

THE NEW CONTROL / MONITORING SYSTEM WILL INCLUDE AN ALARM TIED INTO THE OWNER'S CONTRACTED 24 HOUR MONITORING SERVICE COMPANY. THE CONTINUOUS LEAK DETECTION SYSTEM WILL AUTOMATICALLY CHECK FOR FLUID LEAKS AND AUTOMATICALLY WILL STOP THE FLOW OF GASOLINE IN THE EVENT OF AN ALARM CONDITION.

SE MEMPHIS, TN 3771 HACKS CROSS ROAD MEMPHIS, TN 38125

COSTCO

WHOLESALE CORPORATION 730 LAKE DRIVE

ISSAQUAH, WA 98027 T: 425.313.8100

www.costco.com

8444 Westpark Dr, Ste 120 McLean, VA 22102 703 564 8484

ISSUE FOR

DOUGLAS E. BROOKBANK, ARCHITECT

MG2. All rights reserved. No part of this document may be reproduced i DATE DESCRIPTION

98-4150-19 PM: AGNES VINCENT DRAWN: AV AUGUST 08, 2023

TITLE SHEET

TSG-1



3771 HACKS CROSS ROAD MEMPHIS, TN 38125

### **DEMOLITION NOTES** HE BUILDING WILL BE OCCUPIED AND IN FULL USE BY THE OWNER DURING CONSTRUCTION.

. AREAS OF DEMOLITION SHALL INCLUDE, BUT ARE NOT LIMITED TO, ANY STRUCTURAL, PLUMBING, ELECTRICAL, MECHANICAL, FIRE SPRINKLER, ETC. ITEMS NECESSARY TO COMPLETE THE DEMOLITION WORK. THE CONTRACTOR IS TO REMOVE ALL WALLS, BOLLARDS, MISC. STEEL, PLUMBING, ELECTRICAL, RAISED CONCRETE SLAB, CURBS, REBARS, ETC. ASSOCIATED WITH DEMOLITION OF AREAS SHOWN. ALL REMOVED ITEMS NOT DESIGNATED FOR REUSE SHALL BE OFFERED IN GOOD CONDITION

I. CONSTRUCTION THAT MAY AFFECT THE PUBLIC SHALL BE DONE DURING OFF-HOURS

TO THE OWNER. DURING THE BIDDING PERIOD, COORDINATE AND VERIFY WITH THE OWNER AS TO WHICH ITEMS ARE TO BE SALVAGED. 4. DO NOT INTERRUPT ANY SERVICES (WATER, PLUMBING, FIRE SPRINKLER, ETC.) WITHOUT

PRIOR WRITTEN APPROVAL OF THE OWNER. . PROVIDE AND MAINTAIN EGRESS PATHS THROUGHOUT CONSTRUCTION INCLUDING, BUT NOT LIMITED TO, EXIT SIGNS, LIGHTING, FIRE SPRINKLERS, ETC. OBTAIN FIRE MARSHAL APPROVAL PRIOR TO CLOSING ANY EGRESS OPENINGS AND/OR EGRESS PATHS DURING

DEMOLITION/CONSTRUCTION. 3. THE CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES WITHIN PROXIMITY OF THE WORK AREA PRIOR TO EXCAVATION. EXISTING UTILITIES SHOWN HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE

SHOWN FOR CONVENIENCE ONLY. VERIFY ALL INVERT ELEVATIONS AT POINTS OF CONNECTIONS OF NEW WORK TO EXISTING PRIOR TO STARTING ANY WORK. PROVIDE TEMPORARY DUST PARTITIONS AS REQUIRED TO PREVENT DUST AND DEBRIS FROM SETTLING ON ADJACENT AREAS NOT UNDER CONSTRUCTION. REVIEW WITH AND OBTAIN OWNER'S APPROVAL FOR LOCATIONS.

. CAP ALL UTILITIES AND DRAIN LINES BELOW THE FLOOR AS REQUIRED. CONTRACTOR IS RESPONSIBLE FOR FLOOR PATCHING. 0. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING OR REPLACING ITEMS DAMAGED DURING DEMOLITION AND CONSTRUCTION. I. WHEN CUTTING AND PATCHING, THE CONTRACTOR SHALL USE METHODS LEAST LIKELY TO DAMAGE ELEMENTS RETAINED OR ADJOINING SURFACES. CUT HOLES AND SLOTS AS SMALL

ADJACENT MATERIALS. 2. WHERE EXISTING CONSTRUCTION IS REMOVED, CUT OR OTHERWISE DISTURBED, PATCH TO MATCH THE EXISTING ADJACENT SURFACES. SEAMS TO BE AS INVISIBLE AS PRACTICAL. REPAIRED FINISHES SHALL BE EXTENDED TO THE NEAREST VISUAL BREAK LINES SUCH AS CORNERS, CEILING LINES, TOP OF BASE, ETC.

AS PRACTICAL/POSSIBLE, NEATLY TO SIZE REQUIRED AND WITH MINIMUM DISTURBANCES OF

13. WHERE NEW WORK ABUTS OR ALIGNS WITH EXISTING, PERFORM A SMOOTH AND EVEN

TRANSITION. PATCHED WORK TO MATCH EXISTING ADJACENT WORK IN TEXTURE AND

14. REMOVE AND REPLACE AREAS, SURFACES OR ITEMS THAT CANNOT BE SATISFACTORILY PATCHED. 15. AS A MINIMUM, THE LEVEL OF WORKMANSHIP SHOULD MATCH THE GENERAL LEVEL OF

16. UNLESS SHOWN ON THE DRAWINGS OTHERWISE, DO NOT SUPPORT OR SUSPEND ITEMS, EQUIPMENT, HANGERS, ETC. FROM EXISTING STRUCTURAL MEMBERS (BEAMS, TRUSSES, JOIST, ETC.) WITHOUT WRITTEN APPROVAL OF A LICENSED STRUCTURAL ENGINEER.

17. DO NOT CUT OR DRILL ANY STRUCTURAL MEMBER (PARTICULARLY ROOF JOIST) WITHOUT WRITTEN APPROVAL OF A LICENSED STRUCTURAL ENGINEER. 18. SHORING OF STRUCTURAL FOUNDATIONS, STRUCTURES, AND/OR TRENCHING REQUIRED TO COMPLETE THE WORK DESCRIBED IN THE DOCUMENTS IS CONSIDERED A MEANS, METHOD OR ECHNIQUE AND IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. IF A REGULATORY

AGENCY REQUIRES A LICENSED ENGINEER TO SUPERVISE, APPROVE, AND/OR PROVIDE DRAWINGS FOR STRUCTURAL FOUNDATIONS, STRUCTURES, AND/OR TRENCHING, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTRACT WITH THE ENGINEER DIRECTLY AND THE COST SHALL BE INCLUDED IN THE BASE BID.

19. ALL SYSTEMS AND SERVICES ARE TO BE LEFT OPERATIONAL PRIOR TO THE END OF EACH

20. THE CONTRACTOR SHALL REMOVE ALL RUBBLE AND DEBRIS FROM THE JOBSITE DAILY AND LEAVE THE BUILDING AND GROUNDS BROOM CLEAN UPON COMPLETION OF THE WORK.

#### OPERATIONAL PHASING NOTES

EXISTING WORKMANSHIP.

**KEYNOTES** 

LEGEND

. TH E CONTRACTOR TO SUBMIT PHASING PLAN AT BID. THE CONTRACTOR SHALL CONFIRM THE OPERATIONAL PHASING WITH THE OWNER PRIOR TO STARTING CONSTRUCTION. 2. THE PHASING SHOWN IS NOT INTENDED TO REPRESENT OR DICTATE CONSTRUCTION

PHASING, MEANS, METHODS OR TECHNIQUES. . THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL CONSTRUCTION PHASING SO AS NOT TO IMPACT THE OWNER'S OPERATIONS.

. REMOVE WALLS, BOLLARDS, PLUMBING, ELECTRICAL, ETC. AND CAP UTILITIES BELOW THE FLOOR AS REQUIRED TO ACHIEVE THE OPERATIONAL PHASING. PROVIDE TEMPORARY UTILITIES AS REQUIRED DURING PHASING.

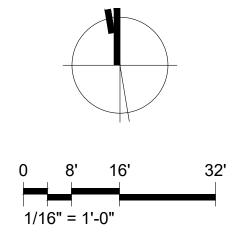
. WHEN THE WORK INCLUDES DEMOLITION OF OR TEMPORARILY COVERING OF THE EXISTING EXTERIOR SIGNAGE, THE CONTRACTOR SHALL PROVIDE A TEMPORARY "COSTCO WHOLESALE" SIGNAGE (OF SIZE COMPARABLE TO EXISTING SIGN) UNTIL PERMANENT SIGN IS INSTALLED. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS. PROVIDE ILLUMINATION FOR THE TEMPORARY SIGN. CONFIRM LOCATIONS WITH THE OWNER.

COSTCO WHOLESALE CORPORATION 730 LAKE DRIVE

ISSAQUAH, WA 98027 T: 425.313.8100 www.costco.com



ISSUE FOR



DOUGLAS E. BROOKBANK, ARCHITECT

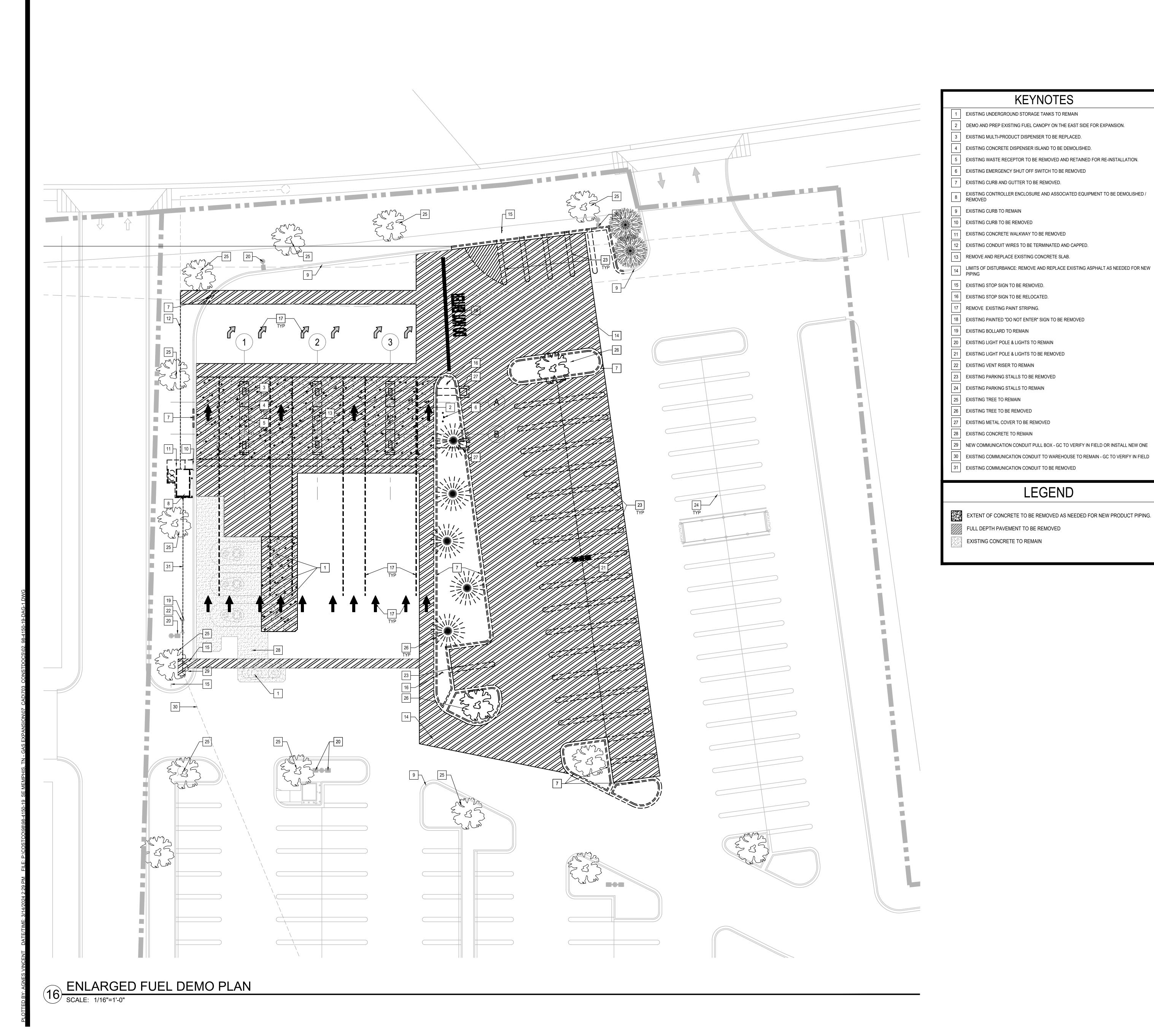
© MG2. All rights reserved. No part of this document may be reproduced in any form or by any means without permission in writing from MG2. DATE DESCRIPTION

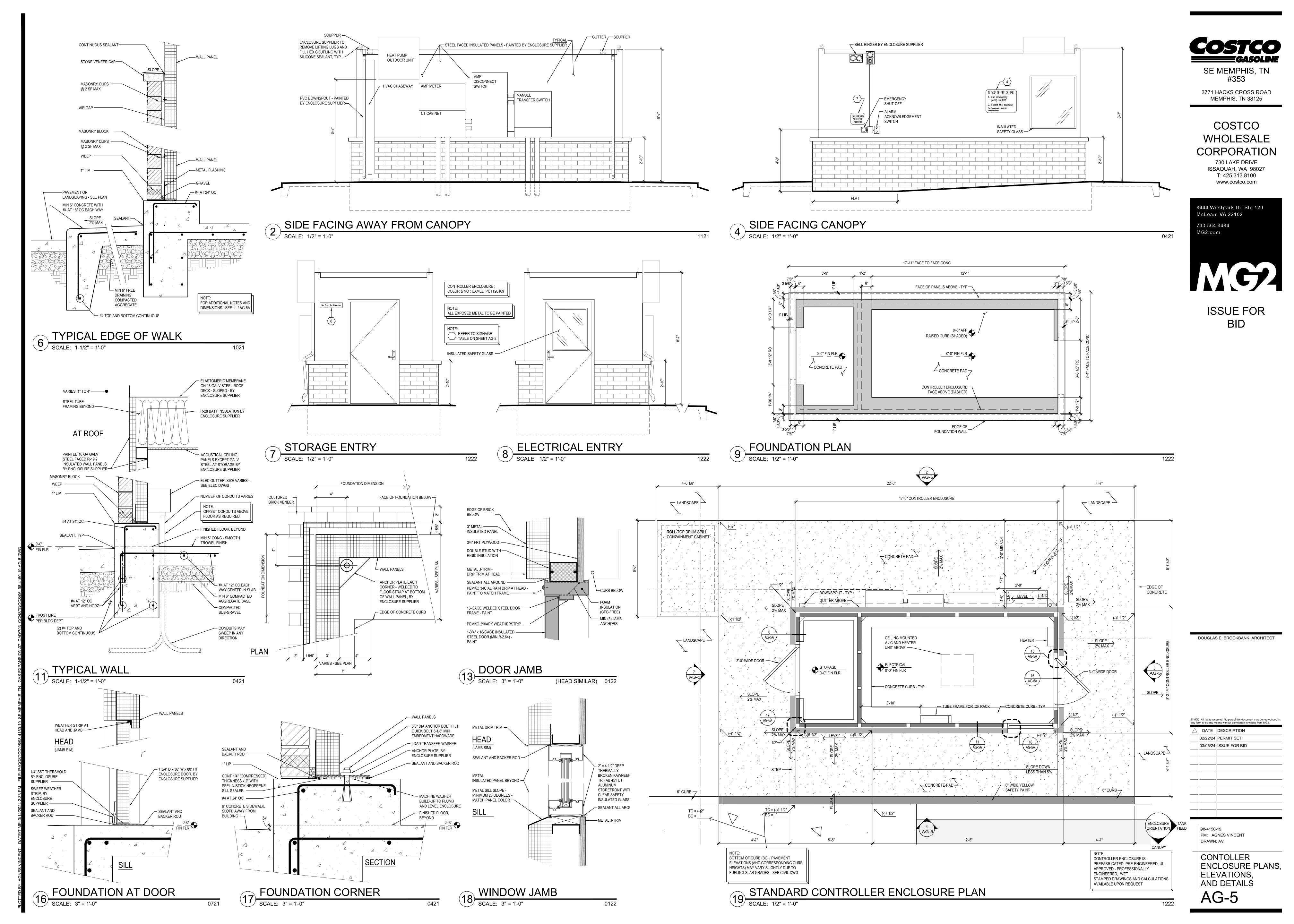
02/22/24 PERMIT SET 03/05/24 ISSUE FOR BID

> 98-4150-19 PM: AGNES VINCENT DRAWN: AV AUGUST 08, 2023

DEMOLITION SITE PLAN

DAG-1







SE MEMPHIS, TN

3771 HACKS CROSS ROAD MEMPHIS, TN 38125

## COSTCO WHOLESALE CORPORATION

730 LAKE DRIVE ISSAQUAH, WA 98027 T: 425.313.8100 www.costco.com



ISSUE FOR

#### GENERAL NOTES

- . ALL WORK, MATERIALS METHODS, ETC SHALL CONFORM TO ALL GOVERNING BUILDING CODES REGULATIONS AND AGENCIES.
- PROVIDE PEDESTRIAN OR VEHICULAR PROTECTION BARRICADES, TEMPORARY GUARD RAILS AND/OR CANOPIES AS REQUIRED BY THE LOCAL AUTHORITIES OR AS NECESSARY FOR PEDESTRIAN
- THE DEVELOPER'S NAME, ADDRESS AND PHONE NUMBER TO BE CONSPICUOUSLY POSTED AT THE CONSTRUCTION SITE DURING ALL PHASES OF CONSTRUCTION.
- ITEMS NOTED AS EXISTING TO REMAIN. CONTRACTOR SHALL REPAIR AND/OR REPLACE, AT CONTRACTOR'S EXPENSE, ANY EXISTING FACILITIES TO REMAIN THAT ARE DAMAGED B CONTRACTOR'S OPERATIONS.
- ALL PAVING OR CONCRETE THAT IS DESTROYED OR REMOVED AS A RESULT OF CONSTRUCTION SHALL BE REPLACED OR PATCHED BY THE GC IN A MANNER THAT MATCHES OR IMPROVES THE CONDITION OF THE EXISTING PAVING. SAWCUT ALL PAVEMENT EDGES PRIOR TO INSTALLING
- ADJACENT PAVEMENT. DOWEL CONCRETE SURFACES WITH #4 DOWELS AT 18" OC. VERIFY DIMENSIONS, LINES, GRADES, ELEVATIONS, AND SITE CONDITIONS PRIOR TO COMMENCING WORK, NOTIFY ARCHITECT PROJECT MANAGER IMMEDIATELY OF ANY DISCREPANCIES.
- CONTRACTOR SHALL TAKE ADEQUATE PROTECTIVE MEASURES AND EXERCISE EXTREME CAUTION WHEN BORING OR TRENCHING SITE IN ORDER TO AVOID DAMAGING EXISTING UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING UNDERGROUND UTILITIES SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- I. SEE CIVIL DRAWINGS FOR GRADING AND DRAINAGE INFORMATION. NO CLEAN-OUTS TO LOCATED IN THE UNDER CANOPY SLAB.
- SEE LANDSCAPE DRAWINGS FOR PLANTING AND IRRIGATION INFORMATION.
- SEE SHEET AG-2, AG-4 AND AG-6 FOR CANOPY ELEVATIONS, CANOPY REFLECTED CEILING PLAN, SIGN DETAILS, AND CANOPY MATERIALS.
- K. SEE ELECTRICAL DRAWINGS FOR SITE ELECTRICAL.
- ALL ELECTRICAL CONDUIT OR CABLE, WHERE SUBJECT TO VEHICULAR TRAFFIC SHALL HAVE A MINIMUM OF 16" COVER.
- . SEE TANK AND ELECTRICAL DRAWINGS FOR FUEL PIPING RUNS AND ELECTRICAL CONDUIT RUNS FOR UNDERGROUND STORAGE TANKS, DISPENSERS, LIGHT FIXTURES AND SECURITY CAMERAS. ALL ELECTRICAL RUNS ARE SHOWN SCHEMATICALLY. THE BEST CONNECTION ROUTE SHOULD BE DETERMINED IN THE FIELD AND INSTALLED PER NATIONAL, STATE AND LOCAL CODE REQUIREMENTS.
- . A CLEARLY LABELED MANUALLY OPERATED EMERGENCY SHUT-OFF SWITCH SHALL BE PROVIDED IN AN APPROVED LOCATION, WITHIN SEVENTY-FIVE (75) FEET OF, BUT NOT NEARER THAN TWENTY-FIVE (17) FEET TO ANY DISPENSER.
- . SIGNS IDENTIFYING THE EMERGENCY SHUT-OFF SWITCH SHALL BE LABELED EMERGENCY SHUTOFF
- . A FIRE EXTINGUISHER WITH A MINIMUM CLASSIFICATION OF 2A29BC SHALL BE PROVIDED AND SO LOCATED TO NOT BE MORE THAN SEVENTY-FIVE (75) FEET FROM ANY PUMP, DISPENSER, OR FILL-PIPE OPENING. LOCATE PER FIRE DEPARTMENT REQUIREMENTS.
- ELD VERIFICATION AND COORDINATION:
  SURVEY DIMENSIONS: DIMENSIONS OF SITE CONDITIONS ARE FROM SURVEY BY OTHERS. TH ARCHITECT BEARS NO RESPONSIBILITY FOR ACCURACY OF SAME.
- EXISTING UTILITY LOCATION: THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF THE SIZE AND LOCATION OF ALL EXISTING UNDERGROUND AND ABOVE GROUND UTILITIES. EXISTING UTILITIES SHOWN HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FO CONVENIENCE ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ADDITIONAL UTILITY LOCATIONS NOT SHOWN. CARE SHOULD BE TAKEN TO AVOID DAMAGE OR DISTURBANCE 1

EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO

## **KEYNOTES**

- 1 EXISTING UNDERGROUND STORAGE TANKS
- 2A EXISTING FUEL CANOPY
- 2B NEW FUEL CANOPY EXPANSION
- NEW ACCESSIBLE MULTI-PRODUCT DISPENSER SEE DETAILS T-6
- NEW CONCRETE DISPENSER ISLAND
- 5A REINSTALL EXISTING WASTE RECEPTOR
- WASTE RECEPTOR, BOLTED DOWN, COMMERCIAL ZONE, 33 US GALLON ROUND, #722101, 5B BLACK - BY GC
- FIRE EXTINGUISHER, QUANTITY AS SHOWN ON PLAN, MOUNTED ON COLUMN SIDE FACING AWAY FROM DISPENSER - FIRE EXTINGUISHER CABINET TO BE, BROOKS MODEL # MARK II, RED CABINET WITH CLEAR COVER (OR EQUAL) - FIRE EXTINGUISHER AND CABINET SUPPLIED BY CONTRACTOR
- NEW EMERGENCY SHUT OFF SWITCH
- 8 PRE-FABRICATED PRE-ENGINEERED NEW CONTROLLER ENCLOSURE: SEE AG-5A
- 9A EXISTING CONCRETE CURB
- 9B NEW CONCRETE CURB
- 10A EXISTING CONCRETE SLAB
- | 10B | NEW CONCRETE SLAB SEE AG-6, STRUCTURAL TANK AND CIVIL DRAWINGS
- 10C NEW HEAVY DUTY ASPHALT: SEE CIVIL
- 11 PROPOSED CONCRETE SWALE SEE CIVIL
- 12 EXISTING VENT STACK TO REMAIN
- 13 EXISTING SITE LIGHT TO REMAIN.
- 14 NEW SITE AREA LIGHT
- 15 NEW BOLLARD SEE DETAIL 14A / AG-6 16 NEW "STOP" AND "DO NOT ENTER" SIGN - SEE DETAIL 8 / AG-6
- 17 RELOCATED EXISTING "STOP" SIGN AND ADD DO NOT ENTER SIGN.
- 18 EXISTING BOLLARDS
- 19 NEW PAINTED TRAFFIC CONTROL STRIPING SEE DETAIL 2 / AG-6 20 NEW PAINTED "DO NOT ENTER" SIGN - SEE DETAIL 13 / AG-6
- 21 NEW PAINTED "STOP" / BAR SEE DETAIL 13 / AG-6
- 22 NEW PAINTED TRAFFIC ARROW SEE DETAIL 4 / AG-6
- 23 EXISTING "MEMBERS ONLY" SIGN 24 NEW "MEMBERS ONLY" SIGN - SEE DETAIL 9 / AG-6
- 25 NEW DRAIN INLET SEE CIVIL
- 26 SPEED BUMP SEE DETAIL 1 / AG-6
- 27 CONTROL JOINT AT END OF ISLANDS SEE 18 / AG-6
- 28 NEW "IN CASE OF FIRE OR SPILL" SIGNAGE ON FOUR SIDES SEE AG-2, 11/AG-6
- 29 NEW "NO SMOKING / STOP ENGINE" SIGNAGE ON TWO SIDES- SEE AG-2, 11/AG-6
- 30 NEW SPLICE BOX BY GC OR UTILITY CO.
- 31 NEW ELECTRICAL CONDUIT BY GC OR UTILITY CO.
- 32 NEW CONTAINMENT SUMP, SEE T1-1
- 33 NEW TMS ADDITIVE CABINET SEE 5 / AG-6
- 34 NEW VST GREEN MACHINE SEE 10 / AG-6
- 35 NEW OVERFILL ALARM ON PEDESTAL SEE 15 / AG-6
- 36 EXISTING PARKING STALLS
- 37 NEW PARKING STALLS
- 38 NEW LANDSCAPING TO MATCH EXISTING ON SITE WHERE DISTURBED
- 39 NOT USED
- 40 EXISTING TREE
- 41 NEW OIL WATER SEPARATOR
- 42 REINSTALL EXISTING DISPENSER
- 43 NEW COMMUNICATION CONDUIT PULL BOX GC TO VERIFY IN FIELD OR INSTALL NEW ONE
- 44 EXISTING COMMUNICATION CONDUIT TO WAREHOUSE GC TO VERIFY IN FIELD
- 45 NEW COMMUNICATION CONDUIT TO CONTROLLER ENCLOSURE BY GC OR UTILITY CO.

## LEGEND

- PROPOSED CONCRETE SLAB
- PROPOSED ASPHALT SLURRY SEAL
- EXISTING CONCRETE

- JCGI NOTES: 1) SJ HAVE BEEN RELOCATED TO END OF ISLAND
- 2) SJ ADDED AT TANK SLAB NEED TO BE AT A MINIMUM OF 2' FROM MAN WAY LIDS TO AVOID SAW-CUTS IN CONCRETE CROWN AT MAN-WAYS 3) DIAGONAL CUTS HAVE BEEN ADDED TO ELIMINATE
- AS MANY DEAD-END SAW-CUTS AS POSSIBLE 4) WHERE SAW CUT MEET CURB LINE TOOLED JOINTS OR SAW-CUTS NEED TO CARRY THROUGH TO ELIMINATE CRACKING AT CURBS

FOR FUEL FACILITY SIGNAGE, SEE TABLE ON AG-2

TANK AND FUELING CONCRETE SLABS NOTE: CJ - CONSTRUCTION JOINT, SEE 16 / AG-6 SJ - SAW-CUT SHRINKAGE JOINT SEE 17 / AG-6

DOUGLAS E. BROOKBANK, ARCHITECT

© MG2. All rights reserved. No part of this document may be reproduced in any form or by any means without permission in writing from MG2.

DATE DESCRIPTION

02/22/24 PERMIT SET 03/05/24 ISSUE FOR BID

98-4150-19 PM: AGNES VINCENT DRAWN: AV

ENLARGED FUEL SITE PLAN

AUGUST 08, 2023

AG-1

ENLARGED FUEL SITE PLAN

NEW CANOPY

NEW CANOPY

26

NEW CANOPY

**CANOPY** 

122'-0"

CANOPY ABOVE

9B

8

16) SCALE: 1/16"=1'-0"